



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois 60560
Telephone: 630-553-4350
www.yorkville.il.us

AGENDA
ECONOMIC DEVELOPMENT COMMITTEE MEETING
Tuesday, August 6, 2019
6:00 p.m.
City Hall Conference Room
800 Game Farm Road, Yorkville, IL

Citizen Comments:

Minutes for Correction/Approval: July 2, 2019

New Business:

1. EDC 2019-59 Building Permit Report for June 2019
2. EDC 2019-60 Building Inspection Report for June 2019
3. EDC 2019-61 Property Maintenance Report for June 2019
4. EDC 2019-62 Economic Development Report for July 2019
5. EDC 2019-63 Lenny's Gas N Wash – Special Use, Sign Variance, and Final Plat
6. EDC 2019-64 208 E. Main Street – Front Yard Setback Variance
7. EDC 2019-65 Tactical Urbanism
8. EDC 2019-66 Downtown Landscape Hill Project – Invitation to Bid
9. EDC 2019-67 Kane/Kendall Council of Mayors Bike Sharing – Memorandum of Understanding
10. EDC 2019-68 Unified Development Ordinance (UDO) – Change Order
11. EDC 2019-69 Old Jail Term Sheet

Old Business:

1. EDC 2019-23 Downtown Form-Based Code and Streetscape Master Plan

Additional Business:

2019/2020 City Council Goals – Economic Development Committee		
Goal	Priority	Staff
“Southside Development”	4	Bart Olson, Krysti Barksdale-Noble & Lynn Dubajic
“Downtown and Riverfront Development”	5	Bart Olson, Tim Evans & Krysti Barksdale-Noble
“Metra Extension”	7	Bart Olson, Rob Fredrickson, Eric Dhuse, Krysti Barksdale-Noble & Erin Willrett
“Manufacturing and Industrial Development”	8 (tie)	Bart Olson, Krysti Barksdale-Noble, Erin Willrett, Lynn Dubajic, Eric Dhuse & Brad Sanderson
“Expand Economic Development Efforts”	10	Krysti Barksdale-Noble & Lynn Dubajic
“Revenue Growth”	13	Rob Fredrickson, Krysti Barksdale-Noble & Lynn Dubajic
“Entrance Signage”	17	Krysti Barksdale-Noble & Erin Willrett

UNITED CITY OF YORKVILLE
WORKSHEET
ECONOMIC DEVELOPMENT COMMITTEE
Tuesday, August 6, 2019
6:00 PM
CITY HALL CONFERENCE ROOM

CITIZEN COMMENTS:

MINUTES FOR CORRECTION/APPROVAL:

1. July 2, 2019
 - Approved _____
 - As presented
 - With corrections

NEW BUSINESS:

1. EDC 2019-59 Building Permit Report for June 2019
 - Informational Item
 - Notes _____

2. EDC 2019-60 Building Inspection Report for June 2019

Informational Item

Notes _____

3. EDC 2019-61 Property Maintenance Report for June 2019

Informational Item

Notes _____

4. EDC 2019-62 Economic Development Report for July 2019

Informational Item

Notes _____

5. EDC 2019-63 Lenny's Gas N Wash – Special Use, Sign Variance, and Final Plat

Moved forward to CC _____

Approved by Committee _____

Bring back to Committee _____

Informational Item

Notes _____

6. EDC 2019-64 208 E. Main Street – Front Yard Setback Variance

Moved forward to CC _____

Approved by Committee _____

Bring back to Committee _____

Informational Item

Notes _____

7. EDC 2019-65 Tactical Urbanism

Moved forward to CC _____

Approved by Committee _____

Bring back to Committee _____

Informational Item

Notes _____

8. EDC 2019-66 Downtown Landscape Hill Project – Invitation to Bid

Moved forward to CC _____

Approved by Committee _____

Bring back to Committee _____

Informational Item

Notes _____

9. EDC 2019-67 Kane/Kendall Council of Mayors Bike Sharing Memorandum of Understanding

Moved forward to CC _____

Approved by Committee _____

Bring back to Committee _____

Informational Item

Notes _____

10. EDC 2019-68 Unified Development Ordinance (UDO) – Change Order

Moved forward to CC _____

Approved by Committee _____

Bring back to Committee _____

Informational Item

Notes _____

11. EDC 2019-69 Old Jail Term Sheet

Moved forward to CC _____

Approved by Committee _____

Bring back to Committee _____

Informational Item

Notes _____

OLD BUSINESS:

1. EDC 2019-23 Downtown Form-Based Code and Streetscape Master Plan

Moved forward to CC _____

Approved by Committee _____

Bring back to Committee _____

Informational Item

Notes _____

ADDITIONAL BUSINESS:



Reviewed By:	
Legal	<input type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

Minutes

Tracking Number

Agenda Item Summary Memo

Title: Minutes of the Economic Development Committee – July 2, 2019

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: _____

Council Action Previously Taken:

Date of Action: _____ Action Taken: _____

Item Number: _____

Type of Vote Required: Majority

Council Action Requested: Committee Approval

Submitted by: Minute Taker
Name Department

Agenda Item Notes:

UNITED CITY OF YORKVILLE
ECONOMIC DEVELOPMENT COMMITTEE
Tuesday, July 2, 2019, 6:00pm
City Conference Room

In Attendance:

Committee Members

Chairman Jackie Milschewski
Alderman Joel Frieders
Alderman Ken Koch

Absent: Alderman Jason Peterson

Other City Officials

Mayor John Purcell (arr. 6:03pm)
City Administrator Bart Olson
Community Development Director Krysti Barksdale-Noble
Senior Planner Jason Engberg
Code Official Pete Ratos

Other Guests

Lynn Dubajic, City Consultant
Katie Finlon, *Kendall County Record*

The meeting was called to order at 6:00pm by Chairman Jackie Milschewski

Citizen Comments: None

Minutes for Correction/Approval: June 4, 2019

The minutes were approved as presented.

New Business

1. EDC 2019-54 Building Permit Report for May 2019

Mr. Ratos said there were 17 single family detached and 5 single family attached for a total of 128 permits for the month.

2. EDC 2019-55 Building Inspection Report for May 2019

There were 389 inspections for the month and most were residential, said Mr. Ratos.

3. EDC 2019-56 Property Maintenance Report for May 2019

Eight cases were heard in May. Mr. Ratos said the hotel under construction was found liable for no silt fence, open manholes and other violations. Mayor Purcell said he received complaints about dumping in a vacant lot preventing the owner from mowing. Mr. Ratos advised the actions that could be taken by the owner.

4. EDC 2019-57 Economic Development Report for June 2019

Ms. Dubajic reported the following:

1. The Roadhouse in Fountain Village is expected to be re-opened by a new group of people who will take over the lease and possibly keep the name.
2. The Pinheadz building is bank-owned and the business is in receivership. The bank also owns a similar property in Richton Park and both properties would both have to be purchased. If it goes into foreclosure the properties can be split.
3. Popeye's is still planning on a freestanding building.

5. EDC 2019-58 Accessory Structures and Uses – Text Amendment

Ms. Noble explained the reason for this amendment and said a resident asked to build a basketball court on their adjoining lot. Their home is one lot and the other lot is for extra space. She said the code was not clear on accessory structures when there was no home on the second lot. Staff decided to revise the code and also researched policies from other communities before suggesting a change in language to be incorporated. The owner will only be allowed one accessory structure and will be required to put in sidewalk and trees prior to the permanent accessory structure. This matter will go to Public Hearing at PZC on July 10th and then to City Council for final approval. Alderman Koch noted in Ward I there are 3 properties like this and one does not have a sidewalk. Mr. Ratos noted that when developers sell adjoining lots, they stipulate the sidewalk must be completed for both.

Old Business

1. EDC 2019-23 Downtown Form-Based Code and Streetscape Master Plan

Ms. Noble said staff had addressed several comments received at last month's meeting and there was brief discussion regarding those comments including overhead lighting. Ms. Noble said 400 people also responded with suggestions for the downtown. Mr. Olson said the purpose of the plan was to pre-load the properties so a permit could easily be pulled and approved in the future. This matter will be brought back in August (or September) to discuss potential downtown uses based on sample buildings on certain properties.

Additional Business

There was no further business and the meeting adjourned at 6:26pm

Minutes respectfully submitted
by Marlys Young, Minute Taker



Reviewed By:	
Legal	<input type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input checked="" type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #1

Tracking Number

EDC 2019-59

Agenda Item Summary Memo

Title: Building Permit Report for June 2019

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: All permits issued in June 2019.

Council Action Previously Taken:

Date of Action: N/A Action Taken: N/A

Item Number: N/A

Type of Vote Required: Informational

Council Action Requested: None

Submitted by: D. Weinert Community Development
 Name Department

Agenda Item Notes:



UNITED CITY OF YORKVILLE

BUILDING PERMIT REPORT

June 2019

TYPES OF PERMITS

	Number of Permits Issued	SFD <i>Single Family Detached</i>	B.U.I.L.D <i>Single Family Detached</i>	SFA <i>Single Family Attached</i>	Multi-Family <i>Apartments Condominiums</i>	Commercial <i>Includes all Permits Issued for Commercial Use</i>	Industrial	Misc.	Construction Cost	Permit Fees
June 2019	240	9	0	0	0	11	0	220	3,314,915.00	125,499.42
Calendar Year 2019	650	74	0	10	0	62	0	504	24,512,878.00	950,158.73
Fiscal Year 2020	369	26	0	5	0	19	0	319	7,042,898.00	340,829.01
June 2018	123	30	0	0	0	11	0	81	61,748,401.00	248,887.93
Calendar Year 2018	543	90	14	36	0	79	0	324	31,278,361.00	1,521,112.55
Fiscal Year 2019	237	48	0	0	0	23	0	166	14,925,169.00	430,762.49
June 2017	92	2	13	0	0	8	0	69	16,688,860.00	559,688.70
Calendar Year 2017	452	27	49	0	0	71	0	305	31,577,273.00	1,343,479.34
Fiscal Year 2018	219	11	21	0	0	27	0	160	20,983,929.00	759,269.81
June 2016	109	0	15	0	0	13	0	81	4,490,566.00	196,837.61
Calendar Year 2016	430	14	53	0	0	64	0	299	18,502,673.00	845,806.19
Fiscal Year 2017	209	7	23	0	0	23	0	156	10,020,497.00	382,404.85



Reviewed By:	
Legal	<input type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input checked="" type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #2

Tracking Number

EDC 2019-60

Agenda Item Summary Memo

Title: Building Inspection Report for June 2019

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: All inspections scheduled in June 2019.

Council Action Previously Taken:

Date of Action: N/A Action Taken: N/A

Item Number: N/A

Type of Vote Required: Informational

Council Action Requested: None

Submitted by: D. Weinert Community Development
Name Department

Agenda Item Notes:

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE	
BF		034-UGE UNDERGROUND ELECTRIC	20170655	577 E KENDALL DR	4&5		06/18/2019	
		Comments1: NORTH SIDE, HOLIDAY INN (INSPECTORS NOTE Comments2: S: TWO 3" PVC PIPES FOR COMED)						
EEI		017-EFL ENGINEERING - FINAL INSPE	20170928	3152 MATLOCK DR	671		06/27/2019	
		Comments1: N - RED POINTE MAPLE 3.2" NOT APPROVED T Comments2: REE. S - RED POINTE MAPLE 2.9" NOT APPRO Comments3: VED TREE.						
PR		014-FIN FINAL INSPECTION	20170934	3168 MATLOCK DR	667		06/10/2019	
PR		015-PLF PLUMBING - FINAL OSR READ					06/10/2019	
EEI		016-EFL ENGINEERING - FINAL INSPE					06/10/2019	
EEI		017-EFL ENGINEERING - FINAL INSPE	20170939	3147 MATLOCK DR	655		06/20/2019	
		Comments1: GREENSPIRE LINDEN NOT ON APPROVED LIST, Comments2: INLET BASKET TO BE REMOVED IN REAR, SURF Comments3: ACE DAMAGE ON SIDEWALK TO BE REPLACED						
PR		016-FIN FINAL INSPECTION	20170940	3177 MATLOCK DR	661		06/10/2019	
PR		017-PLF PLUMBING - FINAL OSR READ					06/10/2019	
EEI		018-EFL ENGINEERING - FINAL INSPE					06/10/2019	
BC		014-FIN FINAL INSPECTION	20170964	1942 RENA LN	10		06/12/2019	
PR		015-PLF PLUMBING - FINAL OSR READ				06/12/2019		
BC		016-FIN FINAL INSPECTION					06/27/2019	
BC		014-FIN FINAL INSPECTION	20170965	1944 RENA LN	10		06/12/2019	
PR		015-PLF PLUMBING - FINAL OSR READ					06/12/2019	
BC		016-FIN FINAL INSPECTION					06/27/2019	
BC		014-FIN FINAL INSPECTION	20170966	1946 RENA LN	10		06/12/2019	
PR		015-PLF PLUMBING - FINAL OSR READ					06/12/2019	
BC		016-FIN FINAL INSPECTION					06/27/2019	
BC		014-FIN FINAL INSPECTION	20170967	1948 RENA LN	10		06/12/2019	
PR		015-PLF PLUMBING - FINAL OSR READ					06/12/2019	

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	_____	016-FIN FINAL INSPECTION					06/27/2019
BC	_____	015-FIN FINAL INSPECTION	20170968	1952 RENA LN	10		06/12/2019
PR	_____	016-PLF PLUMBING - FINAL OSR READ					06/12/2019
BC	_____	017-FIN FINAL INSPECTION					06/27/2019
BC	_____	016-FIN FINAL INSPECTION	20170969	1954 RENA LN	10		06/12/2019
PR	_____	017-PLF PLUMBING - FINAL OSR READ					06/12/2019
BC	_____	018-FIN FINAL INSPECTION					06/27/2019
BC	_____	015-FIN FINAL INSPECTION	20170970	1968 RENA LN	9	06/13/2019	
PR	_____	016-PLF PLUMBING - FINAL OSR READ				06/13/2019	
BF	_____	017-FIN FINAL INSPECTION Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019
PBF	_____	018-PLF PLUMBING - FINAL OSR READ Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019
BC	_____	013-FIN FINAL INSPECTION	20170971	1966 RENA LN	9	06/13/2019	
PR	_____	014-PLF PLUMBING - FINAL OSR READ				06/13/2019	
BF	_____	015-FIN FINAL INSPECTION Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019
PBF	_____	016-PLF PLUMBING - FINAL OSR READ Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019
BC	_____	013-FIN FINAL INSPECTION	20170972	1974 RENA LN	9	06/13/2019	
PR	_____	014-PLF PLUMBING - FINAL OSR READ				06/13/2019	
BF	_____	015-FIN FINAL INSPECTION Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET				06/28/2019	
PBF	_____	016-PLF PLUMBING - FINAL OSR READ Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	_____	012-FIN FINAL INSPECTION	20170973	1962 RENA LN	9	06/13/2019	
PR	_____	013-PLF PLUMBING - FINAL OSR READ				06/13/2019	
BF	_____	014-FIN FINAL INSPECTION Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019
PBF	_____	015-PLF PLUMBING - FINAL OSR READ Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019
BC	_____	014-FIN FINAL INSPECTION	20170974	1972 RENA LN	9	06/13/2019	
PR	_____	015-PLF PLUMBING - FINAL OSR READ				06/13/2019	
BF	_____	016-FIN FINAL INSPECTION Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019
PBF	_____	017-PLF PLUMBING - FINAL OSR READ Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019
BC	_____	012-FIN FINAL INSPECTION	20170975	1964 RENA LN	9	06/13/2019	
PR	_____	013-PLF PLUMBING - FINAL OSR READ				06/13/2019	
BF	_____	014-FIN FINAL INSPECTION Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019
PBF	_____	015-PLF PLUMBING - FINAL OSR READ Comments1: NUMEROUS ITEMS FAILED - REFER TO INSPECT Comments2: ION SHEET					06/28/2019
BF	_____	023-WKS PUBLIC & SERVICE WALKS Comments1: RUAN, BBW	20170994	967 N CARLY CIR	124		06/13/2019
EEI	_____	024-EFL ENGINEERING - FINAL INSPE Comments1: PARKWAY TREE NOT ON APPROVED LIST. DAMAG Comments2: ED SIDEWALK					06/27/2019
BF	_____	021-WKS PUBLIC & SERVICE WALKS Comments1: RUAN, BBW	20180158	1012 S CARLY CIR	113		06/13/2019
BF	_____	PM 009-WKS PUBLIC & SERVICE WALKS Comments1: UPLAND CANCELLED	20180330	3126 MATLOCK DR	677	06/17/2019	

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	_____	008-RFR ROUGH FRAMING	20180331	3122 MATLOCK DR	679		06/17/2019
BC	_____	009-REL ROUGH ELECTRICAL					06/17/2019
BC	_____	010-RMC ROUGH MECHANICAL					06/17/2019
PR	_____	011-PLR PLUMBING - ROUGH					06/17/2019
BC	_____	012-INS INSULATION					06/19/2019
BF	_____	013-WKS PUBLIC & SERVICE WALKS					06/17/2019
EEI	_____	019-EFL ENGINEERING - FINAL INSPE Comments1: REMOVE REAR INLET BASKET. N AMERICAN SEN Comments2: TRY LINDEN 2.3" . GRADING/SOD PITCHED TO Comments3: WARD SW HOLDING WATER. SW ADA & SURFACE Comments4: DAMAGE.	20180335	3101 MATLOCK DR	635		06/27/2019
BF	_____	013-STP STOOP	20180336	3106 MATLOCK DR	681		06/18/2019
BF	_____	014-WKS PUBLIC & SERVICE WALKS Comments1: UPLAND					06/28/2019
EEI	_____	017-EFL ENGINEERING - FINAL INSPE Comments1: GREENSPIRE LINDEN MEASURED 2.3" - NOT AP Comments2: PROVED TREE	20180360	3132 MATLOCK DR	675		06/20/2019
EEI	_____	020-EFL ENGINEERING - FINAL INSPE Comments1: SWAMP WHITE OAK 2.3"	20180362	3105 MATLOCK DR	636		06/27/2019
BC	_____	013-STP STOOP	20180364	3102 MATLOCK DR	682		06/04/2019
BF	_____	014-WKS PUBLIC & SERVICE WALKS Comments1: UPLAND					06/28/2019
EEI	_____	017-EFL ENGINEERING - FINAL INSPE Comments1: REDMOND LINDEN MEASURED 2.1", AUX UB BUR Comments2: IED, REMOVE INLET BASKET IN REAR	20180366	3173 MATLOCK DR	660		06/20/2019
EEI	_____	021-REI REINSPECTION Comments1: ONE SQUARE OF SIDEWALK DAMAGED, MUST BE Comments2: REPLACED	20180386	2505 LYMAN LOOP	78		06/03/2019
BC	_____	011-FIN FINAL INSPECTION	20180463	512 E MAIN ST	2		06/20/2019
BF	_____	020-WKS PUBLIC & SERVICE WALKS Comments1: RUAN, BBW	20180466	971 S CARLY CIR	91		06/13/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BF		018-WKS PUBLIC & SERVICE WALKS Comments1: RUAN, BBW	20180467	946 N CARLY CIR	54		06/13/2019
BC		002-RFR ROUGH FRAMING	20180515	1149 HOMESTEAD DR	138		06/25/2019
EEI		014-EFL ENGINEERING - FINAL INSPE Comments1: MARMO MAPLE NOT APPROVED TREE. BURIED SA Comments2: NITARY STRUCTURE. SURFACE DAMAGE ON SW.	20180546	3166 MATLOCK DR	668		06/28/2019
BF	PM	015-WKS PUBLIC & SERVICE WALKS Comments1: UPLAND	20180547	3124 MATLOCK DR	678		06/17/2019
EEI		017-EFL ENGINEERING - FINAL INSPE Comments1: RED POINTE MAPLE 2.4" NOT APPROVED TREE. Comments2: DAMAGED SW.	20180548	3128 MATLOCK DR	676		06/27/2019
BC		003-FIN FINAL INSPECTION Comments1: GATE LATCH TO REMAIN LOCKED WHEN POOL IS Comments2: NOT IN USE	20180583	110 E KENDALL DR			06/19/2019
EEI		020-EFL ENGINEERING - FINAL INSPE Comments1: TEMP TO FULL	20180593	2628 MCLELLAN BLVD	58		06/03/2019
EEI		022-EFL ENGINEERING - FINAL INSPE Comments1: TEMP TO FINAL	20180594	2631 BURR ST	81		06/03/2019
BC		001-PHF POST HOLE - FENCE	20180673	911 PURCELL ST	65		06/14/2019
EEI		016-REI REINSPECTION	20180720	2843 KETCHUM CT	219		06/28/2019
BC		003-FIN FINAL INSPECTION	20180729	1973 MEADOWLARK LN	121		06/21/2019
BF		017-EPW ENGINEERING- PUBLIC WALK Comments1: CAL, MIDWEST	20180738	3101 LAUREN DR	93		06/13/2019
BC		003-FIN FINAL INSPECTION	20180767	1845 ASTER DR	95		06/10/2019
EEI		020-REI REINSPECTION	20180794	978 S CARLY CIR	107	06/28/2019	
BF		025-PPS PRE-POUR, SLAB ON GRADE Comments1: ADA RAMP, CAL, MIDWEST	20180826	3171 LAUREN DR	86		06/13/2019
PR		013-FIN FINAL INSPECTION	20180854	1131 GRACE DR	62		06/12/2019
PR		014-PLF PLUMBING - FINAL OSR READ					06/12/2019
EEI		015-EFL ENGINEERING - FINAL INSPE					06/12/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
PR		010-FIN FINAL INSPECTION	20180873	211 E SPRING ST			06/27/2019
		Comments1: GUARD EXPOSED ROMEX IN BSMT FROM BOXES U Comments2: P. FLUSH ALL OUTLETS AND SQUARE THEM. RE Comments3: PAIR GFCI IN GARAGE. CONDUIT AROUND POWE Comments4: R TO A/C UNITS.					
BF		019-EPW ENGINEERING- PUBLIC WALK	20180885	3301 LAUREN DR	80		06/03/2019
		Comments1: MIDWEST					
PR		015-FIN FINAL INSPECTION	20180936	1901 WREN RD	13		06/20/2019
PR		016-PLF PLUMBING - FINAL OSR READ					06/20/2019
E EI		017-EFL ENGINEERING - FINAL INSPE					06/21/2019
		Comments1: REMOVE REAR INLET BASKET, MARMO MAPLE NO Comments2: T ON APPROVED LIST					
BF		008-RFR ROUGH FRAMING	20180964	3101 REHBEHN CT	637		06/04/2019
		Comments1: DR HORTON					
BF		009-REL ROUGH ELECTRICAL					06/04/2019
BF		010-RMC ROUGH MECHANICAL					06/04/2019
PR		011-PLR PLUMBING - ROUGH					06/04/2019
BC		012-INS INSULATION					06/06/2019
BF		013-STP STOOP					06/18/2019
		Comments1: UPLAND					
BF		014-WKS PUBLIC & SERVICE WALKS					06/27/2019
		Comments1: UPLAND					
BF		008-RFR ROUGH FRAMING	20180966	3105 REHBEHN CT	638		06/24/2019
BF		009-REL ROUGH ELECTRICAL					06/24/2019
BF		010-RMC ROUGH MECHANICAL					06/24/2019
PR		011-PLR PLUMBING - ROUGH					06/24/2019
BF		012-INS INSULATION					06/26/2019
		Comments1: HORTON, GR					
BF		010-RFR ROUGH FRAMING	20180968	3109 REHBEHN CT	639		06/21/2019
BF		011-REL ROUGH ELECTRICAL					06/21/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BF	_____	012-RMC ROUGH MECHANICAL					06/21/2019
PR	_____	013-PLR PLUMBING - ROUGH					06/21/2019
BF	_____	014-INS INSULATION Comments1: GR - DR HORTON					06/26/2019
BC	_____	008-RFR ROUGH FRAMING	20180969	3121 REHBEHN CT	640		06/07/2019
BC	_____	009-REL ROUGH ELECTRICAL					06/07/2019
BC	_____	010-RMC ROUGH MECHANICAL					06/07/2019
PR	_____	011-PLR PLUMBING - ROUGH					06/07/2019
BC	_____	012-INS INSULATION					06/11/2019
BF	_____	009-BGS BASEMENT GARAGE STOOPS Comments1: UPLAND	20180970	3122 REHBEHN CT	648		06/18/2019
BC	_____	007-BGS BASEMENT GARAGE STOOPS	20180971	3125 REHBEHN CT	641		06/04/2019
BF	_____	010-RFR ROUGH FRAMING	20180972	3126 REHBEHN CT	647		06/13/2019
BF	_____	011-REL ROUGH ELECTRICAL					06/13/2019
BF	_____	012-RMC ROUGH MECHANICAL					06/13/2019
PR	_____	013-PLR PLUMBING - ROUGH				06/13/2019	
BC	_____	014-INS INSULATION					06/17/2019
BF	_____	015-PPS PRE-POUR, SLAB ON GRADE Comments1: UPLAND, STOOPS & GAR FLOOR					06/18/2019
BF	_____	016-WKS PUBLIC & SERVICE WALKS Comments1: UPLAND					06/28/2019
BF	_____	PM 014-WKS PUBLIC & SERVICE WALKS Comments1: UPLAND	20180974	3132 REHBEHN CT	646		06/17/2019
BC	_____	007-BGS BASEMENT GARAGE STOOPS	20180975	3133 REHBEHN CT	643		06/05/2019
BF	_____	007-BGS BASEMENT GARAGE STOOPS Comments1: UPLAND, GRANDE RESERVE	20180977	3137 REHBEHN CT	644		06/12/2019
BC	_____	019-WKS PUBLIC & SERVICE WALKS	20181003	3162 LAUREN DR	113		06/14/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
PR	_____	012-SUM SUMP	20181004	2581 LYMAN LOOP	37		06/04/2019
BF	_____	AM 013-WKS PUBLIC & SERVICE WALKS Comments1: COX, GR					06/20/2019
BC	_____	PM 001-FIN FINAL INSPECTION Comments1: FURNACE & A/C	20181012	211 E SPRING ST			06/28/2019
EEI	_____	019-EFL ENGINEERING - FINAL INSPE	20181019	4210 E MILLBROOK CIR	289		06/03/2019
BF	_____	014-WKS PUBLIC & SERVICE WALKS Comments1: RANDA	20181025	1101 BLACKBERRY SHORE LN	44		06/03/2019
BF	_____	013-PWK PRIVATE WALKS Comments1: RANDA GROUP	20181026	901 BLACKBERRY SHORE LN	24		06/03/2019
PR	_____	008-RFR ROUGH FRAMING	20190001	938 PURCELL ST	82		06/06/2019
PR	_____	009-REL ROUGH ELECTRICAL					06/06/2019
PR	_____	010-RMC ROUGH MECHANICAL					06/06/2019
PR	_____	011-PLR PLUMBING - ROUGH					06/06/2019
BF	_____	012-INS INSULATION Comments1: BBW, MEADOWBROOK					06/13/2019
BC	_____	AM 013-STP STOOP					06/24/2019
BF	_____	019-EPW ENGINEERING- PUBLIC WALK Comments1: CAL, MIDWEST	20190002	3258 BOOMBAH BLVD	140		06/13/2019
BF	_____	AM 001-FIN FINAL INSPECTION	20190007	4510 GARDINER AVE	1138		06/04/2019
BC	_____	AM 012-GAR GARAGE FLOOR	20190016	991 BLACKBERRY SHORE LN	33		06/21/2019
BC	_____	AM 013-STP STOOP					06/21/2019
BC	_____	014-WKS PUBLIC & SERVICE WALKS	20190017	1093 REDWOOD DR	47		06/17/2019
BC	_____	AM 001-ELU ELECTRICAL - UNDERSLAB Comments1: TRENCH	20190028	1652 N BEECHER RD	54		06/18/2019
BF	_____	002-RFR ROUGH FRAMING Comments1: TARGET - JOEL 812-371-4784					06/26/2019
BF	_____	003-REL ROUGH ELECTRICAL Comments1: TARGET, FITTING ROOMS 812-371-4784					06/26/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
EEI	_____	020-EFL ENGINEERING - FINAL INSPE	20190030	4351 E MILLBROOK CIR	209		06/03/2019
BF	_____	015-EPW ENGINEERING- PUBLIC WALK Comments1: MIDWEST	20190032	3291 LAUREN DR	81		06/03/2019
BF	_____	012-WKS PUBLIC & SERVICE WALKS Comments1: MIDWEST	20190033	3251 LAUREN DR	83		06/03/2019
PR	_____	013-FIN FINAL INSPECTION					06/14/2019
PR	_____	014-PLF PLUMBING - FINAL OSR READ					06/14/2019
PR	_____	015-REI REINSPECTION Comments1: FINALS					06/17/2019
PR	_____	014-FIN FINAL INSPECTION	20190035	2683 MCLELLAN BLVD	49		06/10/2019
PR	_____	015-PLF PLUMBING - FINAL OSR READ					06/10/2019
EEI	_____	016-EFL ENGINEERING - FINAL INSPE					06/13/2019
EEI	_____	017-REI REINSPECTION					06/27/2019
PR	_____	PM 014-SUM SUMP	20190036	2692 MCLELLAN BLVD	51		06/18/2019
PR	_____	015-FIN FINAL INSPECTION					06/25/2019
PR	_____	008-SUM SUMP	20190040	2577 LYMAN LOOP	36	06/18/2019	
PR	_____	013-SUM SUMP	20190043	2583 LYMAN LOOP	38		06/04/2019
PR	_____	014-FIN FINAL INSPECTION					06/19/2019
PR	_____	015-PLF PLUMBING - FINAL OSR READ					06/19/2019
EEI	_____	016-EFL ENGINEERING - FINAL INSPE					06/24/2019
BC	_____	001-PPS PRE-POUR, SLAB ON GRADE	20190049	433 E BARBERRY CIR	153		06/17/2019
BC	_____	005-FIN FINAL INSPECTION	20190055	2393 AUTUMN CREEK BLVD	269		06/05/2019
BF	_____	AM 014-WKS PUBLIC & SERVICE WALKS Comments1: WINDETT, COMEX	20190060	2445 WYTHE PL	6		06/07/2019
PR	_____	015-SUM SUMP					06/19/2019
EEI	_____	016-EFL ENGINEERING - FINAL INSPE					06/24/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
PR	_____	017-FIN FINAL INSPECTION					06/24/2019
PR	_____	018-PLF PLUMBING - FINAL OSR READ					06/24/2019
EEI	_____	019-REI REINSPECTION					06/27/2019
BF	_____	AM 014-WKS PUBLIC & SERVICE WALKS Comments1: WINDETT, COMEX	20190061	2451 WYTHE PL	7		06/07/2019
BC	_____	006-RFR ROUGH FRAMING	20190064	981 BLACKBERRY SHORE LN	32		06/06/2019
BC	_____	007-REL ROUGH ELECTRICAL					06/06/2019
BC	_____	008-RMC ROUGH MECHANICAL					06/06/2019
PR	_____	009-PLR PLUMBING - ROUGH					06/06/2019
BC	_____	010-INS INSULATION Comments1: BASEMENT PERIMETER NOT COMPLETE CHECK ON Comments2: FINAL					06/17/2019
BC	_____	011-GAR GARAGE FLOOR					06/25/2019
BC	_____	012-STP STOOP					06/25/2019
BF	_____	008-RFR ROUGH FRAMING Comments1: RYAN, GR	20190066	2835 KETCHUM CT	217		06/18/2019
BF	_____	009-REL ROUGH ELECTRICAL Comments1: INSPECTION NOTES: ALL ELECTRIC BOXES NEE Comments2: D TO BE AIR TIGHT, CEILING BOXES NEED TO Comments3: BE DONE					06/18/2019
BF	_____	010-RMC ROUGH MECHANICAL					06/18/2019
PR	_____	011-PLR PLUMBING - ROUGH					06/18/2019
BC	_____	012-INS INSULATION					06/20/2019
BF	_____	014-EPW ENGINEERING- PUBLIC WALK Comments1: CAL MIDWEST	20190067	3192 LAUREN DR			06/13/2019
EEI	_____	015-REI REINSPECTION					06/27/2019
PR	_____	013-FIN FINAL INSPECTION	20190085	2828 KETCHUM CT	215		06/10/2019
PR	_____	014-PLF PLUMBING - FINAL OSR READ					06/10/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
EEI	_____	014-EFL ENGINEERING - FINAL INSPE	20190086	2847 KETCHUM CT	220		06/24/2019
PR	_____	015-FIN FINAL INSPECTION					06/26/2019
PR	_____	016-PLF PLUMBING - FINAL OSR READ					06/26/2019
EEI	_____	017-REI REINSPECTION					06/26/2019
BF	_____	012-PPS PRE-POUR, SLAB ON GRADE Comments1: PUBLIC WALK & STOOPS UPLAND	20190089	1941 WREN RD	17		06/12/2019
BC	_____	011-RFR ROUGH FRAMING Comments1: REINSPECTION	20190100	501 OMAHA DR	3		06/04/2019
BC	_____	012-RMC ROUGH MECHANICAL Comments1: REINSPECTION					06/04/2019
BC	_____	013-INS INSULATION					06/06/2019
BC	_____	014-GAR GARAGE FLOOR					06/24/2019
BC	_____	015-STP STOOP					06/24/2019
BC	_____	002-TRN TRENCH - (GAS, ELECTRIC,	20190104	1482 WALSH DR	193		06/03/2019
BC	_____	AM 013-STP STOOP	20190105	800 ALEXANDRA LN	20		06/27/2019
BC	_____	014-PPS PRE-POUR, SLAB ON GRADE Comments1: PATIO					06/27/2019
PR	_____	014-FIN FINAL INSPECTION	20190114	2821 SILVER SPRINGS CT	245		06/19/2019
PR	_____	015-PLF PLUMBING - FINAL OSR READ					06/19/2019
EEI	_____	016-EFL ENGINEERING - FINAL INSPE					06/21/2019
BC	_____	AM 002-FTG FOOTING Comments1: MIDWEST	20190115	3168 BOOMBAH BLVD	130		06/06/2019
BC	_____	PM 003-FOU FOUNDATION					06/10/2019
BF	_____	004-BKF BACKFILL Comments1: MIDWEST					06/17/2019
BF	_____	005-BGS BASEMENT GARAGE STOOPS Comments1: NOT READY TO POUR					06/20/2019
PR	_____	006-PLU PLUMBING - UNDERSLAB					06/20/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	_____	007-BGS BASEMENT GARAGE STOOPS					06/20/2019
BC	_____	008-STP STOOP					06/21/2019
BC	_____ pm	006-BSM BASEMENT FLOOR	20190130	1112 CARLY DR	26		06/07/2019
BC	_____	007-RFR ROUGH FRAMING Comments1: ADD HURRICANE TIES TO RAFTERS/JOISTS ON Comments2: REAR PORCH. REINSPECT WITH INSULATION.					06/19/2019
BC	_____	008-REL ROUGH ELECTRICAL					06/19/2019
BC	_____	009-RMC ROUGH MECHANICAL					06/19/2019
PR	_____	010-PLR PLUMBING - ROUGH					06/19/2019
BC	_____ PM	011-INS INSULATION					06/21/2019
BC	_____	001-PHF POST HOLE - FENCE Comments1: CANCEL DUE TO STORM DAMAGE	20190134	2636 BURR ST	12	06/10/2019	
PR	_____	008-PLR PLUMBING - ROUGH	20190160	2799 GAINS CT	195		06/12/2019
BC	_____	009-REL ROUGH ELECTRICAL					06/12/2019
BC	_____	010-RMC ROUGH MECHANICAL					06/12/2019
BC	_____	011-RFR ROUGH FRAMING					06/12/2019
BC	_____	012-INS INSULATION					06/14/2019
BF	_____ AM	013-WKS PUBLIC & SERVICE WALKS					06/21/2019
BC	_____	001-PPS PRE-POUR, SLAB ON GRADE Comments1: SHED	20190162	229 WALSH CIR	40		06/06/2019
BC	_____	002-RFR ROUGH FRAMING	20190182	2411 ALAN DALE LN	134		06/14/2019
PR	_____	001-RFR ROUGH FRAMING	20190183	2544 MADDEN CT	11		06/21/2019
PR	_____	002-REL ROUGH ELECTRICAL					06/21/2019
PR	_____	003-PLR PLUMBING - ROUGH					06/21/2019
BF	_____ AM	004-BKF BACKFILL Comments1: MIDWEST	20190190	4376 E MILLBROOK CIR	269		06/03/2019
PR	_____	005-PLU PLUMBING - UNDERSLAB					06/07/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	_____	AM 006-BGS BASEMENT GARAGE STOOPS					06/10/2019
PR	_____	AM 007-WAT WATER					06/25/2019
BC	_____	AM 006-BG BASEMENT AND GARAGE FLOOR	20190193	272 WINDETT RIDGE RD	17		06/11/2019
PR	_____	005-PLU PLUMBING - UNDERSLAB	20190199	875 PURCELL ST	71		06/17/2019
BF	_____	006-BSM BASEMENT FLOOR					06/25/2019
BC	_____	003-FIN FINAL INSPECTION	20190203	478 HONEYSUCKLE LN	160		06/06/2019
		Comments1: IRC APP G AG105.0(8) ACCESS GATE MUST BE Comments2: SELF CLOSING/LATCHING WITH NO OPENING L Comments3: ARGER THAN 1/2" WITHIN 18" OF RELEASE ME Comments4: CHANISM. GARES NOT 48" TALL, MEASURED FR					
BC	_____	001-FIN FINAL INSPECTION	20190212	310 ILLINI DR	10		06/19/2019
BC	_____	002-FIN FINAL INSPECTION	20190217	1979 MEADOWLARK LN	118		06/11/2019
BC	_____	002-FIN FINAL INSPECTION	20190218	2422 FITZHUGH TURN	146		06/03/2019
		Comments1: DECK					
BC	_____	001-FIN FINAL INSPECTION	20190221	1538 SIENNA DR	76		06/03/2019
BC	_____	002-FIN FINAL INSPECTION	20190222	114 CONOVER CT	11		06/19/2019
		Comments1: GATE LATCHES MUST BE AT LEAST 54" ABOVE Comments2: GRADE, CURRENTLY AT 36"					
BC	_____	AM 001-PPS PRE-POUR, SLAB ON GRADE	20190232	2929 GRANDE TR	381		06/11/2019
		Comments1: EARLY AM					
BC	_____	003-RFR ROUGH FRAMING	20190243	1121 BLACKBERRY SHORE LN	46		06/14/2019
BC	_____	004-REL ROUGH ELECTRICAL					06/14/2019
BC	_____	AM 005-INS INSULATION					06/21/2019
BC	_____	001-FIN FINAL INSPECTION	20190245	131 E HYDRAULIC AVE			06/14/2019
BC	_____	001-PPS PRE-POUR, SLAB ON GRADE	20190250	3183 MATLOCK DR	662		06/13/2019
PR	_____	AM 005-PLU PLUMBING - UNDERSLAB	20190255	841 OMAHA DR	20		06/21/2019
BC	_____	PM 006-BSM BASEMENT FLOOR					06/24/2019
BC	_____	001-FIN FINAL INSPECTION	20190260	114 CONOVER CT	11		06/19/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	_____	AM 001-PHF POST HOLE - FENCE	20190263	2056 HEARTHSTONE AVE	342		06/04/2019
BC	_____	002-FIN FINAL INSPECTION					06/11/2019
BF	_____	007-RFR ROUGH FRAMING	20190265	2785 GAINS CT	191		06/25/2019
		Comments1: GR- RYAN					
BF	_____	008-REL ROUGH ELECTRICAL					06/25/2019
BF	_____	009-RMC ROUGH MECHANICAL					06/25/2019
PR	_____	010-PLR PLUMBING - ROUGH					06/25/2019
BF	_____	011-INS INSULATION					06/27/2019
		Comments1: GR					
PR	_____	005-PLU PLUMBING - UNDERSLAB	20190266	832 ALEXANDRA LN	36		06/10/2019
BC	_____	006-RFR ROUGH FRAMING					06/14/2019
BC	_____	007-REL ROUGH ELECTRICAL					06/14/2019
BC	_____	008-RMC ROUGH MECHANICAL					06/14/2019
PR	_____	009-PLR PLUMBING - ROUGH					06/14/2019
BF	_____	010-BSM BASEMENT FLOOR					06/14/2019
		Comments1: MARKER, HMD					
BF	_____	011-INS INSULATION					06/19/2019
		Comments1: MARKER, HMD					
BC	_____	PM 001-PPS PRE-POUR, SLAB ON GRADE	20190271	861 PRAIRIE CROSSING DR	172		06/25/2019
BC	_____	001-PHD POST HOLE - DECK	20190272	2322 WINTERTHUR GREEN	178		06/27/2019
BC	_____	001-FTG FOOTING	20190276	3271 LAUREN DR	82		06/06/2019
BC	_____	AM 002-FOU FOUNDATION					06/11/2019
BC	_____	003-BKF BACKFILL					06/17/2019
BF	_____	004-BGS BASEMENT GARAGE STOOPS					06/20/2019
		Comments1: NOT READY TO POUR					
PR	_____	005-PLU PLUMBING - UNDERSLAB					06/20/2019
BC	_____	PM 006-BGS BASEMENT GARAGE STOOPS					06/20/2019
		Comments1: CRAWL					

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	_____	007-STP STOOP					06/21/2019
BC	_____	003-FOU FOUNDATION	20190279	1992 MEADOWLARK LN	143		06/05/2019
BC	_____	004-BKF BACKFILL Comments1: NORWOOD, COUNTRY HILLS					06/10/2019
BF	_____	AM 005-BKF BACKFILL Comments1: RSS	20190287	709 CLOVER CT	5		06/05/2019
BC	_____	PM 001-PPS PRE-POUR, SLAB ON GRADE	20190289	711 KENTSHIRE DR	134		06/20/2019
BC	_____	AM 001-PPS PRE-POUR, SLAB ON GRADE Comments1: EARLY AM - PATIO	20190290	2943 GRANDE TR	383		06/11/2019
BC	_____	003-TRN TRENCH - (GAS, ELECTRIC,	20190291	1991 MEADOWLARK LN	113		06/04/2019
PR	_____	004-ESW ENGINEERING - SEWER / WAT	20190292	1455 CRIMSON LN	222-1		06/05/2019
PR	_____	004-ESW ENGINEERING - SEWER / WAT	20190293	1453 CRIMSON LN	222-2	06/05/2019	
PR	_____	004-ESW ENGINEERING - SEWER / WAT	20190294	1451 CRIMSON LN	222-3	06/05/2019	
PR	_____	004-ESW ENGINEERING - SEWER / WAT	20190295	1449 CRIMSON LN	222-4	06/05/2019	
PR	_____	004-ESW ENGINEERING - SEWER / WAT	20190296	1447 CRIMSON LN	222-5	06/05/2019	
BF	_____	AM 001-FTG FOOTING Comments1: MIDWEST, GR	20190302	2037 SQUIRE CIR	208		06/14/2019
BF	_____	PM 002-FOU FOUNDATION Comments1: MIDWEST					06/21/2019
BF	_____	AM 003-BKF BACKFILL					06/25/2019
PR	_____	005-WAT WATER				06/27/2019	
PR	_____	006-WAT WATER				06/28/2019	
BC	_____	PM 001-FTG FOOTING	20190303	2052 SQUIRE CIR	190		06/06/2019
BC	_____	AM 002-FOU FOUNDATION					06/10/2019
BC	_____	003-BKF BACKFILL					06/12/2019
PR	_____	004-ESS ENGINEERING - STORM Comments1: NOT READY					06/14/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
PR		005-WAT WATER Comments1: NOT READY					06/14/2019
PR		006-PLU PLUMBING - UNDERSLAB					06/20/2019
PR		007-REI REINSPECTION Comments1: WATER/STORM					06/18/2019
BF		AM 008-BGS BASEMENT GARAGE STOOPS Comments1: GR-RYAN					06/21/2019
BC		001-FIN FINAL INSPECTION Comments1: RECEPTACLE	20190309	503 SUTTON ST	212		06/10/2019
BC		001-TRN TRENCH - (GAS, ELECTRIC,	20190311	2867 MCMURTRIE WAY			06/27/2019
BC		PM 001-PHF POST HOLE - FENCE	20190312	2651 MCLELLAN BLVD	46		06/05/2019
BF		002-FOU FOUNDATION Comments1: MIDWEST, GR	20190320	2078 SQUIRE CIR	182		06/07/2019
PR		003-PLU PLUMBING - UNDERSLAB					06/17/2019
PR		AM 004-WAT WATER					06/11/2019
PR		005-ESS ENGINEERING - STORM				06/11/2019	
BC		006-BKF BACKFILL					06/12/2019
BF	13:00	007-BGS BASEMENT GARAGE STOOPS Comments1: MIDWEST, GR					06/17/2019
BC		002-FTG FOOTING	20190321	4262 E MILLBROOK CIR	282		06/03/2019
BC		PM 003-FOU FOUNDATION					06/05/2019
PR		004-PLU PLUMBING - UNDERSLAB					06/14/2019
BC		AM 005-BKF BACKFILL					06/11/2019
BF	13:00	006-BGS BASEMENT GARAGE STOOPS Comments1: MIDWEST, GR					06/17/2019
PR		004-PLU PLUMBING - UNDERSLAB	20190322	2782 GAINS CT	188		06/10/2019
BC		AM 005-BGS BASEMENT GARAGE STOOPS					06/10/2019
PR		004-PLU PLUMBING - UNDERSLAB	20190323	1952 WREN RD	6		06/05/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
PR	_____	005-ESW ENGINEERING - SEWER / WAT					06/06/2019
BF	_____	006-BG BASEMENT AND GARAGE FLOOR Comments1: LATE AM, PRESTWICK UPLAND					06/07/2019
PR	_____	004-PLU PLUMBING - UNDERSLAB	20190324	1921 WREN RD	15		06/05/2019
PR	_____	005-ESW ENGINEERING - SEWER / WAT					06/06/1929
BC	_____	006-BG BASEMENT AND GARAGE FLOOR Comments1: UPLAND, PRESTWICK					06/13/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190327	4433 PLEASANT COURT			06/10/2019
BC	_____	AM 001-PHF POST HOLE - FENCE	20190338	1847 COLUMBINE DR	71	06/11/2019	
PR	_____	PM 001-ESW ENGINEERING - SEWER / WAT	20190340	976 S CARLY CIR	106		06/30/2019
BC	_____	002-FTG FOOTING Comments1: MCCUE					06/06/2019
BF	_____	003-FOU FOUNDATION Comments1: MCCUE, BBW					06/13/2019
BF	_____	PM 004-BKF BACKFILL Comments1: MCCUE, BBW					06/19/2019
BC	14:30	001-FIN FINAL INSPECTION Comments1: ABOVE GROUND POOL	20190345	1201 WILLOW WAY	202		06/07/2019
BC	_____	002-FIN FINAL INSPECTION	20190346	587 W BARBERRY CIR			06/04/2019
BC	_____	003-FIN FINAL INSPECTION	20190347	1533 CORAL DR	171		06/10/2019
PR	_____	PM 001-ESW ENGINEERING - SEWER / WAT	20190351	834 ALEXANDRA LN	37		06/03/2019
BF	_____	AM 002-FTG FOOTING					06/05/2019
BF	_____	003-BKF BACKFILL Comments1: MARKER, HMD					06/14/2019
BC	_____	002-RFR ROUGH FRAMING	20190355	2778 GOLDENROD DR	254		06/13/2019
BC	_____	001-FIN FINAL INSPECTION Comments1: FENCE WK OF 6/24/19	20190356	3153 MATLOCK DR	656		06/24/2019
BC	_____	AM 001-PPS PRE-POUR, SLAB ON GRADE Comments1: PATIO INSPECTION NOTES: PIN PATIO TO FOU Comments2: NDATION USING #4 OR LARGER REBAR AT 30" Comments3: INTERVALS BEFORE POURING	20190359	493 KELLY AVE	109		06/07/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	11:00	001-ROF ROOF UNDERLAYMENT ICE & W	20190361	1008 JOHN ST			06/05/2019
BC	_____	002-FIN FINAL INSPECTION					06/13/2019
BC	_____	001-PHF POST HOLE - FENCE Comments1: LATE AM	20190364	2988 ELLSWORTH DR	348	06/12/2019	
BC	_____	001-PHF POST HOLE - FENCE	20190369	2623 BURR ST	80	06/11/2019	
BF	_____	AM 001-FTG FOOTING	20190371	2858 CRYDER WAY	445		06/05/2019
BF	_____	AM 002-FOU FOUNDATION Comments1: R&J CONC, GRANDE RESERVE					06/12/2019
BF	_____	003-BKF BACKFILL Comments1: R & J CONC, GR					06/17/2019
PR	_____	004-PLU PLUMBING - UNDERSLAB					06/20/2019
BC	_____	001-PHD POST HOLE - DECK	20190376	838 GREENFIELD TURN	152		06/10/2019
BC	_____	002-RFR ROUGH FRAMING					06/12/2019
BC	_____	002-PHD POST HOLE - DECK Comments1: LANDING	20190377	722 KENTSHIRE DR	118		06/04/2019
BC	_____	003-RFR ROUGH FRAMING					06/04/2019
BC	_____	004-FIN FINAL INSPECTION					06/11/2019
BF	_____	AM 001-FTG FOOTING Comments1: GR - RYAN	20190380	2684 PATRIOT CT	221		06/24/2019
BF	_____	AM 002-FOU FOUNDATION Comments1: GR RYAN					06/27/2019
PR	_____	AM 001-WAT WATER	20190381	2792 GAINS CT	186		06/11/2019
BF	_____	AM 002-FTG FOOTING Comments1: MIDWEST					06/13/2019
BF	_____	PM 003-FOU FOUNDATION Comments1: MIDWEST GR					06/19/2019
BF	_____	AM 004-BKF BACKFILL Comments1: GR- RYAN				06/21/2019	
PR	_____	005-PLU PLUMBING - UNDERSLAB				06/28/2019	

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BF		006-REI REINSPECTION Comments1: BACKFILL					06/24/2019
BC		001-FTG FOOTING	20190384	1991 WREN RD	22		06/10/2019
BF		002-FOU FOUNDATION Comments1: UPLAND					06/12/2019
BF		003-BKF BACKFILL Comments1: UPLAND					06/17/2019
PR		004-PLU PLUMBING - UNDERSLAB					06/26/2019
BF		AM 001-FTG FOOTING Comments1: UPLAND, PRESTWICK	20190386	1942 WREN RD	5		06/07/2019
BC		AM 002-FOU FOUNDATION					06/11/2019
BF		003-BKF BACKFILL Comments1: UPLAND					06/17/2019
PR		004-PLU PLUMBING - UNDERSLAB					06/26/2019
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190396	111 N CONOVER CT			06/03/2019
BC		001-PHF POST HOLE - FENCE Comments1: CANCEL DUE TO STORM DAMAGE	20190405	3333 CALEDONIA DR	19	06/10/2019	
BF		001-PPS PRE-POUR, SLAB ON GRADE Comments1: DRIVE, PATIO W/WALK CHUY 630-330-7580	20190406	216 LEISURE ST B	103		06/28/2019
PR		001-PPS PRE-POUR, SLAB ON GRADE	20190418	508 CENTER PKWY	6B		06/06/2019
PR		001-PPS PRE-POUR, SLAB ON GRADE	20190419	508 CENTER PKWY	6B		06/06/2019
BC		001-PPS PRE-POUR, SLAB ON GRADE	20190421	732 KENTSHIRE DR	117		06/03/2019
BC		001-PPS PRE-POUR, SLAB ON GRADE	20190422	2511 EMERALD LN	122		06/26/2019
BC		001-RFR ROUGH FRAMING Comments1: NEED RESUBMIT PLANS, RE SCHEDULE	20190423	1282 SPRING ST	189	06/17/2019	
BC		002-PHD POST HOLE - DECK					06/27/2019
BC		001-FIN FINAL INSPECTION Comments1: ROOF	20190427	4572 HARRISON ST	1116		06/18/2019
BC		AM 001-FTG FOOTING Comments1: DECK	20190429	2435 FAIRFAX WAY	242		06/06/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	_____	002-RFR ROUGH FRAMING					06/18/2019
PR	_____	PM 001-WAT WATER	20190438	2829 SHERIDAN CT	208		06/25/2019
BC	_____	002-FTG FOOTING					06/28/2019
PR	_____	AM 001-WAT WATER	20190439	2801 OWEN CT	178		06/25/2019
BC	_____	002-FTG FOOTING					06/28/2019
BC	_____	001-PPS PRE-POUR, SLAB ON GRADE	20190444	2795 CRANSTON CIR	140		06/17/2019
BC	_____	001-PPS PRE-POUR, SLAB ON GRADE	20190449	2481 CATALPA TR	174		06/13/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190451	4538 MARQUETTE ST			06/06/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190460	2210 NORTHLAND LN	76	06/07/2019	
		Comments1: CANCELLED PER BOB					
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190468	4675 PLYMOUTH AVE	1026		06/14/2019
BC	_____	002-FIN FINAL INSPECTION					06/24/2019
		Comments1: WK OF 6/24					
BF	11:00	001-FTG FOOTING	20190469	824 ALEXANDRA LN	32		06/19/2019
		Comments1: SOPRIS, HMD					
PR	_____	002-UGE UNDERGROUND ELECTRIC					06/27/2019
PR	_____	003-PLU PLUMBING - UNDERSLAB					06/27/2019
BF	_____	004-BKF BACKFILL					06/27/2019
		Comments1: HMD					
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190470	1002 WHITE PLAINS LN	57		06/14/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190471	2464 SUMAC DR	59		06/13/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190472	319 W KENDALL DR			06/13/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190475	2942 OLD GLORY AVE	267		06/10/2019
		Comments1: FINAL					
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190476	682 YELLOWSTONE LN	73		06/13/2019
BC	_____	001-FIN FINAL INSPECTION	20190480	2605 OVERLOOK CT	25		06/25/2019
		Comments1: PATIO DOOR					

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	_____	AM 001-ROF ROOF UNDERLAYMENT ICE & W	20190481	4579 GARDINER AVE	1105		06/10/2019
BC	_____	001-FIN FINAL INSPECTION	20190482	2901 ELLSWORTH DR.			06/28/2019
BC	_____	001-FTG FOOTING	20190483	1405 WOODSAGE AVE	23		06/19/2019
BF	_____	002-FOU FOUNDATION Comments1: RSS, BRIARWOOD					06/26/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190484	1107 S BRIDGE ST			06/21/2019
BC	_____	AM 001-ROF ROOF UNDERLAYMENT ICE & W Comments1: LATE AM	20190490	492 HONEYSUCKLE LANE			06/13/2019
BC	_____	001-PHD POST HOLE - DECK	20190492	794 HAYDEN DR	62		06/27/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190495	428 WINTERBERRY DR.	98		06/14/2019
BF	_____	AM 001-FTG FOOTING Comments1: PRESTWICK - UPLAND	20190497	1555 MONTROSE CT	34		06/24/2019
BF	_____	002-FOU FOUNDATION Comments1: LATE AM, UPLAND					06/27/2019
BF	_____	AM 001-FTG FOOTING Comments1: PRESTWICK - UPLAND	20190498	1961 WREN RD	19		06/21/2019
BF	_____	AM 002-FOU FOUNDATION Comments1: PW- UPLAND					06/25/2019
BF	_____	003-BKF BACKFILL Comments1: UPLAND					06/28/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190503	1203 SUNSET AVE			06/18/2019
BC	_____	002-FIN FINAL INSPECTION					06/24/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190505	2121 IROQUOIS LN	70		06/14/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190509	1000 INDEPENDENCE BLVD	12		06/18/2019
PR	_____	001-ESW ENGINEERING - SEWER / WAT	20190515	836 ALEXANDRA LN	38		06/27/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190516	2512 BOOMER LN			06/13/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190519	1124 WESTERN LN	75		06/20/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190527	377 BERTRAM DR	1112		06/17/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190531	2481 CATALPA TR	174		06/20/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190534	1022 WHITE PLAINS LN	56		06/17/2019
BC	_____	AM 001-ROF ROOF UNDERLAYMENT ICE & W	20190535	1021 WHITE PLAINS LN	50		06/18/2019
BC	_____	AM 001-PPS PRE-POUR, SLAB ON GRADE	20190537	687 PARKSIDE LN	114		06/24/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190538	2398 IROQUOIS LN	34		06/26/2019
		Comments1: 10-11					
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190543	2521 ALAN DALE LN	124		06/25/2019
		Comments1: LATE AM					
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190545	2977 OLD GLORY DR	260		06/20/2019
BC	_____	001-FIN FINAL INSPECTION	20190546	807 STATE ST			06/21/2019
		Comments1: WINDOWS/PATIO DOOR					
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190549	405 W RIDGE ST			06/25/2019
BC	_____	001-FIN FINAL INSPECTION	20190550	105 APPLE TREE COURT A	0		06/24/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190552	668 YELLOWSTONE LANE			06/21/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190555	2255 NORTHLAND LN			06/28/2019
BC	_____	AM 001-ROF ROOF UNDERLAYMENT ICE & W	20190559	442 HONEYSUCKLE LN	157		06/25/2019
BC	_____	PM 001-ROF ROOF UNDERLAYMENT ICE & W	20190560	2421 ALAN DALE LN	133		06/25/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190564	428 FAIRHAVEN DR	58		06/19/2019
BC	_____	PM 001-ROF ROOF UNDERLAYMENT ICE & W	20190568	2835 CRYDER WAY	480		06/26/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190574	873 CANYON TRAIL			06/28/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190577	2469 CATALPA TR.			06/28/2019
BC	_____	001-ROF ROOF UNDERLAYMENT ICE & W	20190584	1032 WHITE PLAINS LN	55		06/20/2019
BC	_____	AM 001-ROF ROOF UNDERLAYMENT ICE & W	20190585	2007 SWITCHGRASS LN			06/25/2019
		Comments1: LATE AM (11AM)					
BC	_____	AM 001-ROF ROOF UNDERLAYMENT ICE & W	20190588	2391 IROQUOIS LN	5	06/26/2019	
		Comments1: 10AM					
BC	_____	AM 001-ROF ROOF UNDERLAYMENT ICE & W	20190589	893 CANYON TR	123		06/26/2019
		Comments1: 10:30-11					

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE
BC		AM 001-ROF ROOF UNDERLAYMENT ICE & W	20190591	884 CANYON TR	106		06/25/2019
		Comments1: 10:30-11					
BC		AM 001-ROF ROOF UNDERLAYMENT ICE & W	20190592	603 WHITE OAK WAY	35		06/21/2019
		Comments1: MID AM					
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190593	2837 CRANSTON CR			06/26/2019
BC		AM 001-ROF ROOF UNDERLAYMENT ICE & W	20190594	2351 SUMAC DR.	22		06/25/2019
		Comments1: MID AM (10AM)					
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190599	407 DOVER COURT NORTH	25		06/26/2019
		Comments1: 11-12					
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190611	1925 CONEFLOWER CT	162		06/26/2019
		Comments1: 1030-1100					
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190612	772 HAYDEN DR	63		06/26/2019
		Comments1: 1230-1200					
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190615	2341 SUMAC DR	17		06/27/2019
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190620	445 WINTERBERRY DR	110		06/27/2019
PR		001-WK SERVICE WALK	20190621	1447 CANNONBALL TR			06/25/2019
BC		PM 001-ROF ROOF UNDERLAYMENT ICE & W	20190623	2178 ALAN DALE LN.			06/27/2019
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190630	2492 CATALPA TRAIL			06/27/2019
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190638	2222 IROQUOIS LANE			06/26/2019
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190650	688 RED TAIL CT	30		06/28/2019
BC	11:30	001-ROF ROOF UNDERLAYMENT ICE & W	20190651	443 TWINLEAF TR	86		06/27/2019
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190665	756 HEARTLAND			06/27/2019
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190674	1104 WESTERN LN			06/26/2019
BC		001-ROF ROOF UNDERLAYMENT ICE & W	20190679	2268 MEADOWVIEW LN	11		06/27/2019

INSPECTIONS SCHEDULED FROM 06/01/2019 TO 06/30/2019

INSPECTOR	TIME	TYPE OF INSPECTION	PERMIT	ADDRESS	LOT	SCHED. DATE	COMP. DATE

PERMIT TYPE SUMMARY:							
		ADD ADDITION				3	
		AGP ABOVE-GROUND POOL				4	
		BDO COMMERCIAL BUILD-OUT				2	
		BIP BUILD INCENTIVE PROGRAM SFD				2	
		BSM BASEMENT REMODEL				3	
		COM COMMERCIAL BUILDING				1	
		CRM COMMERCIAL REMODEL				3	
		DCK DECK				21	
		DRV DRIVEWAY				3	
		FNC FENCE				11	
		GAR GARAGE				1	
		HVC HVAC UNIT/S				1	
		IGP IN-GROUND POOL				1	
		MIS MISCELLANEOUS				1	
		PRG PERGOLA				1	
		PTO PATIO / PAVERS				11	
		REP REPAIR				1	
		ROF ROOFING				63	
		SDW SIDEWALK				1	
		SFA SINGLE-FAMILY ATTACHED				47	
		SFD SINGLE-FAMILY DETACHED				273	
		SHD SHED/ACCESSORY BUILDING				1	
		SOL SOLAR PANELS				1	
		WHR WATER HEATER REPLACEMENT				2	
		WIN WINDOW REPLACEMENT				3	
INSPECTION SUMMARY:							
		BG BASEMENT AND GARAGE FLOOR				3	
		BGS BASEMENT GARAGE STOOPS				13	
		BKF BACKFILL				17	
		BSM BASEMENT FLOOR				4	
		EFL ENGINEERING - FINAL INSPECTION				22	
		ELU ELECTRICAL - UNDERSLAB				1	
		EPW ENGINEERING- PUBLIC WALK				5	
		ESS ENGINEERING - STORM				2	
		ESW ENGINEERING - SEWER / WATER				10	
		FIN FINAL INSPECTION				67	
		FOU FOUNDATION				16	
		FTG FOOTING				19	
		GAR GARAGE FLOOR				3	
		INS INSULATION				15	
		PHD POST HOLE - DECK				5	
		PHF POST HOLE - FENCE				8	
		PLF PLUMBING - FINAL OSR READY				29	
		PLR PLUMBING - ROUGH				14	
		PLU PLUMBING - UNDERSLAB				17	
		PPS PRE-POUR, SLAB ON GRADE				20	
		PWK PRIVATE WALKS				1	



Reviewed By:	
Legal	<input type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input checked="" type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #3

Tracking Number

EDC 2019-61

Agenda Item Summary Memo

Title: Property Maintenance Report for June 2019

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: _____

Council Action Previously Taken:

Date of Action: _____ Action Taken: _____

Item Number: _____

Type of Vote Required: Informational

Council Action Requested: None

Submitted by: Pete Ratos Community Development
Name Department

Agenda Item Notes:



Memorandum

To: Economic Development Committee
From: Pete Ratos, Code Official
CC: Bart Olson, Krysti Barksdale-Noble, Lisa Pickering
Date: July 1, 2019
Subject: June Property Maintenance

Property Maintenance Report June 2019

Adjudication:

12 Property Maintenance Cases heard in June

6/3/2019

N 3941	4512 Marquette St.	Motor Vehicles	Dismissed
N 3943	2772 Hobbs Ct.	Motor Vehicles	Dismissed
N 3944	98 E Schoolhouse Rd.	Unpermitted Signs	Liabe \$250

6/24/2019

N 3945	2366 Sumac Dr.	Motor Vehicles	Dismissed
N 3946	2744 Alan Dale Ln.	Motor Vehicles	Dismissed
N 3947	2374 Titus Dr.	Motor Vehicles	Dismissed
N 3948	2374 Titus Dr.	Motor Vehicles	Dismissed
N 3949	410 E Main St.	Accessory Building	Liabe \$2,625
N 3950	1569 Walsh Dr.	Weeds	Liabe \$500
N 4151	706 Heustis St.	Weeds	Liabe \$500
N 4152	1084 Redwood Dr.	Weeds	Dismissed
N 4153	577 E Kendall Dr.	Exterior Property	Liabe \$500



Case Report

06/01/2019 - 06/30/2019

Case #	Case Date	ADDRESS OF COMPLAINT	TYPE OF VIOLATION	STATUS	VIOLATION LETTER SENT	FOLLOW UP STATUS	CITATION ISSUED	DATE OF HEARING	POSTED	PUBLIC WORKS TO MOW
20190420	6/26/2019	541 Omaha Dr	Grass & Weeds	IN VIOLATION						
20190419	6/28/2019	GREENBRIAR & RT 47	SIGN BLOCKING SIGHTLINE	IN VIOLATION						
20190418	6/24/2019	706 Heustis St	Grass Height	IN VIOLATION					6/24/2019	
20190417	6/24/2019	1569 Walsh Dr	Grass Height	IN VIOLATION					6/24/2019	
20190416	6/27/2019	Fairfax Way	Grass & Weeds	IN VIOLATION						
20190415	6/27/2019	Country Hills Rt 71	Weeds & Grass	IN VIOLATION						
20190414	6/26/2019	442 Honeysuckle Ln	Damage to Parkway	IN VIOLATION	6/27/2019					
20190413	6/26/2019	2765 Cranston Cir	Junk, Trash & Refuse	IN VIOLATION	6/27/2019					
20190412	6/26/2019	372 Sutton Dr	RV Parking	IN VIOLATION	6/26/2019					
20190411	6/26/2019	2967 Ellsworth Dr	Accessory Building Too Close to Main Structure	IN VIOLATION	6/26/2019					
20190410	6/26/2019	4499 Tampa Dr	Grass Height	IN VIOLATION	6/26/2019					
20190409	6/17/2019	711 Greenfield Turn	Lack of Public Sidewalk	CLOSED		COMPLIANT				
20190408	6/19/2019	4512 Marquette St	Vehicle	IN VIOLATION	6/20/2019					
20190406	6/19/2019	112 S Conover Ct	Fence in Disrepair & Grass Height	IN VIOLATION	6/20/2019					
20190405	6/19/2019	120 Strawberry Ln	Vehicle Parking	IN VIOLATION	6/20/2019					
20190404	6/19/2019	2042 Wild Indigo Ln	Grass Height	COMPLIANT		COMPLIANT				
20190403	6/13/2019	613 Greenfield Turn	Damage to Parkway	IN VIOLATION	6/14/2019					
20190402	6/13/2019	2588 Lyman Loop	Grass Clippings in Street	CLOSED	6/14/2019	COMPLIANT				
20190401	6/13/2019	2587 Lyman Loop	Grass Clippings in Street	CLOSED	6/14/2019	COMPLIANT				
20190400	6/13/2019	3284 Pinewood Dr	Grass Height	CLOSED		COMPLIANT			6/13/2019	
20190399	6/12/2019	301 Adams	Grass Height	IN VIOLATION					6/12/2019	
20190398	6/12/2019	102 W Fox St	Wind Feathers Installed without Permit	PENDING	6/12/2019					
20190397	6/12/2019	2923 Grande Trail	Auxillary building too close to main structure	CLOSED	6/12/2019	COMPLIANT				
20190396	6/12/2019	702 S Main St	Grass & Weeds	IN VIOLATION					6/12/2019	
20190395	6/12/2019	112 Blackberry Ct	Vehicle & Junk, Trash & Refuse	IN VIOLATION	6/12/2019					
20190394	6/12/2019	2957 Ellsworth Dr	Grass Height	IN VIOLATION					6/12/2019	
20190393	6/11/2019	1192 Taus Cir	Grass & Weeds	IN VIOLATION					6/11/2019	
20190392	6/11/2019	1142 Kate Dr	Grass & Weeds	IN VIOLATION					6/11/2019	

20190391	6/5/2019	2389 Iroquois Ln	Weeds	CLOSED		COMPLIANT			6/5/2019	
20190390	6/11/2019	302 Olsen St	Grass Height	IN VIOLATION	6/11/2019				6/11/2019	
20190389	6/11/2019	0208100006	NOXIOUS ODOR COMING FROM GREEN ORGANICS	CLOSED						
20190388	6/11/2019	203 Center Pkwy	Vehicle	IN VIOLATION	6/11/2019					
20190387	6/10/2019	904 S Main St	Lack of Permit	CLOSED		COMPLIANT				
20190386	6/10/2019	512 Redbud	Grass Height	CLOSED	6/11/2019	COMPLIANT			6/10/2019	
20190385	6/7/2019	807 Morgan St	Possible fence without a permit	CLOSED		COMPLIANT				
20190384	6/7/2019	1901 S Bridge St	Light Pole	IN VIOLATION	6/10/2019					
20190383	6/7/2019	602 Greenfield Turn	Vehicle	IN VIOLATION	6/10/2019					
20190382	6/7/2019	1824 B Country Hills	Grass Height	CLOSED		COMPLIANT			6/7/2019	
20190381	6/7/2019	851 Greenfield Turn	Vehicle	CLOSED	6/10/2019	COMPLIANT				
20190380	6/7/2019	847 Greenfield Turn	Vehicle	CLOSED	6/10/2019	COMPLIANT				
20190379	6/7/2019	847 Greenfield Turn	Grass Height	CLOSED		COMPLIANT			6/7/2019	
20190378	6/7/2019	906 S Carly Cir	Grass & Weeds	IN VIOLATION					6/7/2019	
20190377	6/7/2019	966 S Carly Cir	Grass & Weeds	CLOSED		COMPLIANT			6/7/2019	
20190376	6/7/2019	947 N Carly Circle	Grass & Weeds	CLOSED		COMPLIANT			6/7/2019	
20190375	6/6/2019	956 & 966 N Carly	Grass & Weeds	IN VIOLATION					6/7/2019	
20190374	6/5/2019	Shadow Wood Dr	Grass & Weeds	IN VIOLATION					6/5/2019	
20190373	6/6/2019	320 Blackberry Ln	Tree Branches/Rubbish	IN VIOLATION	6/6/2019		6/25/2019	7/29/2019		
20190372	6/6/2019	Lyman Loop - Grande Reserve Subdivision	Soil Erosion and Sediment Control Maintenance	IN VIOLATION	6/6/2019					
20190371	6/15/2019	373 Bertram Dr	GRASS & WEEDS	DUPLICATE		COMPLIANT			6/6/2019	
20190370	6/5/2019	TIMBER GLEN SUBDIVISION	Soil Erosion and Sediment Control Maintenance	IN VIOLATION			5/5/2019	7/8/2019		
20190369	6/4/2019	98 E Schoolhouse Road	Sign without Permit	IN VIOLATION	6/5/2019	IN VIOLATION				
20190368	6/5/2019	2477 Wilton Ct	Grass Height	CLOSED		COMPLIANT			6/4/2019	
20190367	6/4/2019	602 Windette Ridge	Grass Height	CLOSED		COMPLIANT			6/4/2019	
20190366	6/4/2019	437 Sutton St	Grass Height	CLOSED		COMPLIANT			6/4/2019	
20190365	6/4/2019	Bldg 2 Lot 3044	Grass & Weeds	CLOSED	6/5/2019	COMPLIANT			6/4/2019	
20190364	6/4/2019	1021 Blackberry	Grass & Weeds	IN VIOLATION					6/4/2019	
20190363	6/4/2019	Behind 261 Barrett	Grass & Weeds	IN VIOLATION						
20190362	6/4/2019	Greenbriar Retention	Grass & Weeds	COMPLIANT		COMPLIANT				
20190361	6/4/2019	Prestwick	Grass Height	PENDING						
20190360	6/3/2019	307 W Ridge St	Grass Height	CLOSED	6/4/2019	COMPLIANT				
20190359	6/4/2019	2947 Ellsworth	Junk, Trash & Refuse	CLOSED		COMPLIANT				
20190358	6/4/2019	8591 Route 126	Open Burning	CLOSED	6/4/2019	COMPLIANT				
20190357	6/4/2019	373 Bertram Dr	Grass Height	CLOSED		COMPLIANT			6/6/2019	
20190356	6/3/2019	1084 Redwood Dr	Grass Height	CLOSED		COMPLIANT	5/23/2019	6/24/2019	5/15/2019	YES
20190355	6/3/2019	307 W Ridge St	Vehicle	IN VIOLATION	6/4/2019					
20190354	6/3/2019	407 Adams St	Grass & Weeds	PENDING		COMPLIANT	6/11/2019	7/15/2019	5/31/2019	YES

--	--	--	--	--	--	--	--	--	--	--

Total Records: 66

7/1/2019



Reviewed By:	
Legal	<input type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #4

Tracking Number

EDC 2019-62

Agenda Item Summary Memo

Title: Economic Development Report for July 2019

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: See attached.

Council Action Previously Taken:

Date of Action: N/A Action Taken: _____

Item Number: _____

Type of Vote Required: _____

Council Action Requested: _____

Submitted by: Bart Olson Administration
Name Department

Agenda Item Notes:



651 Prairie Pointe Drive, Suite 102 • Yorkville, Illinois 60560
Phone 630-553-0843 • FAX 630-553-0889

Monthly Report – for August 2019 EDC Meeting of the United City of Yorkville

July 2019 Activity

Downtown Redevelopment:

- Working with tenants who are interested in the commercial space of the Old Historic Jail. Also working with Peter McKnight and his business partner Cary Coles.
- Working with a new restaurant on the former Cobblestone addition (last known as 1836 Provisions and Drink Lab). The name of the restaurant is “**Parma Pizza Bar**”. www.parmapizzabar.com The menu will include Italian specialties and Neapolitan style pizza. It is hopeful that the new restaurant will be open this fall. The entrance to the restaurant will be in the rear of the building.
- Working with owner of the Casa Santiago building and perspective tenant(s).

Development south of Fox River:

- Continue working with Eleno Silva on banquet center. Eleno is completing both exterior and interior construction in Stagecoach Crossing. Official name of the business is “**Martini Banquets**”. He is focusing on opening very soon. The first official wedding is scheduled for Saturday, September 7th.
- “**Roadhouse Rte 47**”, at Fountain Village, is officially opening on Friday, August 2nd.
- Continue working with a business who is interested in the NE corner of Route 47 and 71 to do a major ground up project.
- Working with the “**School of Expressive Arts & Learning (SEAL)**” www.seal-il.com . Karen Larsen, currently owns and operates 3 other locations in Romeoville, Lombard & Woodstock. The location that she has selected in Yorkville is 109 Beaver (former Meadowvale facility) in the Fox Industrial Park. Currently 18 students from Yorkville School District 115 travel to the Romeoville location. Yorkville School District is very supportive of this project, and has provided a letter of support to the City. This school works with students from Kindergarten through age 22. The school focus on life skills, and is also vocational. They work with the local business community, as well. When fully operational, there be approximately 80 students at the Yorkville location, and this facility will create 40 new jobs. They have applied for a Special Use, for the location. There will be significant renovations made to the building, after special use process is complete, and they close on the property.

Development north of the Fox River:

- **Kendall Crossing**...Construction is in full swing for the “**Hacienda Real**” building (opening December 2019), the “**Flight Tasting Room & Bottle Shoppe**” (opening September 2019) building, and of course the “**Holiday Inn Express & Suites**” and “**Kendall Banquets**”. “**Burnt Barrel Social**” (opening Fall 2019) with 2,300 square feet and “**Chicago Title**” (opening September 2019) with 1,600 square feet will complete the remainder of the multi-tenant building. “**Burnt Barrel Social**” is a new local gathering place with great food, and small batch whiskey and will be owned and operated by Yorkville resident, Matt Strong.
- **Kendall Marketplace**...Continue to work perspective inline tenants, tenants for a future multi-tenant out lot building, and a national restaurant for new construction on an out lot and with Alex’s broker, Jason Pesola.
- “**Arby’s**” remodel has begun. It is hopeful that the building renovation will be complete in late September. Then the owner, Yonas Hagos, has to begin training. Opening is anticipated in October 2019.
- “**Gas N Wash**” has a new site in Yorkville. They have submitted plans and are officially beginning their approval process. This is for a 5 plus acre parcel under contract at northeast corner of Route 47 and Waterpark Way. They would like to begin construction this fall, and be open in the spring of 2020.
- Working with a variety of retail and service-based businesses that are exploring opportunities in Yorkville.

Industrial Development:

- Continue to work with “**Morton Buildings**” as they begin building their construction center in Yorkville Business Center.

Recreation:

- **Go for it Sports**...continue working with the center. “Go For It Sports” continues to develop exciting new programs such as “Summer Sports Camps”, “Indoor Adult Flag Football League”, and “Private Birthday Parties”.

Other Activity:

- Personally, met with 47 existing Yorkville businesses in July.
- Attended the quarterly SBA meeting in Chicago, to learn about programs available to assist our local business community.

Respectfully submitted,

A handwritten signature in cursive script that reads "Lynn Dubajic".

Lynn Dubajic
651 Prairie Pointe Drive, Suite 102
Yorkville, IL 60560
lynn@dlkllc.com
630-209-7151 cell



Reviewed By:	
Legal	<input checked="" type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input checked="" type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #5

Tracking Number

EDC 2019-63

Agenda Item Summary Memo

Title: Lenny's Gas N Wash (Special Use, Sign Variance, Final Plat)

Meeting and Date: Economic Development Committee - August 6, 2019

Synopsis: Details proposed Gas N Wash Special Use, Sign Variance, and Final Plat requests

Council Action Previously Taken:

Date of Action: _____ Action Taken: _____

Item Number: _____

Type of Vote Required: _____

Council Action Requested: _____

Submitted by: Jason Engberg Community Development
Name Department

Agenda Item Notes:

See attached memo.



Memorandum

To: Economic Development Committee
 From: Jason Engberg, Senior Planner
 Date: July 23, 2019
 Subject: **PZC 2019-19 Lenny’s Gas N Wash – Waterpark Way**
 Proposed New Fuel Station & Convenience Store
 (Special Use, Sign Variance & Final Plat)

SUMMARY:

The petitioner, Leonard McEnery, on behalf of Lenny’s Gas N Wash, LLC, has filed an application requesting special use permit and final plat approval pursuant to Section 10-6-0 of the Yorkville City Code for a gasoline service station with accessory convenience store and ancillary car wash. The petitioner is also seeking sign variance approval to increase the maximum sign area for a monument sign in a business district from 64 square feet to 107 square feet. The property is zoned B-3 General Business District and is located at the northeast corner of the State Route 47 (Bridge Street) and Waterpark Way intersection. This property will be a subdivision of the larger, approximately 72-acre, O’Keefe property which was originally annexed into the City in 2006 via Ordinances 2006-101 and 2006-102.

PROPERTY INFORMATION:

The 5.04-acre property is currently being utilized for agricultural purposes and is currently zoned as B-3 General Business District. The following are the current immediate surrounding zoning and land uses:

	Zoning	Land Use
North	B-3 General Business District	Undeveloped Land
East	B-3 General Business District	Raging Waves/Farmland
South	B-3 General Business District	Raging Waves Parking
West	R-2/R-4 PUD (Westbury East)	Undeveloped Land



PROJECT DESCRIPTION:

As proposed, the Gas N Wash will be a full-service gas station for personal vehicles and large semi-trucks. The project will also include an automated car wash and convenience store for its patrons. Two potential fast food establishment will be attached to the convenience store and both will be accessible internally and one will be accessible externally via a drive-through lane.

SITE PLAN:

The proposed site plan was reviewed by various City departments and outside agencies to ensure compliance with applicable ordinances, regulations and standards with regard to building setbacks, parking/parking setbacks, circulation/access roads, stormwater management/utilities, landscaping and signage. Below are the summaries of those reviews.

Bulk Regulations

The petitioner has depicted the following property’s building setback locations:

Building Setback	Required Minimum	Proposed Setback
Front Yard	20 feet	95.5 feet (Car Wash)
Interior Side Yard	50 feet	128.8 feet (Car Wash)
Corner Side Yard	30 feet	82 feet (C-Store)
Rear Yard	20 feet	145 feet (C-Store)

Section 10-3-5-B states that “No accessory building or structure shall be closer than ten feet (10’) to any main building or closer to the public way than the principal building on the lot.” Therefore, due to its location on the property, the Car Wash will be deemed as the primary structure since it is located closer to the public way than the C-Store.

The maximum building height in the B-3 District is 80 feet. The overall height of the primary building is about 34 feet at its peak (Car Wash) and the accessory building maximum height is about 21 feet. The maximum lot coverage for the B-3 General Business District including all impervious surfaces is 80 percent. As stated on the site data table, the proposed impervious lot coverage is 67 percent. Therefore, the petitioner meets all bulk regulations according to the submitted materials.

Parking and Loading

The submitted plans show a total of 31 parking spaces including 2 handicap accessible spaces. Additionally, there are 15 parking spots detailed as “vacuum stalls” to accommodate the car wash with another 1 handicap accessible spot. The total minimum required parking spaces needed per the Yorkville Zoning Ordinance is 21 spaces (Section 10-16-3). Therefore, the proposed 31 spaces exceed the required minimum.

Due to the overall gross floor area of the C-Store, the petitioner is not required to locate a loading berth as designated in Section 10-16-9. While it is not required, the petitioner has designated an area away from the main structure for box car deliveries. This space is adequate in size and location to allow for deliveries without interfering with onsite traffic and parking.

The petitioner meets the required parking lot setback of 20 feet from arterial roadways and 10 feet from non-arterial roadways (Section 10-7-1).

Stacking

All drive-in and drive-through facilities shall provide vehicle stacking in accordance with table 10.16.05 of the Yorkville Zoning Ordinance:

Use	Minimum Number of Vehicles
Car Wash, Automatic	5 vehicles per bay at entrance 2 vehicles per bay at exit
Restaurant, Fast Food	3 vehicles behind menu board 3 vehicles behind first window

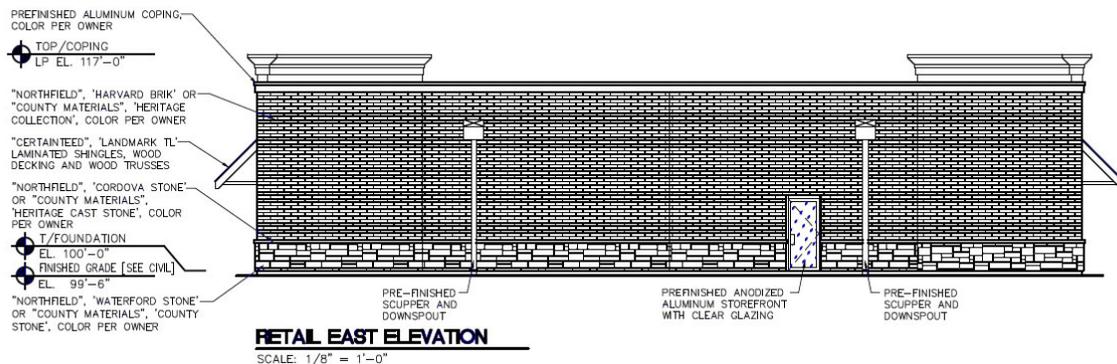
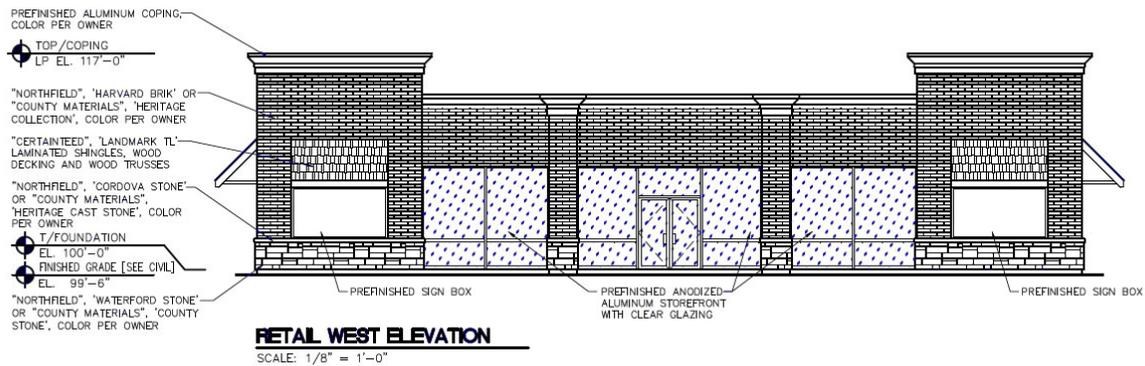
The petitioner meets these requirements by providing 5 vehicle spots at the entrance and 2 vehicle spots at the exit of the car wash. Additionally, the petitioner is providing 7 vehicle spots behind the menu board and 3 vehicle spots before the first window.

Appearance Code

The petitioner has submitted elevations and renderings which illustrate that the project will comply with the City’s Appearance Code (Section 8-15-5):

Criteria for Appearance of the City Code, new non-residential structures shall have at least fifty percent (50%) of the total building constructed of masonry products or precast concrete incorporated as follows:

- i. Front Façade: At least 50% shall incorporate masonry products or precast concrete.
- ii. Any other facade that abuts a street shall incorporate masonry products or precast concrete.



Signage

The petitioner has submitted a sign package for the entire project and is requesting a sign variance for the size of the monument sign. Per Section 10-20-9-A of the Yorkville Zoning Ordinance, free standing monument signs on lots three (3) acres or larger with more than one street frontage, one freestanding sign sixty-four (64) square feet or less in area and twelve feet (12') or less in height per street frontage with an entrance/exit is allowed.

The petitioner is requesting a single monument sign which is just under 12 feet in height and is 107 square feet in area. The sign height meets the City's standards, but the area of the sign is 40% larger than the City's maximum allowable area. Therefore, the petitioner is requesting a sign variance to increase the maximum allowable sign area.



The most recent gas station developed within Yorkville, Casey's Gas Station, also requested a variance for their monument sign which requested a 32% increase in size due to state laws requiring the E-85 price to be shown. This request was recommended for approval by Planning and Zoning Commission and approved by City Council. Staff supports the sign variance request due to the increased right-of-way acquired for Illinois Route 47 (55 feet) and the State regulations requiring the E-85 pricing being shown.

Driveways

Per Section 10-16-3-D of the Yorkville Zoning Ordinance, nonresidential driveways shall be a minimum of two hundred feet (200') from the driveway edge to the nearest intersecting street right of way line. Driveways not meeting the minimum distances may be approved administratively by the City Administrator with a recommendation to approve made by the City Engineer. If administrative approval is not granted, the petitioner must request a variance for this item. The proposed driveways meet this standard but staff is recommending providing a "Do Not Block Entrance" sign at both southern driveway entrances with pavement striping on Waterpark Way to avoid potential conflicts with vehicles in turning lanes exiting the waterpark and/or gas station.

Lighting

A photometric plan has been provided along with manufacturers cut sheets of the proposed light standards to be installed within the parking lot area. Maximum illumination at the property line shall not exceed 0.1 foot-candle and no glare shall spill onto adjacent properties or rights of way. Section 10-16-3-D-7 of the Zoning Code requires the average foot candle to be between 2.0 and 2.5. Proposed average is 2.31. However, the maximum to minimum light intensity ratio should be no more than 20:1. The petitioner's submission meets these standards except for the light intensity ration which is a proposed 22.1:1. The petitioner is aware of this requirement and is revising the lighting plan.

Truck Turning Template

A truck turning template demonstrating the maneuverability of standard sized semitrucks within the site layout has been provided. It appears that there are a few spots where a truck may breach the curbs. Specifically, the right-in-right-out access point off Route 47 shows interference on both northbound ingress and egress. The petitioner will need to address this issue by enlarging the lanes or by making the island mountable to avoid damage to the curbs.

Sidewalks

Existing sidewalks are located along the southern side of Waterpark Way and the site plans show a sidewalk on the north side of Waterpark Way from Route 47 to the eastern edge of the property.

Stormwater Detention Area

A backup/dormant Special Service Area will be required per the Stormwater Ordinance to ensure future maintenance of the detention facility.

COMPREHENSIVE PLAN:

The 2016 Comprehensive Plan designates this for neighborhood retail. This land use is designated for small to medium scale auto-oriented commercial uses, such as retail centers and restaurants located near residential areas. Additionally, this land use should have landscaping treatments between front parking lots and right-of-ways as well as containing high quality signage which is scaled appropriately. Therefore, the proposed use is consistent with the designated future land use plan.

ENGINEERING REVIEW:

The City Engineer has reviewed the plans and continues to work with the petitioner in updating the plans as comments are given. All requests made will be required as part of the special use request.

STAFF COMMENTS:

Staff is seeking input and comments from the Economic Development Committee. The proposed special use, sign variance and final plat is scheduled for a public hearing on August 14, 2019 before the Planning and Zoning Commission. A recommendation will be forwarded to the City Council for consideration at the September 10, 2019 regularly scheduled meeting if all requested materials are submitted and comments from the public and commissions are addressed. Staff will be available to answer any question the Economic Development Committee may have at Tuesday night's meeting.

ATTACHMENTS:

1. Petitioner Applications
2. ALTA Survey
3. Gas N Wash Final Plat
4. Gas N Wash Preliminary Site Plans (7-19-19 submission)
5. Gas N Wash Landscape Plan (7-19-19 submission)
6. Gas N Wash Elevations (7-19-19 submission)
7. Gas N Wash Photometric Plan (6-21-19 submission)

8. Gas N Wash Renderings (7-19-19 submission)
9. Gas N Wash Sign Package (7-19-19 submission)



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SPECIAL USE

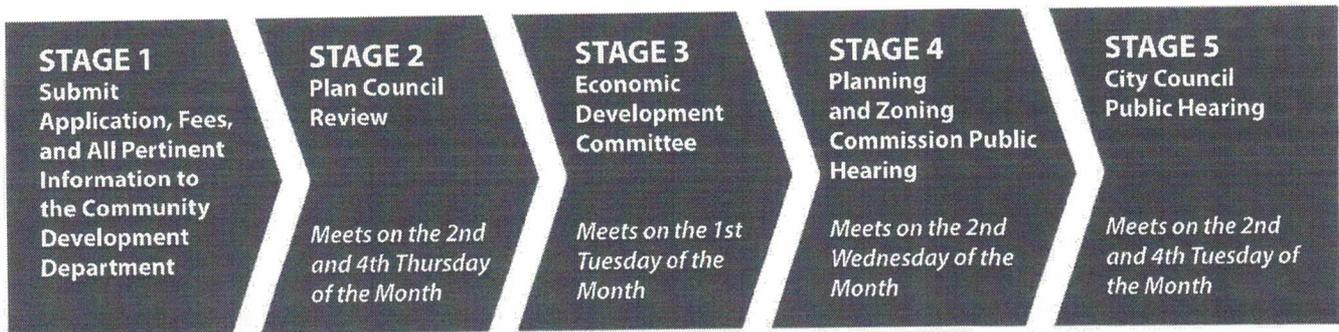
INTENT AND PURPOSE:

The purpose of the zoning code is based upon the authority of the city to divide its land into districts by use, bulk, location of building and structures, in a substantially uniform manner. It is recognized that while some uses are permitted under the zoning code to keep uniformity, a case-by-case analysis must be conducted for certain permitted uses to discover the impact of those uses on neighboring land and of the public need for the particular use at the particular location. In these cases a special use must be granted.

This packet explains the process to successfully submit and complete an Application for Special Use. It includes a detailed description of the process and the actual application itself. Please type the required information in the application on your computer. The application will need to be printed and signed by the petitioner. The only item that needs to be submitted to the city from this packet is the application. The rest of the packet is to help guide you through the process unto completion.

For a complete explanation of what is legally required throughout the Special Use process, please refer to "Title 10, Chapter 4, Section 9 Special Uses" of the Yorkville, Illinois City Code.

APPLICATION PROCEDURE:



STAGE 1: APPLICATION SUBMITTAL

The following must be submitted to the Community Development Department:

- One (1) original signed application with legal description.
- Three (3) 11" x 17" copies each of the exhibits, proposed drawings, location map, and site plan. Large items must be folded to fit in a 10" x 13" envelope.
- Appropriate filing fee.
- One (1) CD or portable USB drive containing one (1) electronic copy (pdf) of each of the following: signed application (complete with exhibits), proposed drawings, location map, and site plan. A Microsoft Word document with the legal description is also required.

Within one (1) week of submittal, the Community Development Department will determine if the application is complete or if additional information is needed. These materials must be submitted a minimum of forty five (45) days prior to the targeted Planning and Zoning Commission meeting. An incomplete submittal could delay the scheduling of the project.

Petitioner will be responsible for payment of recording fees and public hearing costs, including written transcripts of the public hearing and outside consultant costs (i.e. legal review, land planner, zoning coordinator, environmental, etc.). The petitioner will be required to establish a deposit account with the city to cover these fees. The Petitioner Deposit Account/Acknowledgment of Financial Responsibility form is attached to this document and must be submitted with the application.



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR SPECIAL USE

STAGE 2: PLAN COUNCIL

Petitioner must present the proposed plan to the Plan Council. The Plan Council meets on the 2nd and 4th Thursday of the month. The members of the Council include the Community Development Director, City Engineer, the Building Department Official, the Public Works Director, the Director of Parks and Recreation, a Fire Department Representative, and a Police Department Representative. Upon recommendation by the Plan Council, petitioner will move forward to the Planning and Zoning Commission hearing.

STAGE 3: ECONOMIC DEVELOPMENT COMMITTEE

Applicant must present the proposed plan to the Economic Development Committee. Economic Development Committee meets at 7:00 p.m. on the 1st Tuesday of each month. This session is to discuss and consider recommendations prior to full City Council considerations and provide informal feedback. The Economic Development Committee will submit its recommendation to City Council.

STAGE 4: PLANNING AND ZONING COMMISSION

Petitioner will attend a public hearing conducted by the Planning and Zoning Commission. The Planning and Zoning Commission meets on the 2nd Wednesday of the Month at 7:00pm. Notice will be placed in the Kendall County Record by the United City of Yorkville. The petitioner is responsible for sending certified public hearing notices to adjacent property owners within five hundred (500) feet of the subject property no less than fifteen (15) days and no more than thirty (30) days prior to the public hearing date. Twenty four (24) hours prior to the public hearing, a certified affidavit must be filed by the petitioner with the Community Development Department containing the names, addresses and permanent parcel numbers of all parties that were notified. The Planning and Zoning Commission will conduct a public hearing on the request, take public comments, discuss the request, and make a recommendation to City Council. No special use shall be recommended by the Planning and Zoning Commission unless it follows the standards set forth in City's Zoning Ordinance. The Certified Mailing Affidavit form is attached to this document and must be submitted prior to the scheduled Planning and Zoning Commission meeting.

STAGE 5: CITY COUNCIL

Petitioner will attend the City Council meeting where the recommendation of the special use will be considered. The City Council meets on the 2nd and 4th Tuesdays of the month at 7:00pm. City Council will make the final approval of the special use.



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR SPECIAL USE

DORMANT APPLICATIONS

The Community Development Director shall determine if an application meets or fails to meet the requirements stated above. If the Director determines that the application is incomplete it will become dormant under these circumstances:

- The applicant has been notified of such deficiencies and has not responded or provided a time line for completing the application within ninety (90) days from the time of notification.
- The applicant has not responded in writing to a request for information or documentation from the initial plan commission review within six (6) months from the date of that request.
- The applicant has not responded to a request for legal or engineering deposit replenishment for city incurred costs and fees within ninety (90) days from the date of the request.

If the Community Development Director has sent the required notice and the applicant has not withdrawn their application or brought it into compliance, then the director shall terminate the application. After termination, the application shall not be reconsidered except after the filing of a completely new application.

Withdrawal or termination of an application shall not affect the applicant's responsibility for payment of any costs and fees, or any other outstanding debt owed to the city. The balance of any funds deposited with the city that is not needed to pay for costs and fees shall be returned to the applicant. (Ord. 2011-34, 7-26-2011)



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SPECIAL USE

INVOICE & WORKSHEET PETITION APPLICATION			
CONCEPT PLAN REVIEW	<input type="checkbox"/> Engineering Plan Review deposit	\$500.00	Total: \$
AMENDMENT	<input type="checkbox"/> Annexation <input type="checkbox"/> Plan <input type="checkbox"/> Plat <input type="checkbox"/> P.U.D.	\$500.00 \$500.00 \$500.00 \$500.00	Total: \$
ANNEXATION	<input type="checkbox"/> \$250.00 + \$10 per acre for each acre over 5 acres		Total: \$
	$\underline{\hspace{2cm}} - 5 = \underline{\hspace{2cm}}$ # of Acres Acres over 5	$\times \$10 = \underline{\hspace{2cm}}$ Amount for Extra Acres	$+ \$250 = \$ \underline{\hspace{2cm}}$ Total Amount
REZONING	<input type="checkbox"/> \$200.00 + \$10 per acre for each acre over 5 acres		Total: \$
<i>If annexing and rezoning, charge only 1 per acre fee; if rezoning to a PUD, charge PUD Development Fee - not Rezoning Fee</i>			
	$\underline{\hspace{2cm}} - 5 = \underline{\hspace{2cm}}$ # of Acres Acres over 5	$\times \$10 = \underline{\hspace{2cm}}$ Amount for Extra Acres	$+ \$200 = \$ \underline{\hspace{2cm}}$ Total Amount
SPECIAL USE	<input checked="" type="checkbox"/> \$250.00 + \$10 per acre for each acre over 5 acres		\$250.00 Total: \$
	$\underline{5.0} - 5 = \underline{\hspace{2cm}}$ # of Acres Acres over 5	$\times \$10 = \underline{\hspace{2cm}}$ Amount for Extra Acres	$+ \$250 = \$ \underline{250.00}$ Total Amount
ZONING VARIANCE	<input type="checkbox"/> \$85.00 + \$500.00 outside consultants deposit		Total: \$
PRELIMINARY PLAN FEE	<input type="checkbox"/> \$500.00		Total: \$
PUD FEE	<input type="checkbox"/> \$500.00		Total: \$
FINAL PLAT FEE	<input type="checkbox"/> \$500.00		Total: \$
ENGINEERING PLAN REVIEW DEPOSIT	<input type="checkbox"/> Less than 1 acre <input checked="" type="checkbox"/> Over 1 acre, less than 10 acres <input type="checkbox"/> Over 10 acres, less than 40 acres <input type="checkbox"/> Over 40 acres, less than 100 acres <input type="checkbox"/> Over 100 acres	\$5,000.00 \$10,000.00 \$15,000.00 \$20,000.00 \$25,000.00	\$10,000.00 Total: \$
OUTSIDE CONSULTANTS DEPOSIT	<i>Legal, land planner, zoning coordinator, environmental services</i> For Annexation, Subdivision, Rezoning, and Special Use:		\$2,500.00 Total: \$
	<input type="checkbox"/> Less than 2 acres <input checked="" type="checkbox"/> Over 2 acres, less than 10 acres <input type="checkbox"/> Over 10 acres	\$1,000.00 \$2,500.00 \$5,000.00	
TOTAL AMOUNT DUE:			\$12,750.00



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SPECIAL USE

DATE:	PZC NUMBER:	DEVELOPMENT NAME:
PETITIONER INFORMATION		
NAME: LEONARD MCENERY, MANAGER		COMPANY: LENNY'S GAS N WASH YORKVILLE, LLC
MAILING ADDRESS: 8200 W. 185TH ST., UNIT K		
CITY, STATE, ZIP: TINLEY PARK, IL 60487		TELEPHONE: 708-444-0117 CELL: 708-805-2525
EMAIL: LENMCENERY@AOL.COM		FAX: 708-444-0159
PROPERTY INFORMATION		
NAME OF HOLDER OF LEGAL TITLE: BETTY O'KEEFE FAMILY LIMITED PARTNERSHIP C/O BETTY O'KEEFE PROPERTIES, LLC		
IF LEGAL TITLE IS HELD BY A LAND TRUST, LIST THE NAMES OF ALL HOLDERS OF ANY BENEFICIAL INTEREST THEREIN:		
PROPERTY STREET ADDRESS:		
DESCRIPTION OF PROPERTY'S PHYSICAL LOCATION: NORTHEAST CORNER RTE. 47 AND WATERPARK WAY		
CURRENT ZONING CLASSIFICATION: B-3 GENERAL BUSINESS DISTRICT		COMPREHENSIVE PLAN FUTURE LAND USE DESIGNATION: COMMERCIAL
REQUESTED SPECIAL USE: GASOLINE SERVICE STATION. NO OVERNIGHT TRUCK PARKING WILL BE PERMITTED.		
ZONING AND LAND USE OF SURROUNDING PROPERTIES		
NORTH: B-3 VACANT AG USE		
EAST: B-3 VACANT AG USE		
SOUTH: B-3 VACANT AG USE		
WEST: R-4 VACANT AG USE		
KENDALL COUNTY PARCEL IDENTIFICATION NUMBER(S)		
PART OF O2-09-100-015		



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR SPECIAL USE

ATTORNEY INFORMATION

NAME: LYMAN C. TIEMAN

COMPANY: LAW OFFICE OF LYMAN C. TIEMAN

MAILING ADDRESS: 12417 TAHOE LANE

CITY, STATE, ZIP: MOKENA, IL 60448

TELEPHONE: 815-370-3383

EMAIL: LCTIEMAN@YAHOO.COM

FAX: 815-717-6352

ENGINEER INFORMATION

NAME: BRIAN HERTZ

COMPANY: M. GINGERICH GEREAX & ASSOC. MG2A

MAILING ADDRESS: 25620 S GOUGAR RD.

CITY, STATE, ZIP: MANHATTAN, IL 60442

TELEPHONE: 815-478-9680

EMAIL: BHERTZ@MG2A.COM

FAX: 815-478-9685

LAND PLANNER/SURVEYOR INFORMATION

NAME: SCOTT PRITCHETT

COMPANY: ARSA SCHNEIDER ARCHITECTS

MAILING ADDRESS: 1411 W PETERSON AVE., STE 203

CITY, STATE, ZIP: PARK RIDGE, IL 60068

TELEPHONE: 847-698-4438

EMAIL: ARSA@ENTERACT.COM

FAX: 847-698-9889

ATTACHMENTS

Petitioner must attach a legal description of the property to this application and title it as "Exhibit A".

Petitioner must list the names and addresses of any adjoining or contiguous landowners within five hundred (500) feet of the property that are entitled notice of application under any applicable City Ordinance or State Statute. Attach a separate list to this application and title it as "Exhibit B".



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR SPECIAL USE

SPECIAL USE STANDARDS

PLEASE STATE HOW THE ESTABLISHMENT, MAINTENANCE OR OPERATION OF THE SPECIAL USE WILL NOT BE UNREASONABLY DETRIMENTAL TO OR ENDANGER THE PUBLIC HEALTH, SAFETY, MORALS, COMFORT OR GENERAL WELFARE:

APPLICANT PROPOSES THE DEVELOPMENT OF A GASOLINE SERVICE STATION WITH CONVENIENCE STORE AND ANCILLARY CAR WASH. THE SUBJECT PROPERTY IS CURRENTLY ZONED B-3 AND APPLICANT'S USE IS A DESIGNATED SPECIAL USE IN THE B-3 ZONING DISTRICT. THE SUBJECT PROPERTY IS BORDERED ON THE NORTH, EAST AND SOUTH BY EXISTING B-3 ZONED PROPERTY. THE PROPERTY IS BORDERED ON THE WEST SIDE BY R-4 ZONED PROPERTY. ALL SURROUNDING PROPERTIES ARE CURRENTLY BEING FARMED. THE PROPERTY WILL BE DEVELOPED IN SUCH A FASHION AS TO MINIMIZE THE IMPACT ON NEIGHBORING PROPERTIES WITH RESPECT TO NOISE, LIGHT AND TRAFFIC FLOW AND WILL BE IN FULL CONFORMITY WITH EXISTING STATE AND CITY LAWS, ORDINANCES AND DESIGN STANDARDS. NO VARIATIONS FROM THE ZONING ORDINANCE ARE ANTICIPATED. NO OVERNIGHT TRUCK PARKING WILL BE PERMITTED ON THE PROPERTY.

PLEASE STATE HOW THE SPECIAL USE WILL NOT BE INJURIOUS TO THE USE AND ENJOYMENT OF OTHER PROPERTY IN THE IMMEDIATE VICINITY FOR THE PURPOSE ALREADY PERMITTED, NOR SUBSTANTIALLY DIMINISH AND IMPAIR PROPERTY VALUES WITHIN THE NEIGHBORHOOD:

SUBJECT PROPERTY WILL BE DEVELOPED WITH DIRECT INGRESS AND EGRESS FROM AND TO WATERPARK WAY AND ROUTE 47 WITH LITTLE OR NO IMPACT UPON NEIGHBORING PROPERTIES. APPLICANT IS MAKING A SUBSTANTIAL MONETARY INVESTMENT IN THIS PROJECT AND ALL FACILITIES WILL BE DESIGNED AND CONSTRUCTED TO BE COMPATIBLE WITH THE EXISTING AND PROPOSED USES OF SURROUNDING PROPERTIES.

PLEASE STATE HOW THE ESTABLISHMENT OF THE SPECIAL USE WILL NOT IMPEDE THE NORMAL AND ORDERLY DEVELOPMENT AND IMPROVEMENT OF SURROUNDING PROPERTY FOR USES PERMITTED IN THE DISTRICT:

ALL SURROUNDING PROPERTIES ARE CURRENTLY VACANT AND ARE BEING FARMED. PROPERTIES TO THE NORTH, SOUTH AND EAST ARE ZONED B-3 (SAME AS SUBJECT PROPERTY). PROPERTY ON THE WEST SIDE OF RTE. 47 IS ZONED R-4 MULTIFAMILY. BECAUSE OF THE PROPOSED INGRESS AND EGRESS TO WATERPARK WAY AND RTE. 47, THE DEVELOPMENT OF THE SUBJECT PROPERTY WILL HAVE NO DIRECT IMPACT ON SURROUNDING PROPERTIES.

PLEASE STATE HOW ADEQUATE UTILITIES, ACCESS ROADS, DRAINAGE OR OTHER NECESSARY FACILITIES HAVE BEEN OR ARE BEING PROVIDED:
ACCESS IS PROPOSED IN ACCORDANCE WITH THE ATTACHED SITE PLAN FROM ROUTE 47 AND WATERPARK WAY. UTILITIES ARE CURRENTLY AVAILABLE IN CLOSE PROXIMITY TO THE SITE.



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SPECIAL USE

SPECIAL USE STANDARDS

PLEASE STATE HOW ADEQUATE MEASURES HAVE BEEN OR WILL BE TAKEN TO PROVIDE INGRESS OR EGRESS SO DESIGNED AS TO MINIMIZE TRAFFIC CONGESTION IN THE PUBLIC STREETS:

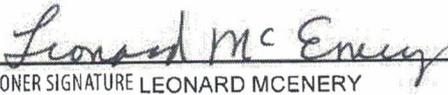
A RIGHT-IN RIGHT-OUT ACCESS WILL BE PROVIDED TO IDOT STANDARDS FOR ACCESS TO ROUTE 47. TWO FULL ACCESS POINTS WILL BE PROVIDED TO WATERPARK WAY ALL IN ACCORDANCE WITH STATE AND CITY DESIGN STANDARDS AS PROPOSED ON THE ATTACHED SITE PLAN.

PLEASE STATE HOW THE SPECIAL USE SHALL IN ALL OTHER RESPECTS CONFORM TO THE APPLICABLE REGULATIONS OF THE DISTRICT IN WHICH IT IS LOCATED, EXCEPT AS SUCH REGULATIONS MAY IN EACH INSTANCE BE MODIFIED BY THE CITY COUNCIL PURSUANT TO THE RECOMMENDATIONS OF THE PLANNING AND ZONING COMMISSION:
 A GASOLINE SERVICE STATION IS DESIGNATED AS A SPECIAL USE IN THE B-3 ZONING DISTRICT. NO VARIANCES TO THE ZONING ORDINANCE ARE BEING REQUESTED BY THE APPLICANT.

AGREEMENT

I VERIFY THAT ALL THE INFORMATION IN THIS APPLICATION IS TRUE TO THE BEST OF MY KNOWLEDGE. I UNDERSTAND AND ACCEPT ALL REQUIREMENTS AND FEES AS OUTLINED AS WELL AS ANY INCURRED ADMINISTRATIVE AND PLANNING CONSULTANT FEES WHICH MUST BE CURRENT BEFORE THIS PROJECT CAN PROCEED TO THE NEXT SCHEDULED COMMITTEE MEETING.

I UNDERSTAND ALL OF THE INFORMATION PRESENTED IN THIS DOCUMENT AND UNDERSTAND THAT IF AN APPLICATION BECOMES DORMANT IT IS THROUGH MY OWN FAULT AND I MUST THEREFORE FOLLOW THE REQUIREMENTS OUTLINED ABOVE.



 PETITIONER SIGNATURE LEONARD MCENERY

DATE MAY 28, 2019

OWNER HEREBY AUTHORIZES THE PETITIONER TO PURSUE THE APPROPRIATE ENTITLEMENTS ON THE PROPERTY.

OWNER SIGNATURE O'KEEFE FAMILY LIMITED PARTNERSHIP, BY
 BETTY O'KEEFE PROPERTIES, LLC, BY
 KIM W O'KEEFE, SOLE MEMBER

DATE

**THIS APPLICATION MUST BE
NOTARIZED PLEASE NOTARIZE HERE:**



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SPECIAL USE

SPECIAL USE STANDARDS

PLEASE STATE HOW ADEQUATE MEASURES HAVE BEEN OR WILL BE TAKEN TO PROVIDE INGRESS OR EGRESS SO DESIGNED AS TO MINIMIZE TRAFFIC CONGESTION IN THE PUBLIC STREETS:

A RIGHT-IN RIGHT-OUT ACCESS WILL BE PROVIDED TO IDOT STANDARDS FOR ACCESS TO ROUTE 47. TWO FULL ACCESS POINTS WILL BE PROVIDED TO WATERPARK WAY ALL IN ACCORDANCE WITH STATE AND CITY DESIGN STANDARDS AS PROPOSED ON THE ATTACHED SITE PLAN.

PLEASE STATE HOW THE SPECIAL USE SHALL IN ALL OTHER RESPECTS CONFORM TO THE APPLICABLE REGULATIONS OF THE DISTRICT IN WHICH IT IS LOCATED, EXCEPT AS SUCH REGULATIONS MAY IN EACH INSTANCE BE MODIFIED BY THE CITY COUNCIL PURSUANT TO THE RECOMMENDATIONS OF THE PLANNING AND ZONING COMMISSION: A GASOLINE SERVICE STATION IS DESIGNATED AS A SPECIAL USE IN THE B-3 ZONING DISTRICT. NO VARIANCES TO THE ZONING ORDINANCE ARE BEING REQUESTED BY THE APPLICANT.

AGREEMENT

I VERIFY THAT ALL THE INFORMATION IN THIS APPLICATION IS TRUE TO THE BEST OF MY KNOWLEDGE. I UNDERSTAND AND ACCEPT ALL REQUIREMENTS AND FEES AS OUTLINED AS WELL AS ANY INCURRED ADMINISTRATIVE AND PLANNING CONSULTANT FEES WHICH MUST BE CURRENT BEFORE THIS PROJECT CAN PROCEED TO THE NEXT SCHEDULED COMMITTEE MEETING.

I UNDERSTAND ALL OF THE INFORMATION PRESENTED IN THIS DOCUMENT AND UNDERSTAND THAT IF AN APPLICATION BECOMES DORMANT IT IS THROUGH MY OWN FAULT AND I MUST THEREFORE FOLLOW THE REQUIREMENTS OUTLINED ABOVE.

 PETITIONER SIGNATURE LEONARD MCENERY

 DATE MAY 28, 2019

OWNER HEREBY AUTHORIZES THE PETITIONER TO PURSUE THE APPROPRIATE ENTITLEMENTS ON THE PROPERTY.

Kim W O'Keefe

 OWNER SIGNATURE O'KEEFE FAMILY LIMITED PARTNERSHIP, BY
 BETTY O'KEEFE PROPERTIES, LLC, BY
 KIM W O'KEEFE, SOLE MEMBER

 DATE

5/29/19

**THIS APPLICATION MUST BE
NOTARIZED PLEASE NOTARIZE HERE:**



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

PETITIONER DEPOSIT ACCOUNT/ ACKNOWLEDGMENT OF FINANCIAL RESPONSIBILITY

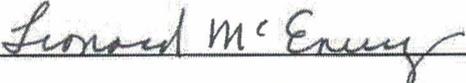
PROJECT NUMBER:	FUND ACCOUNT NUMBER:	PROPERTY ADDRESS:
PETITION/APPROVAL TYPE (check appropriate box(es) of approval requested): <input type="checkbox"/> CONCEPT PLAN REVIEW <input type="checkbox"/> AMENDMENT (TEXT) <input type="checkbox"/> ANNEXATION <input type="checkbox"/> REZONING <input checked="" type="checkbox"/> SPECIAL USE <input type="checkbox"/> MILE AND 1/2 REVIEW <input type="checkbox"/> ZONING VARIANCE <input type="checkbox"/> PRELIMINARY PLAN <input type="checkbox"/> FINAL PLANS <input type="checkbox"/> PLANNED UNIT DEVELOPMENT <input type="checkbox"/> FINAL PLAT		
PETITIONER DEPOSIT ACCOUNT FUND: It is the policy of the United City of Yorkville to require any petitioner seeking approval on a project or entitlement request to establish a Petitioner Deposit Account Fund to cover all actual expenses occurred as a result of processing such applications and requests. Typical requests requiring the establishment of a Petitioner Deposit Account Fund include, but are not limited to, plan review of development approvals/engineering permits. Deposit account funds may also be used to cover costs for services related to legal fees, engineering and other plan reviews, processing of other governmental applications, recording fees and other outside coordination and consulting fees. Each fund account is established with an initial deposit based upon the estimated cost for services provided in the INVOICE & WORKSHEET PETITION APPLICATION . This initial deposit is drawn against to pay for these services related to the project or request. Periodically throughout the project review/approval process, the Financially Responsible Party will receive an invoice reflecting the charges made against the account. At any time the balance of the fund account fall below ten percent (10%) of the original deposit amount, the Financially Responsible Party will receive an invoice requesting additional funds equal to one-hundred percent (100%) of the initial deposit if subsequent reviews/fees related to the project are required. In the event that a deposit account is not immediately replenished, review by the administrative staff, consultants, boards and commissions may be suspended until the account is fully replenished. If additional funds remain in the deposit account at the completion of the project, the city will refund the balance to the Financially Responsible Party. A written request must be submitted by the Financially Responsible Party to the city by the 15th of the month in order for the refund check to be processed and distributed by the 15th of the following month. All refund checks will be made payable to the Financially Responsible Party and mailed to the address provided when the account was established.		
ACKNOWLEDGMENT OF FINANCIAL RESPONSIBILITY		
NAME: LEONARD MCENERY		COMPANY LENNY'S GAS N WASH YORKVILLE, LLC
MAILING ADDRESS: 8200 W. 185TH ST., UNIT K		
CITY, STATE, ZIP: TINLEY PARK, IL 60487		TELEPHONE: 708-444-0117
EMAIL: LENMCENERY@AOL.COM		FAX: 708-444-0159
FINANCIALLY RESPONSIBLE PARTY: I acknowledge and understand that as the Financially Responsible Party, expenses may exceed the estimated initial deposit and, when requested by the United City of Yorkville, I will provide additional funds to maintain the required account balance. Further, the sale or other disposition of the property does not relieve the individual or Company/ Corporation of their obligation to maintain a positive balance in the fund account, unless the United City of Yorkville approves a Change of Responsible Party and transfer of funds. Should the account go into deficit, all City work may stop until the requested replenishment deposit is received.		
PRINT NAME LEONARD MCENERY		TITLE MANAGER
 SIGNATURE		DATE MAY 28, 2019
ACCOUNT CLOSURE AUTHORIZATION		
DATE REQUESTED: _____	<input type="checkbox"/> COMPLETED	<input type="checkbox"/> INACTIVE
PRINT NAME: _____	<input type="checkbox"/> WITHDRAWN	<input type="checkbox"/> COLLECTIONS
SIGNATURE: _____	<input type="checkbox"/> OTHER	
DEPARTMENT ROUTING FOR AUTHORIZATION:	<input type="checkbox"/> COM. DEV.	<input type="checkbox"/> BUILDING <input type="checkbox"/> ENGINEERING <input type="checkbox"/> FINANCE <input type="checkbox"/> ADMIN.

EXHIBIT A
LEGAL DESCRIPTION
LENNY'S GAS N WASH
RTE.47 AND WATERPARK WAY
UNITED CITY OF YORKVILLE, ILLINOIS

THAT PART OF THE WEST HALF OF SECTION 9, TOWNSHIP 37 NORTH, RANGE 7 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN KENDALL COUNTY, ILLINOIS, DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHWEST CORNER OF LOT 3 IN O'KEEFE SUBDIVISION UNIT ONE, RECORDED AS DOCUMENT 200800012915, THENCE NORTH 87 DEGREES 00 MINUTES 59 SECONDS EAST ALONG THE NORTH LINE OF SAID LOT 3 55.01 FEET; THENCE SOUTH 47 DEGREES 30 MINUTES 51 SECONDS EAST ALONG THE NORTH LINE OF SAID LOT 3 70.14 FEET TO THE NORTH RIGHT OF WAY OF ROSEWINKEL STREET; THENCE NORTH 87 DEGREES 00 MINUTES 59 SECONDS EAST ALONG SAID RIGHT OF WAY 108.35 FEET TO A CURVE, CONCAVE TO THE NORTHWEST, HAVING A RADIUS OF 315.00 FEET, A CHORD OF 285.32 FEET AND A CHORD BEARING OF NORTH 60 DEGREES 05 MINUTES 15 SECONDS EAST; THENCE ALONG SAID CURVE AND SAID RIGHT OF WAY 296.10 FEET TO A CURVE, CONCAVE TO THE SOUTHEAST, HAVING A RADIUS OF 385.00 FEET, A CHORD OF 82.31 FEET, AND A CHORD BEARING OF NORTH 39 DEGREES 17 MINUTES 42 SECONDS EAST; THENCE ALONG SAID CURVE AND SAID RIGHT OF WAY 82.47 FEET; THENCE NORTH 02 DEGREES 02 MINUTES 41 SECONDS WEST 324.93 FEET; THENCE SOUTH 87 DEGREES 00 MINUTES 59 SECONDS WEST 520.01 FEET TO THE WEST RIGHT OF WAY OF ROUTE 47; THENCE SOUTH 02 DEGREES 02 MINUTES 41 SECONDS EAST ALONG SAID RIGHT OF WAY 465.06 FEET TO THE POINT OF BEGINNING.



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR PRELIMINARY PLAN & FINAL PLAT

INTENT AND PURPOSE:

The purpose of this application is to allow land to be divided and distributed in a way that conforms to the City of Yorkville's standards and enhances the land's value. Preliminary Plans and Final Plats allow the City Staff and local government to review how land will be developed and if it is ideal for that location within the city. This application is used to submit new preliminary plans and final plats but may also be used to amend either of those documents.

This packet explains the process to successfully submit and complete an Application for a Preliminary Plan and Final Plat. It includes a detailed description of the process and the actual application itself. Please type the required information in the application on your computer. The application will need to be printed and signed by the applicant. The only item that needs to be submitted from this packet is the application. The rest of the packet is to help guide you through the process unto completion.

APPLICATION PROCEDURE:



STAGE 1: APPLICATION SUBMITTAL

The following must be submitted to the Community Development Department:

- One (1) original signed application with legal description.
- Two (2) 11" x 17" copies each of the proposed drawings, location map, and site plan. Large items must be folded to fit in a 10" x 13" envelope.
- Appropriate filing fee.
- One (1) CD or portable USB drive containing an electronic copy (pdf) of each of the signed application (complete with exhibits), proposed drawings, location map, and site plan.
- Subdivision Plats: Three (3) full size copies and one (1) 11" by 17" copy depicting the originally platted lots, the proposed new lots, the proposed modifications and adjustments.

Within one (1) week of submittal, the Community Development Department will determine if the application is complete or if additional information is needed. These materials must be submitted a minimum of forty five (45) days prior to the targeted Planning and Zoning Commission meeting. An incomplete submittal could delay the scheduling of the project.

Applicant is responsible for making submittals to other review agencies such as Kendall County, Illinois Department of Transportation, Illinois Department of Natural Resources, U.S. Army Corps of Engineers, etc., to allow timely review by the City.

Applicant will be responsible for payment of recording fees and public hearing costs, including written transcripts of the public hearing and outside consultant costs (i.e. legal review, land planner, zoning coordinator, environmental, etc.). The applicant will be required to establish a deposit account with the city to cover these fees. The Petitioner Deposit Account/Acknowledgement of Financial Responsibility form is attached to this document and must be submitted with the application.



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR PRELIMINARY PLAN & FINAL PLAT

STAGE 2: PLAN COUNCIL REVIEW

Applicant must present the proposed plan to the Plan Council. The Plan Council meets on the 2nd and 4th Thursday of the month. The members of the Council include the Community Development Director, City Engineer, the Building Department Official, the Public Works Director, the Director of Parks and Recreation, a Fire Department Representative, and a Police Department Representative. Upon recommendation by the Plan Council, applicant will move forward to the Planning and Zoning Commission meeting.

STAGE 3: ECONOMIC DEVELOPMENT COMMITTEE

Applicant must present the proposed plan to the Economic Development Committee. Economic Development Committee meets at 7:00 p.m. on the 1st Tuesday of each month. This session is to discuss and consider recommendations prior to full City Council considerations and provide informal feedback. The Economic Development Committee will submit its recommendation to City Council.

STAGE 4: PLANNING & ZONING COMMISSION PUBLIC HEARING

Applicant will attend a meeting conducted by the Planning & Zoning Commission. The Planning & Zoning Commission meets on the 2nd Wednesday of the Month at 7:00pm. The Planning & Zoning Commission will conduct a meeting on the request, discuss the request, and make a recommendation to City Council.

STAGE 5: CITY COUNCIL PUBLIC HEARING

Applicant must present the proposed subdivision replat to the City Council. The City Council meets the 2nd and 4th Tuesdays of every month at 7:00 p.m. in the Yorkville City Hall Council Chambers. The proposal will be discussed at the City Council hearing where formal voting takes place. City Council will make the final approval of the replatting.

STAGE 6: FINAL PLAT RECORDING

Once the final subdivision plat is approved by the City Council and all required documents, bonds, and letters of credit are submitted to the city, the final plat must be recorded with Kendall County. Submit the final plat mylar to the Deputy Clerk for signatures. When all city signatures are in place, the developer or his surveyor may take the mylar to the Kendall County Clerk for their signature. The next step is to have six (6) paper prints made and return to the Kendall County Recorder's office for recording. Kendall County requires the mylar and four (4) paper copies. The City of Yorkville requires that you submit two (2) recorded paper copies to the Deputy Clerk.



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR PRELIMINARY PLAN & FINAL PLAT

DORMANT APPLICATIONS

The Community Development Director shall determine if an application meets or fails to meet the requirements stated above. If the Director determines that the application is incomplete it will become dormant under these circumstances:

- The applicant has been notified of such deficiencies and has not responded or provided a time line for completing the application within ninety (90) days from the time of notification.
- The applicant has not responded in writing to a request for information or documentation from the initial plan commission review within six (6) months from the date of that request.
- The applicant has not responded to a request for legal or engineering deposit replenishment for city incurred costs and fees within ninety (90) days from the date of the request.

If the Community Development Director has sent the required notice and the applicant has not withdrawn their application or brought it into compliance, then the director shall terminate the application. After termination, the application shall not be reconsidered except after the filing of a completely new application.

Withdrawal or termination of an application shall not affect the applicant's responsibility for payment of any costs and fees, or any other outstanding debt owed to the city. The balance of any funds deposited with the city that is not needed to pay for costs and fees shall be returned to the applicant. (Ord. 2011-34, 7-26-2011)



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR PRELIMINARY PLAN & FINAL PLAT

INVOICE & WORKSHEET PETITION APPLICATION			
CONCEPT PLAN REVIEW	<input type="checkbox"/> Engineering Plan Review deposit	\$500.00	Total: \$
AMENDMENT	<input type="checkbox"/> Annexation \$500.00 <input type="checkbox"/> Plan \$500.00 <input type="checkbox"/> Plat \$500.00 <input type="checkbox"/> P.U.D. \$500.00		Total: \$
ANNEXATION	<input type="checkbox"/> \$250.00 + \$10 per acre for each acre over 5 acres		Total: \$
$\underline{\hspace{2cm}} - 5 = \underline{\hspace{2cm}} \times \$10 = \underline{\hspace{2cm}} + \$250 = \$ \underline{\hspace{2cm}}$ <small># of Acres Acres over 5 Amount for Extra Acres Total Amount</small>			
REZONING	<input type="checkbox"/> \$200.00 + \$10 per acre for each acre over 5 acres		Total: \$
<i>If annexing and rezoning, charge only 1 per acre fee; if rezoning to a PUD, charge PUD Development Fee - not Rezoning Fee</i>			
$\underline{\hspace{2cm}} - 5 = \underline{\hspace{2cm}} \times \$10 = \underline{\hspace{2cm}} + \$200 = \$ \underline{\hspace{2cm}}$ <small># of Acres Acres over 5 Amount for Extra Acres Total Amount</small>			
SPECIAL USE	<input type="checkbox"/> \$250.00 + \$10 per acre for each acre over 5 acres		Total: \$
$\underline{\hspace{2cm}} - 5 = \underline{\hspace{2cm}} \times \$10 = \underline{\hspace{2cm}} + \$250 = \$ \underline{\hspace{2cm}}$ <small># of Acres Acres over 5 Amount for Extra Acres Total Amount</small>			
ZONING VARIANCE	<input type="checkbox"/> \$85.00 + \$500.00 outside consultants deposit		Total: \$
PRELIMINARY PLAN FEE	<input type="checkbox"/> \$500.00		Total: \$
PUD FEE	<input type="checkbox"/> \$500.00		Total: \$
FINAL PLAT FEE	<input checked="" type="checkbox"/> \$500.00		Total: \$
ENGINEERING PLAN REVIEW DEPOSIT	<input type="checkbox"/> Less than 1 acre \$5,000.00 <input type="checkbox"/> Over 1 acre, less than 10 acres \$10,000.00 <input type="checkbox"/> Over 10 acres, less than 40 acres \$15,000.00 <input type="checkbox"/> Over 40 acres, less than 100 acres \$20,000.00 <input type="checkbox"/> Over 100 acres \$25,000.00		Total: \$
OUTSIDE CONSULTANTS DEPOSIT	<i>Legal, land planner, zoning coordinator, environmental services</i> For Annexation, Subdivision, Rezoning, and Special Use:		Total: \$
	<input type="checkbox"/> Less than 2 acres \$1,000.00 <input type="checkbox"/> Over 2 acres, less than 10 acres \$2,500.00 <input type="checkbox"/> Over 10 acres \$5,000.00		
TOTAL AMOUNT DUE:			500



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR PRELIMINARY PLAN & FINAL PLAT

DATE:	PZC NUMBER:	DEVELOPMENT NAME:
PETITIONER INFORMATION		
NAME: LEONARD McENERY, MANAGER		COMPANY: LENNY'S GAS N WASH YORKVILLE, LLC
MAILING ADDRESS: 8200 W. 185th ST., UNIT K		
CITY, STATE, ZIP: TINLEY PARK, IL 60487		TELEPHONE: 708-444-0117
EMAIL: lenmcenery@aol.com		FAX: 708-444-0159
PROPERTY INFORMATION		
NAME OF HOLDER OF LEGAL TITLE: BETTY O'KEEFE FAMILY LIMITED PARTNERSHIP C/O BETTY O'KEEFE PROPERTIES, LLC		
IF LEGAL TITLE IS HELD BY A LAND TRUST, LIST THE NAMES OF ALL HOLDERS OF ANY BENEFICIAL INTEREST THEREIN:		
PROPERTY STREET ADDRESS:		
TYPE OF REQUEST:		
<input type="checkbox"/> PRELIMINARY PLAN <input checked="" type="checkbox"/> FINAL PLAT <input type="checkbox"/> AMENDED PRELIMINARY PLAN <input type="checkbox"/> AMENDED FINAL PLAT		
TOTAL LOT ACREAGE: 5.63		CURRENT ZONING CLASSIFICATION: B-3 GENERAL BUSINESS DISTRICT
ATTACHMENTS		
Petitioner must attach a legal description of the property to this application and title it as "Exhibit A".		



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR PRELIMINARY PLAN & FINAL PLAT

ATTORNEY INFORMATION

NAME: LYMAN C. TIEMAN COMPANY: LAW OFFICE OF LYMAN C. TIEMAN
 MAILING ADDRESS: 12417 TAHOE LANE
 CITY, STATE, ZIP: MOKENA, IL 60448 TELEPHONE: 815-370-3383
 EMAIL: lctieman@yahoo.com FAX: 815-717-6352

ENGINEER INFORMATION & LAND SURVEYOR

NAME: BRIAN HERTZ, P.E. COMPANY: M. GINGERICH, GEREAX & ASSOC. (MG2A)
 MAILING ADDRESS: 25620 S. GOUGAR ROAD
 CITY, STATE, ZIP: MANHATTAN, IL 60442 TELEPHONE: 815-478-9680
 EMAIL: bhertz@mg2a.com FAX: 815-478-9685

LAND PLANNER/SURVEYOR INFORMATION

NAME: SCOTT PRITCHETT COMPANY: ARSA SCHNEIDER ARCHITECTS
 MAILING ADDRESS: 1411 W. PETERSON AVE., SUITE 203
 CITY, STATE, ZIP: PARK RIDGE, IL 60068 TELEPHONE: 847-698-4438
 EMAIL: arsa@enteract.com FAX: 847-698-9889

AGREEMENT

I VERIFY THAT ALL THE INFORMATION IN THIS APPLICATION IS TRUE TO THE BEST OF MY KNOWLEDGE. I UNDERSTAND AND ACCEPT ALL REQUIREMENTS AND FEES AS OUTLINED AS WELL AS ANY INCURRED ADMINISTRATIVE AND PLANNING CONSULTANT FEES WHICH MUST BE CURRENT BEFORE THIS PROJECT CAN PROCEED TO THE NEXT SCHEDULED COMMITTEE MEETING.

I UNDERSTAND ALL OF THE INFORMATION PRESENTED IN THIS DOCUMENT AND UNDERSTAND THAT IF AN APPLICATION BECOMES DORMANT IT IS THROUGH MY OWN FAULT AND I MUST THEREFORE FOLLOW THE REQUIREMENTS OUTLINED ABOVE.

Leonard McEnery June 5, 2019
 PETITIONER SIGNATURE DATE

OWNER HEREBY AUTHORIZES THE PETITIONER TO PURSUE THE APPROPRIATE ENTITLEMENTS ON THE PROPERTY.

K.W. Keefe June 5, 2019
 OWNER SIGNATURE DATE



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

PETITIONER DEPOSIT ACCOUNT/ ACKNOWLEDGMENT OF FINANCIAL RESPONSIBILITY

PROJECT NUMBER:	FUND ACCOUNT NUMBER:	PROPERTY ADDRESS:	
APPLICATION/APPROVAL TYPE (check appropriate box(es) of approval requested): <input type="checkbox"/> CONCEPT PLAN REVIEW <input type="checkbox"/> AMENDMENT (TEXT) <input type="checkbox"/> ANNEXATION <input type="checkbox"/> REZONING <input type="checkbox"/> SPECIAL USE <input type="checkbox"/> MILE AND 1/2 REVIEW <input type="checkbox"/> ZONING VARIANCE <input type="checkbox"/> PRELIMINARY PLAN <input type="checkbox"/> FINAL PLANS <input type="checkbox"/> PLANNED UNIT DEVELOPMENT <input checked="" type="checkbox"/> FINAL PLAT			
PETITIONER DEPOSIT ACCOUNT FUND: It is the policy of the United City of Yorkville to require any petitioner seeking approval on a project or entitlement request to establish a Petitioner Deposit Account Fund to cover all actual expenses occurred as a result of processing such applications and requests. Typical requests requiring the establishment of a Petitioner Deposit Account Fund include, but are not limited to, plan review of development approvals/engineering permits. Deposit account funds may also be used to cover costs for services related to legal fees, engineering and other plan reviews, processing of other governmental applications, recording fees and other outside coordination and consulting fees. Each fund account is established with an initial deposit based upon the estimated cost for services provided in the INVOICE & WORKSHEET PETITION APPLICATION . This initial deposit is drawn against to pay for these services related to the project or request. Periodically throughout the project review/approval process, the Financially Responsible Party will receive an invoice reflecting the charges made against the account. At any time the balance of the fund account fall below ten percent (10%) of the original deposit amount, the Financially Responsible Party will receive an invoice requesting additional funds equal to one-hundred percent (100%) of the initial deposit if subsequent reviews/fees related to the project are required. In the event that a deposit account is not immediately replenished, review by the administrative staff, consultants, boards and commissions may be suspended until the account is fully replenished. If additional funds remain in the deposit account at the completion of the project, the city will refund the balance to the Financially Responsible Party. A written request must be submitted by the Financially Responsible Party to the city by the 15th of the month in order for the refund check to be processed and distributed by the 15th of the following month. All refund checks will be made payable to the Financially Responsible Party and mailed to the address provided when the account was established.			
ACKNOWLEDGMENT OF FINANCIAL RESPONSIBILITY			
NAME: LEONARD McENERY, MANAGER		COMPANY: LENNY'S GAS N WASH YORKVILLE, LLC	
MAILING ADDRESS: 8200 W. 185th ST., UNIT K			
CITY, STATE, ZIP: TINLEY PARK, IL 60487		TELEPHONE: 708-444-0117	
EMAIL: lenmcenery@aol.com		FAX: 708-444-0159	
FINANCIALLY RESPONSIBLE PARTY: I acknowledge and understand that as the Financially Responsible Party, expenses may exceed the estimated initial deposit and, when requested by the United City of Yorkville, I will provide additional funds to maintain the required account balance. Further, the sale or other disposition of the property does not relieve the individual or Company/ Corporation of their obligation to maintain a positive balance in the fund account, unless the United City of Yorkville approves a Change of Responsible Party and transfer of funds. Should the account go into deficit, all City work may stop until the requested replenishment deposit is received.			
LEONARD McENERY		MANAGER	
<small>PRINT NAME</small>		<small>TITLE</small>	
		June 5, 2019	
<small>SIGNATURE</small>		<small>DATE</small>	
ACCOUNT CLOSURE AUTHORIZATION			
DATE REQUESTED: _____		<input type="checkbox"/> COMPLETED	<input type="checkbox"/> INACTIVE
PRINT NAME: _____		<input type="checkbox"/> WITHDRAWN	<input type="checkbox"/> COLLECTIONS
SIGNATURE: _____		<input type="checkbox"/> OTHER	
DEPARTMENT ROUTING FOR AUTHORIZATION:		<input type="checkbox"/> COM. DEV.	<input type="checkbox"/> BUILDING
		<input type="checkbox"/> ENGINEERING	<input type="checkbox"/> FINANCE
		<input type="checkbox"/> ADMIN.	

EXHIBIT A

THAT PART OF THE WEST HALF OF SECTION 9, TOWNSHIP 37 NORTH, RANGE 7 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN KENDALL COUNTY, ILLINOIS, DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHWEST CORNER OF LOT 3 IN O'KEEFE SUBDIVISION UNIT ONE, RECORDED AS DOCUMENT 200800012915, THENCE NORTH 87 DEGREES 00 MINUTES 59 SECONDS EAST ALONG THE NORTH LINE OF SAID LOT 3 55.01 FEET; THENCE SOUTH 47 DEGREES 30 MINUTES 51 SECONDS EAST ALONG THE NORTH LINE OF SAID LOT 3 70.14 FEET TO THE NORTH RIGHT OF WAY OF ROSEWINKEL STREET; THENCE NORTH 87 DEGREES 00 MINUTES 59 SECONDS EAST ALONG SAID RIGHT OF WAY 108.35 FEET TO A CURVE, CONCAVE TO THE NORTHWEST, HAVING A RADIUS OF 315.00 FEET, A CHORD OF 285.32 FEET AND A CHORD BEARING OF NORTH 60 DEGREES 05 MINUTES 15 SECONDS EAST; THENCE ALONG SAID CURVE AND SAID RIGHT OF WAY 296.10 FEET TO A CURVE, CONCAVE TO THE SOUTHEAST, HAVING A RADIUS OF 385.00 FEET, A CHORD OF 82.31 FEET, AND A CHORD BEARING OF NORTH 39 DEGREES 17 MINUTES 42 SECONDS EAST; THENCE ALONG SAID CURVE AND SAID RIGHT OF WAY 82.47 FEET; THENCE NORTH 02 DEGREES 02 MINUTES 41 SECONDS WEST 324.93 FEET; THENCE SOUTH 87 DEGREES 00 MINUTES 59 SECONDS WEST 520.01 FEET TO THE WEST RIGHT OF WAY OF ROUTE 47; THENCE SOUTH 02 DEGREES 02 MINUTES 41 SECONDS EAST ALONG SAID RIGHT OF WAY 465.06 FEET TO THE POINT OF BEGINNING.



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SIGN VARIANCE

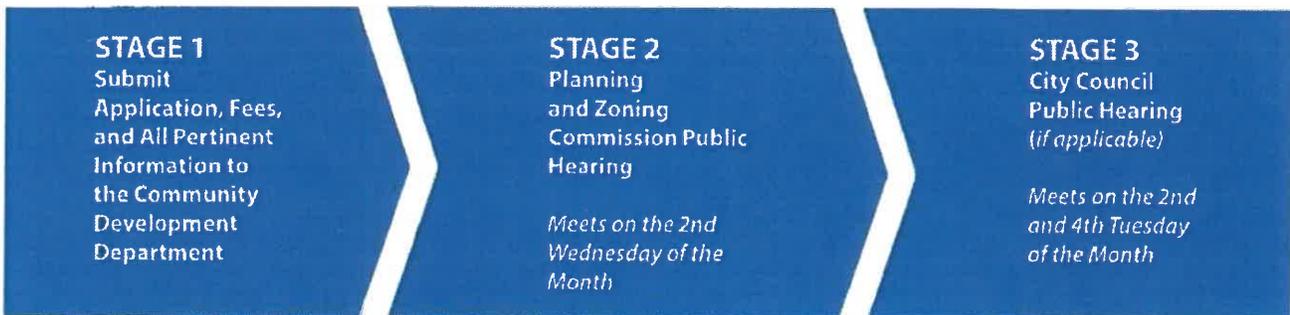
INTENT AND PURPOSE:

The purpose of a variance is to provide relief from certain regulations of the zoning ordinance to permit the use of land in a way that is not otherwise permitted under the ordinance. A variance is granted when the terms of the zoning ordinance, if literally applied, would create an unreasonable hardship on the landowner, making the property virtually useless.

This packet explains the process to successfully submit and complete an Application for a Variance Request. It includes a detailed description of the process and the actual application itself. Please type the required information in the application on your computer. The application will need to be printed and be signed by the petitioner. The only item that needs to be submitted to the city from this packet is the application. The rest of the packet is to help guide you through the process unto completion.

For a complete explanation of what is legally required throughout the Variance Request process, please refer to "Title 10, Chapter 20, Section 13 Sign Variations" of the Yorkville, Illinois City Code.

APPLICATION PROCEDURE:



STAGE 1: APPLICATION SUBMITTAL

The following must be submitted to the Community Development Department:

- One (1) original signed application with legal description.
- Three (3) 11" x 17" copies each of the exhibits, proposed drawings, location map, and site plan. Large items must be folded to fit in a 10" x 13" envelope.
- Appropriate filing fee.
- One (1) CD or portable USB drive containing one (1) electronic copy (pdf) of each of the following: signed application (complete with exhibits), proposed drawings, location map, and site plan. A Microsoft Word document with the legal description is also required.

Within one (1) week of submittal, the Community Development Department will determine if the application is complete or if additional information is needed. These materials must be submitted a minimum of forty five (45) days prior to the targeted Planning and Zoning Commission meeting. An incomplete submittal could delay the scheduling of the project.

Petitioner will be responsible for payment of recording fees and public hearing costs, including written transcripts of the public hearing and outside consultant costs (i.e. legal review, land planner, zoning coordinator, environmental, etc.). The petitioner will be required to establish a deposit account with the city to cover these fees. The Petitioner Deposit Account/Acknowledgment of Financial Responsibility form is attached to this document and must be submitted with the application.



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR SIGN VARIANCE

STAGE 2: PLANNING AND ZONING COMMISSION

Petitioner will attend a public hearing conducted by the Planning and Zoning Commission. The Planning and Zoning Commission meets on the 2nd Wednesday of the Month at 7:00pm. Notice will be placed in the Kendall County Record by the United City of Yorkville. The petitioner is responsible for sending certified public hearing notices to adjacent property owners within five hundred (500) feet of the subject property no less than fifteen (15) days and no more than thirty (30) days prior to the public hearing date. Twenty four (24) hours prior to the public hearing, a certified affidavit must be filed by the petitioner with the Community Development Department containing the names, addresses and permanent parcel numbers of all parties that were notified. The Certified Mailing Affidavit form is attached to this document and must be submitted prior to the scheduled Planning and Zoning Commission meeting.

STAGE 3: CITY COUNCIL

If necessary, the petitioner will attend a City Council public hearing where the Planning and Zoning Commission decision will be discussed and reviewed. The City Council meets on the 2nd and 4th Tuesdays of the month at 7:00pm. Variations may be granted by the City Council, but only after a public hearing for an authorized variation. The concurring vote of two-thirds (2/3) of all members of the City Council shall be necessary to reverse the recommendations of the Planning and Zoning Commission.

DORMANT APPLICATIONS

The Community Development Director shall determine if an application meets or fails to meet the requirements stated above. If the Director determines that the application is incomplete it will become dormant under these circumstances:

- The petitioner has been notified of such deficiencies and has not responded or provided a time line for completing the application within ninety (90) days from the time of notification.
- The petitioner has not responded in writing to a request for information or documentation from the initial plan commission review within six (6) months from the date of that request.
- The petitioner has not responded to a request for legal or engineering deposit replenishment for city incurred costs and fees within ninety (90) days from the date of the request.

If the Community Development Director has sent the required notice and the petitioner has not withdrawn their application or brought it into compliance, then the director shall terminate the application. After termination, the application shall not be reconsidered except after the filing of a completely new application.

Withdrawal or termination of an application shall not affect the petitioner's responsibility for payment of any costs and fees, or any other outstanding debt owed to the city. The balance of any funds deposited with the city that is not needed to pay for costs and fees shall be returned to the petitioner. (Ord. 2011-34, 7-26-2011)



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SIGN VARIANCE

INVOICE & WORKSHEET PETITION APPLICATION			
CONCEPT PLAN REVIEW	<input type="checkbox"/> Engineering Plan Review deposit	\$500.00	Total: \$
AMENDMENT	<input type="checkbox"/> Annexation <input type="checkbox"/> Plan <input type="checkbox"/> Plat <input type="checkbox"/> P.U.D.	\$500.00 \$500.00 \$500.00 \$500.00	Total: \$
ANNEXATION	<input type="checkbox"/> \$250.00 + \$10 per acre for each acre over 5 acres		Total: \$
$\underline{\hspace{2cm}} - 5 = \underline{\hspace{2cm}} \times \$10 = \underline{\hspace{2cm}} + \$250 = \$ \underline{\hspace{2cm}}$ <p style="text-align: center;"># of Acres Acres over 5 Amount for Extra Acres Total Amount</p>			
REZONING	<input type="checkbox"/> \$200.00 + \$10 per acre for each acre over 5 acres		Total: \$
<i>If annexing and rezoning, charge only 1 per acre fee; if rezoning to a PUD, charge PUD Development Fee not Rezoning Fee</i>			
$\underline{\hspace{2cm}} - 5 = \underline{\hspace{2cm}} \times \$10 = \underline{\hspace{2cm}} + \$200 = \$ \underline{\hspace{2cm}}$ <p style="text-align: center;"># of Acres Acres over 5 Amount for Extra Acres Total Amount</p>			
SPECIAL USE	<input type="checkbox"/> \$250.00 + \$10 per acre for each acre over 5 acres		Total: \$
$\underline{\hspace{2cm}} - 5 = \underline{\hspace{2cm}} \times \$10 = \underline{\hspace{2cm}} + \$250 = \$ \underline{\hspace{2cm}}$ <p style="text-align: center;"># of Acres Acres over 5 Amount for Extra Acres Total Amount</p>			
ZONING VARIANCE	<input type="checkbox"/> \$85.00 + \$500.00 outside consultants deposit		Total: \$
PRELIMINARY PLAN FEE	<input type="checkbox"/> \$500.00		Total: \$
PUD FEE	<input type="checkbox"/> \$500.00		Total: \$
FINAL PLAT FEE	<input type="checkbox"/> \$500.00		Total: \$
ENGINEERING PLAN REVIEW DEPOSIT	<input type="checkbox"/> Less than 1 acre <input type="checkbox"/> Over 1 acre, less than 10 acres <input type="checkbox"/> Over 10 acres, less than 40 acres <input type="checkbox"/> Over 40 acres, less than 100 acres <input type="checkbox"/> Over 100 acres	\$5,000.00 \$10,000.00 \$15,000.00 \$20,000.00 \$25,000.00	Total: \$
OUTSIDE CONSULTANTS DEPOSIT	<i>Legal, land planner, zoning coordinator, environmental services</i> For Annexation, Subdivision, Rezoning, and Special Use: <input type="checkbox"/> Less than 2 acres <input type="checkbox"/> Over 2 acres, less than 10 acres <input type="checkbox"/> Over 10 acres		Total: \$
TOTAL AMOUNT DUE:			



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SIGN VARIANCE

DATE:	PZC NUMBER:	DEVELOPMENT NAME:
-------	-------------	-------------------

PETITIONER INFORMATION

NAME: ^{McENERY} LEONARD MCHENRY, MANAGER COMPANY: LENNY'S GAS N WASH YORKVILLE, LLC
 MAILING ADDRESS 8200 W. 185th St., UNIT K
 CITY, STATE, ZIP: TINLEY PARK, IL 60487 TELEPHONE: (708) 444-0117 CELL: (708) 805-2525
 EMAIL: LENMCHENERY@AOL.COM FAX: (708) 444-0159

PROPERTY INFORMATION

NAME OF HOLDER OF LEGAL TITLE: BETTY O'KEEFE FAMILY LIMITED PARTNERSHIP % BETTY O'KEEFE PROPERTIES LLC
 IF LEGAL TITLE IS HELD BY A LAND TRUST, LIST THE NAMES OF ALL HOLDERS OF ANY BENEFICIAL INTEREST THEREIN:

PROPERTY STREET ADDRESS:
 DESCRIPTION OF PROPERTY'S PHYSICAL LOCATION. NORTHEAST CORNER RTE. 47 AND WATERPARK WAY

CURRENT ZONING CLASSIFICATION: B-3 GENERAL BUSINESS DISTRICT

ZONING AND LAND USE OF SURROUNDING PROPERTIES

NORTH: B-3 VACANT AG USE
 EAST: B-3 VACANT AG USE
 SOUTH: B-3 VACANT AG USE
 WEST: R-4 VACANT AG USE

KENDALL COUNTY PARCEL IDENTIFICATION NUMBER(S)

PART OF 02-09-100-015		



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SIGN VARIANCE

ATTORNEY INFORMATION

NAME: **LYMAN C. TIEMAN** COMPANY: **LAW OFFICE OF LYMAN C. TIEMAN**
 MAILING ADDRESS: **12417 TAHOE LN.**
 CITY, STATE, ZIP: **MOKENA, IL 60448** TELEPHONE: **(815) 370-3383**
 EMAIL: **LCTIEMAN@YAHOO.COM** FAX: **(815) 717-6352**

ENGINEER INFORMATION

NAME: **BRIAN HERTZ** COMPANY: **M. GINGERICH GEREUX & ASSOC. INC. MGR2A**
 MAILING ADDRESS: **25620 S. GOUGAR RD.**
 CITY, STATE, ZIP: **MANHATTAN, IL 60442** TELEPHONE: **(815) 478-9680**
 EMAIL: **BHERTZ@MG2A.COM** FAX: **(815) 478-9685**

LAND PLANNER/SURVEYOR INFORMATION

NAME: **SCOTT PRITCHETT** COMPANY: **ARSA SCHNEIDER ARCHITECTS**
 MAILING ADDRESS: **1411 W. PETERSON AVE., STE 203**
 CITY, STATE, ZIP: **PARK RIDGE, IL 60068** TELEPHONE: **(847) 698-4438**
 EMAIL: **ARSA@ENTERACT.COM** FAX: **(847) 698-9889**

ATTACHMENTS

PETITIONER MUST ATTACH A LEGAL DESCRIPTION OF THE PROPERTY TO THIS APPLICATION AND TITLE IT AS "EXHIBIT A".

PETITIONER MUST LIST THE NAMES AND ADDRESSES OF ANY ADJOINING OR CONTIGUOUS LANDOWNERS WITHIN FIVE HUNDRED (500) FEET OF THE PROPERTY THAT ARE ENTITLED NOTICE OF APPLICATION UNDER ANY APPLICABLE CITY ORDINANCE OR STATE STATUTE. ATTACH A SEPARATE LIST TO THIS APPLICATION AND TITLE IT AS "EXHIBIT B".



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR SIGN VARIANCE

SIGN VARIANCE STANDARDS

- | | | |
|--|---|--|
| WAS THE SIGN ERECTED LEGALLY WITH A SIGN PERMIT? | <input type="checkbox"/> YES | <input checked="" type="checkbox"/> NO |
| ARE THERE LIMITED AVAILABLE LOCATIONS FOR SIGNAGE ON THE PROPERTY? | <input checked="" type="checkbox"/> YES | <input type="checkbox"/> NO |
| DOES THE SIGN FACE A STREET WITH A FORTY (40) MILE PER HOUR OR HIGHER SPEED LIMIT? | <input checked="" type="checkbox"/> YES | <input type="checkbox"/> NO |
| IS THE SIGN ON A STREET WITH TWENTY THOUSAND (20,000) OR HIGHER VEHICLE TRIPS PER DAY? | <input checked="" type="checkbox"/> YES | <input type="checkbox"/> NO |
| IS THE SIGN ON A WALL FACING A PUBLIC RIGHT-OF-WAY WITHOUT A PUBLIC ENTRANCE? | <input type="checkbox"/> YES | <input checked="" type="checkbox"/> NO |
| WOULD THE SIGN BE BLOCKED BY EXISTING OR REQUIRED LANDSCAPING? | <input checked="" type="checkbox"/> YES | <input type="checkbox"/> NO |

PLEASE STATE THE VARIANCE REQUESTED AND THE CITY ORDINANCE INCLUDING THE SECTION NUMBERS TO BE VARIED:

Requesting a square footage variance regarding section 10-20-9: A
The variance would allow for (1) 107 square foot monument sign.

PLEASE STATE HOW THE PROPOSED VARIATION WILL NOT IMPAIR AN ADEQUATE SUPPLY OF LIGHT AND AIR TO ADJACENT PROPERTY, OR SUBSTANTIALLY INCREASE THE CONGESTION IN THE PUBLIC STREETS, OR INCREASE THE DANGER TO THE PUBLIC SAFETY, OR SUBSTANTIALLY DIMINISH OR IMPAIR PROPERTY VALUES WITHIN THE NEIGHBORHOOD:

The proposed variation will not impair an adequate supply of light and air to adjacent properties. The variation will not increase congestion in the streets, nor will it increase the danger to the public. The variation may increase property values within the surrounding area.

PLEASE CONFIRM THE PROPOSED VARIATION IS CONSISTENT WITH THE OFFICIAL COMPREHENSIVE PLAN AND OTHER DEVELOPMENT STANDARDS AND POLICIES OF THE CITY.

The proposed variation & monument sign is consistent with the official comprehensive plan and other standards & policies of the city.



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR SIGN VARIANCE

SIGN VARIANCE STANDARDS

PLEASE DESCRIBE THE COST TO THE PETITIONER OF COMPLYING WITH THE SIGN ORDINANCE REQUIREMENTS:

This Gas station must communicate several items, like 3 gas prices units, the name of the station, any food service offered within, and any special items available inside. If the sign were to conform, the square footage would be too small to properly display all items.

IF THERE ARE ANY UNIQUE PHYSICAL CHARACTERISTICS OF THE PROPERTY, PLEASE DESCRIBE THEM BELOW:

The property has a 55' R.O.W. dedication along Rt.47 which will force the sign a good distance away from the main road.

PLEASE STATE HOW THE GRANTING OF THE VARIATION WILL NOT BE DETRIMENTAL TO THE PUBLIC WELFARE OR INJURIOUS TO OTHER PROPERTY OR IMPROVEMENTS IN THE NEIGHBORHOOD IN WHICH THE PROPERTY IS LOCATED:

Granting the variance will enhance public welfare, + neighboring properties.

PLEASE STATE HOW THE PARTICULAR SURROUNDINGS, SHAPE OR TOPOGRAPHICAL CONDITIONS OF THE SPECIFIC PROPERTY INVOLVED, A PARTICULAR HARDSHIP TO THE OWNER WOULD RESULT, AS DISTINGUISHED FROM A MERE INCONVENIENCE, IF THE STRICT LETTER OF REGULATIONS WAS CARRIED OUT:

A hardship would result from strict regulation as in not being able to effectively display the appropriate information customers need to know.



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SIGN VARIANCE

SIGN VARIANCE STANDARDS

PLEASE STATE HOW THE CONDITIONS UPON WHICH THE APPLICATION FOR A VARIATION IS BASED ARE UNIQUE TO THE PROPERTY FOR WHICH THE VARIATION IS SOUGHT AND ARE NOT APPLICABLE, GENERALLY, TO OTHER PROPERTY WITHIN THE SAME ZONING CLASSIFICATION:

A gas station must display gas prices in an effective manner, along with the name of the station, and any addition tenants inside.

PLEASE STATE HOW THE ALLEGED DIFFICULTY OR HARDSHIP IS CAUSED BY THIS TITLE AND HAS NOT BEEN CREATED BY ANY PERSON PRESENTLY HAVING AN INTEREST IN THE PROPERTY:

The hardship is caused by the ordinance as stated, and has not been created by any person having an interest in the property.

AGREEMENT

I VERIFY THAT ALL THE INFORMATION IN THIS APPLICATION IS TRUE TO THE BEST OF MY KNOWLEDGE. I UNDERSTAND AND ACCEPT ALL REQUIREMENTS AND FEES AS OUTLINED AS WELL AS ANY INCURRED ADMINISTRATIVE AND PLANNING CONSULTANT FEES WHICH MUST BE CURRENT BEFORE THIS PROJECT CAN PROCEED TO THE NEXT SCHEDULED COMMITTEE MEETING

I UNDERSTAND ALL OF THE INFORMATION PRESENTED IN THIS DOCUMENT AND UNDERSTAND THAT IF AN APPLICATION BECOMES DORMANT IT IS THROUGH MY OWN FAULT AND I MUST THEREFORE FOLLOW THE REQUIREMENTS OUTLINED ABOVE.

Samuel McEneaney
 PETITIONER SIGNATURE

June 14, 2019
 DATE

OWNER HEREBY AUTHORIZES THE PETITIONER TO PURSUE THE APPROPRIATE ENTITLEMENTS ON THE PROPERTY.

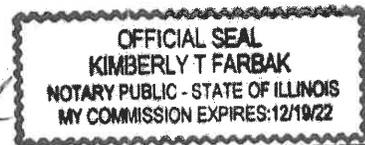
Betty O'Keefe Family Limited Partnership

By: K. W. O'Keefe
 OWNER SIGNATURE

June 17, 2019
 DATE

THIS APPLICATION MUST BE NOTARIZED PLEASE NOTARIZE HERE:

Kimberly T. Farbak





United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR SIGN VARIANCE

SIGN VARIANCE STANDARDS

PLEASE STATE HOW THE CONDITIONS UPON WHICH THE APPLICATION FOR A VARIATION IS BASED ARE UNIQUE TO THE PROPERTY FOR WHICH THE VARIATION IS SOUGHT AND ARE NOT APPLICABLE, GENERALLY, TO OTHER PROPERTY WITHIN THE SAME ZONING CLASSIFICATION:

A gas station must display gas prices in an effective manner, along with the name of the station, and any addition tenants inside.

PLEASE STATE HOW THE ALLEGED DIFFICULTY OR HARDSHIP IS CAUSED BY THIS TITLE AND HAS NOT BEEN CREATED BY ANY PERSON PRESENTLY HAVING AN INTEREST IN THE PROPERTY:

The hardship is caused by the ordinance as stated, and has not been created by any person having an interest in the property.

AGREEMENT

I VERIFY THAT ALL THE INFORMATION IN THIS APPLICATION IS TRUE TO THE BEST OF MY KNOWLEDGE. I UNDERSTAND AND ACCEPT ALL REQUIREMENTS AND FEES AS OUTLINED AS WELL AS ANY INCURRED ADMINISTRATIVE AND PLANNING CONSULTANT FEES WHICH MUST BE CURRENT BEFORE THIS PROJECT CAN PROCEED TO THE NEXT SCHEDULED COMMITTEE MEETING

I UNDERSTAND ALL OF THE INFORMATION PRESENTED IN THIS DOCUMENT AND UNDERSTAND THAT IF AN APPLICATION BECOMES DORMANT IT IS THROUGH MY OWN FAULT AND I MUST THEREFORE FOLLOW THE REQUIREMENTS OUTLINED ABOVE.

Leonard McEneaney
 PETITIONER SIGNATURE

June 14, 2019
 DATE

OWNER HEREBY AUTHORIZES THE PETITIONER TO PURSUE THE APPROPRIATE ENTITLEMENTS ON THE PROPERTY.

 OWNER SIGNATURE

 DATE

THIS APPLICATION MUST BE NOTARIZED PLEASE NOTARIZE HERE:

Kimberly T. Farbak





United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

PETITIONER DEPOSIT ACCOUNT/ ACKNOWLEDGMENT OF FINANCIAL RESPONSIBILITY

PROJECT NUMBER:	FUND ACCOUNT NUMBER:	PROPERTY ADDRESS:
-----------------	----------------------	-------------------

APPLICATION/APPROVAL TYPE (check appropriate box(es) of approval requested):

<input type="checkbox"/> CONCEPT PLAN REVIEW	<input type="checkbox"/> AMENDMENT (TEXT)	<input type="checkbox"/> ANNEXATION	<input type="checkbox"/> REZONING
<input type="checkbox"/> SPECIAL USE	<input type="checkbox"/> MILE AND 1/2 REVIEW	<input checked="" type="checkbox"/> ZONING VARIANCE	<input type="checkbox"/> PRELIMINARY PLAN
<input type="checkbox"/> FINAL PLANS	<input type="checkbox"/> PLANNED UNIT DEVELOPMENT	<input type="checkbox"/> FINAL PLAT	

PETITIONER DEPOSIT ACCOUNT FUND:
 It is the policy of the United City of Yorkville to require any petitioner seeking approval on a project or entitlement request to establish a Petitioner Deposit Account Fund to cover all actual expenses occurred as a result of processing such applications and requests. Typical requests requiring the establishment of a Petitioner Deposit Account Fund include, but are not limited to, plan review of development approvals/engineering permits. Deposit account funds may also be used to cover costs for services related to legal fees, engineering and other plan reviews, processing of other governmental applications, recording fees and other outside coordination and consulting fees. Each fund account is established with an initial deposit based upon the estimated cost for services provided in the INVOICE & WORKSHEET PETITION APPLICATION. This initial deposit is drawn against to pay for these services related to the project or request. Periodically throughout the project review/approval process, the Financially Responsible Party will receive an invoice reflecting the charges made against the account. At any time the balance of the fund account fall below ten percent (10%) of the original deposit amount, the Financially Responsible Party will receive an invoice requesting additional funds equal to one-hundred percent (100%) of the initial deposit if subsequent reviews/fees related to the project are required. In the event that a deposit account is not immediately replenished, review by the administrative staff, consultants, boards and commissions may be suspended until the account is fully replenished. If additional funds remain in the deposit account at the completion of the project, the city will refund the balance to the Financially Responsible Party. A written request must be submitted by the Financially Responsible Party to the city by the 15th of the month in order for the refund check to be processed and distributed by the 15th of the following month. All refund checks will be made payable to the Financially Responsible Party and mailed to the address provided when the account was established.

ACKNOWLEDGMENT OF FINANCIAL RESPONSIBILITY

NAME: <u>LEONARD MCEENERY</u>	COMPANY: <u>LENNY'S GAS N WASH YORKVILLE, LLC</u>
MAILING ADDRESS: <u>8200 W. 185th ST, UNIT K</u>	
CITY, STATE, ZIP: <u>TINLEY PARK, IL 60487</u>	TELEPHONE: <u>(708) 444-0117</u>
EMAIL: <u>LENMCEENERY@AOL.COM</u>	FAX: <u>(708) 444-0159</u>

FINANCIALLY RESPONSIBLE PARTY:
 I acknowledge and understand that as the Financially Responsible Party, expenses may exceed the estimated initial deposit and, when requested by the United City of Yorkville, I will provide additional funds to maintain the required account balance. Further, the sale or other disposition of the property does not relieve the Individual or Company/ Corporation of their obligation to maintain a positive balance in the fund account, unless the United City of Yorkville approves a Change of Responsible Party and transfer of funds. Should the account go into deficit, all City work may stop until the requested replenishment deposit is received.

<u>Leonard McEenery</u>	<u>owner - messaging member</u>
PRINT NAME	TITLE
<u>Leonard McEenery</u>	<u>June 14, 2019</u>
SIGNATURE	DATE

ACCOUNT CLOSURE AUTHORIZATION

DATE REQUESTED: _____	<input type="checkbox"/> COMPLETED	<input type="checkbox"/> INACTIVE			
PRINT NAME: _____	<input type="checkbox"/> WITHDRAWN	<input type="checkbox"/> COLLECTIONS			
SIGNATURE: _____	<input type="checkbox"/> OTHER				
DEPARTMENT ROUTING FOR AUTHORIZATION:	<input type="checkbox"/> COM. DEV.	<input type="checkbox"/> BUILDING	<input type="checkbox"/> ENGINEERING	<input type="checkbox"/> FINANCE	<input type="checkbox"/> ADMIN.

ALTA/NSPS LAND TITLE SURVEY

Items Corresponding to Schedule B

THE FOLLOWING ARE ITEMS IN SCHEDULE B IN TITLE INSURANCE COMPANY TITLE COMMITMENT NO. WITH AN EFFECTIVE DATE OF _____, 2019

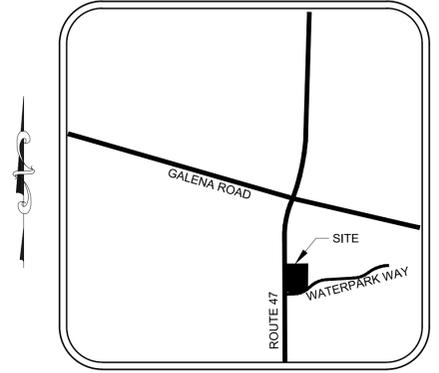
Utility Notes

- UN1** THE LOCATION OF UTILITIES SHOWN HEREON ARE FROM OBSERVED EVIDENCE OF ABOVE GROUND APPURTENANCES ONLY. THE SURVEYOR WAS NOT PROVIDED WITH UNDERGROUND PLANS OR SURFACE GROUND MARKINGS TO DETERMINE THE LOCATION OF ANY SUBTERRANEAN USES.
- UN2** CALL "J.U.L.L.E." FOR FIELD LOCATIONS OF UNDERGROUND UTILITY LINES 1-800-892-0123

Miscellaneous Notes

- MN1** DIMENSIONS ON THIS PLAT ARE EXPRESSED IN FEET AND DECIMAL PARTS THEREOF UNLESS OTHERWISE NOTED.
- MN2** AREA = 245,233 SQ. FT. OR 5.63 ACRES
- MN3** PART OF PIN # 02-09-100-003-0000
- MN4** BEARING BASIS PER SUBDIVISION PLAT
- MN5** AT THE TIME OF THE SURVEY THERE WAS NO OBSERVABLE EVIDENCE OF CHANGES IN STREET RIGHT-OF-WAY LINES OR STREET AND SIDEWALK REPAIRS OR CONSTRUCTION.
- MN6** AT THE TIME OF THE SURVEY THERE WAS NO OBSERVABLE EVIDENCE OF EARTH MOVING, BUILDING CONSTRUCTION, OR BUILDING ADDITIONS WITHIN RECENT MONTHS.

Vicinity Map

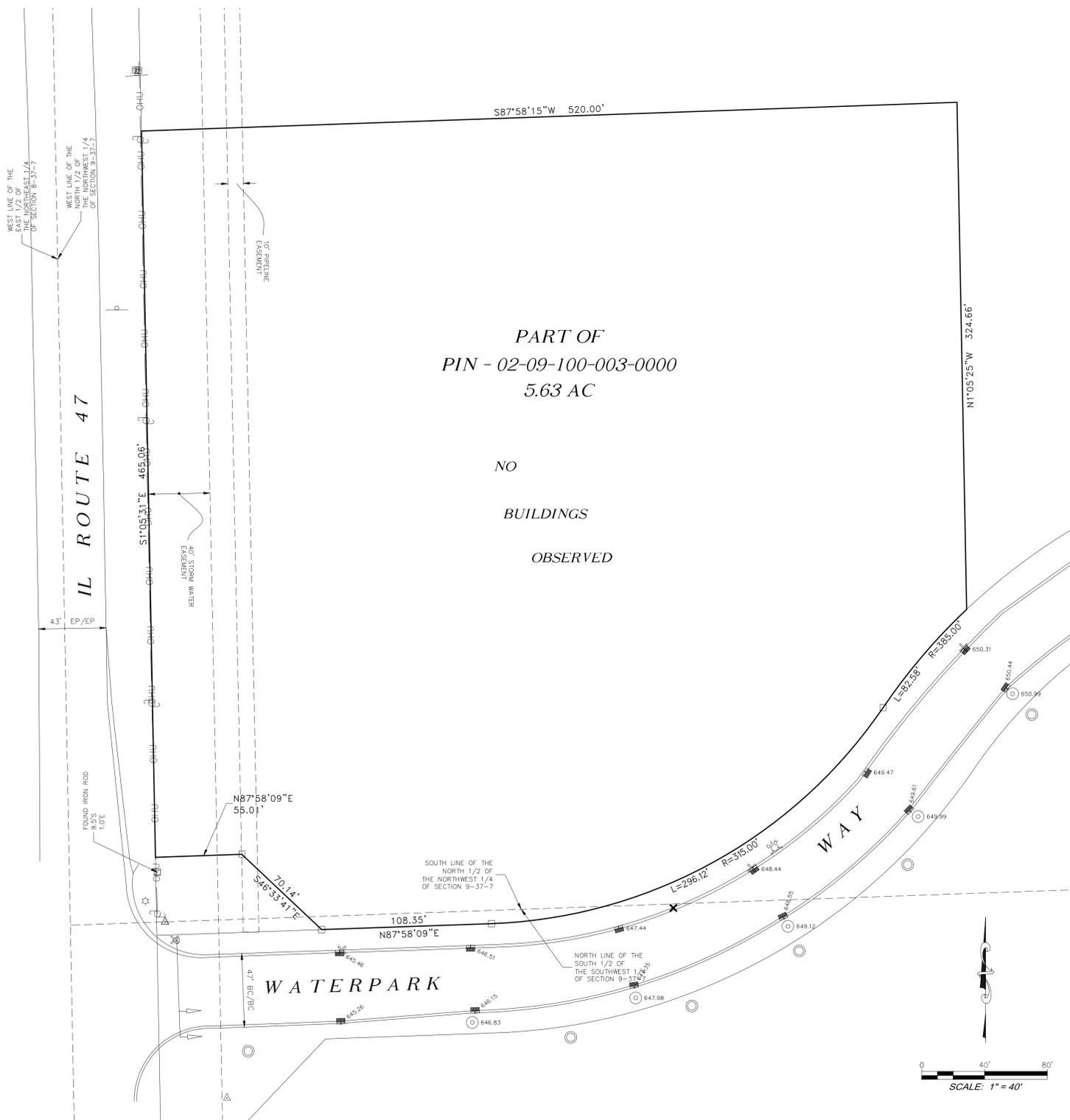


Legend of Symbols & Abbreviations

- | | | |
|---|--|---|
| <ul style="list-style-type: none"> ■ SET IRON ROD □ FOUND IRON ROD ▲ SET MAG NAIL 7.77 MEASURED DATA (7.77) RECORDED DATA P.U.E. PUBLIC UTILITY EASEMENT D.E. DRAINAGE EASEMENT L.E. LANDSCAPE EASEMENT B.S.L. BUILDING SETBACK LINE | <ul style="list-style-type: none"> ○ STORM SEWER MANHOLE ○ SANITARY SEWER MANHOLE ○ FIRE HYDRANT ○ POWER POLE ○ GUY WIRE ○ SIGN ○ UNKNOWN MANHOLE ○ TRAFFIC SIGNAL | <ul style="list-style-type: none"> □ ELECTRIC METER □ TRANSFORMER □ TELEPHONE PEDESTAL ▨ ASPHALT SURFACE ▨ AGGREGATE SURFACE ▨ CONCRETE SURFACE |
|---|--|---|

LAND DESCRIPTION

THAT PART OF THE WEST HALF OF SECTION 9, TOWNSHIP 37 NORTH, RANGE 7 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN KENDALL COUNTY, ILLINOIS, DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHWEST CORNER OF LOT 3 IN O'KEEFE SUBDIVISION UNIT ONE, RECORDED AS DOCUMENT 200800012915, THENCE NORTH 87 DEGREES 58 MINUTES 09 SECONDS EAST ALONG THE NORTH LINE OF SAID LOT 3 55.01 FEET; THENCE SOUTH 46 DEGREES 33 MINUTES 41 SECONDS EAST ALONG THE NORTH LINE OF SAID LOT 3 70.14 FEET TO THE NORTH RIGHT OF WAY OF ROSEWINKEL STREET; THENCE NORTH 87 DEGREES 58 MINUTES 09 SECONDS EAST ALONG SAID RIGHT OF WAY 108.35 FEET TO A CURVE, CONCAVE TO THE NORTHWEST, HAVING A RADIUS OF 315.00 FEET, A CHORD OF 285.34 FEET AND A CHORD BEARING OF NORTH 61 DEGREES 00 MINUTES 09 SECONDS EAST; THENCE ALONG SAID CURVE AND SAID RIGHT OF WAY 296.12 FEET TO A CURVE, CONCAVE TO THE SOUTHEAST, HAVING A RADIUS OF 385.00 FEET, A CHORD OF 82.42 FEET, AND A CHORD BEARING OF NORTH 40 DEGREES 15 MINUTES 11 SECONDS EAST; THENCE ALONG SAID CURVE AND SAID RIGHT OF WAY 82.58 FEET; THENCE NORTH 01 DEGREES 05 MINUTES 25 SECONDS WEST 324.66 FEET; THENCE SOUTH 87 DEGREES 58 MINUTES 15 SECONDS WEST 520.00 FEET TO THE WEST RIGHT OF WAY OF ROUTE 47; THENCE SOUTH 01 DEGREES 05 MINUTES 31 SECONDS EAST ALONG SAID RIGHT OF WAY 465.06 FEET TO THE POINT OF BEGINNING.



SCALE: 1" = 40'

TO: INSURANCE COMPANY

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS () OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON JUNE 08, 2019.

DATED: 06/08/2019

ROBERT F. SLUIS
ILLINOIS PROFESSIONAL LAND SURVEYOR #35-3558
LICENSE EXPIRES NOVEMBER 30, 2020



**DRAFT
FOR REVIEW**
 REV 7 / 18 / 19

REVISIONS		
DATE	BY	DESCRIPTION

MGA CIVIL ENGINEERING SURVEYING

M GINGERICH GEREUX & ASSOCIATES
Professional Design Firm License # 184.005003
P. 815-478-9680 www.mg2a.com F. 815-478-9685
25620 S. GOUGAR RD | MANHATTAN, IL. 60442

ORDERED BY: **GAS N WASH**

DATE: 06-08-2019 DR: B1/NIB CK: B1/RFS TITLE: _____

CD: 19-451 PLOT 1 of 1

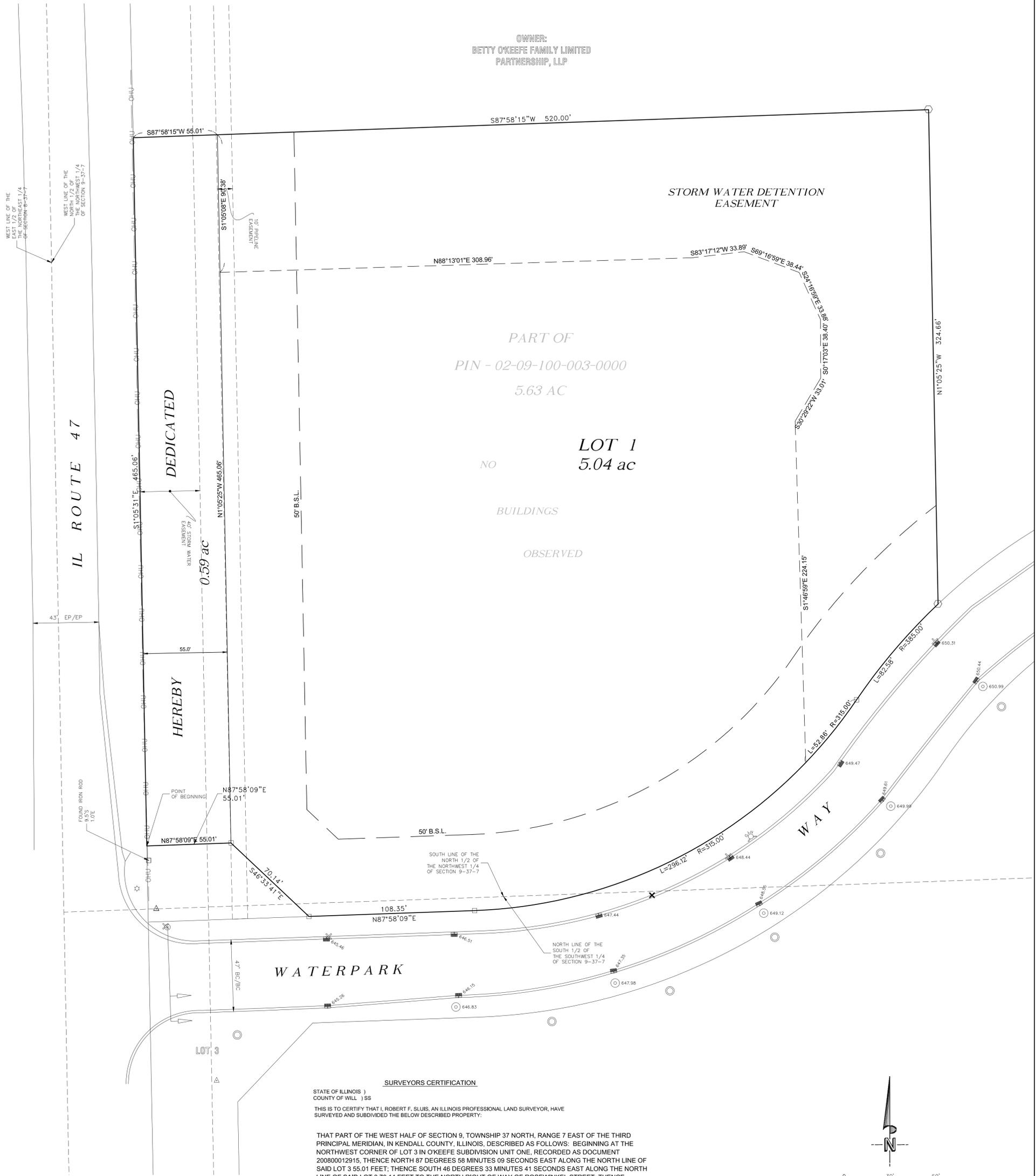
FINAL PLAT

of

GAS N WASH ROUTE 47 YORKVILLE SUBDIVISION

BEING A SUBDIVISION OF PART OF THE WEST HALF OF SECTION 9, TOWNSHIP 37 NORTH, RANGE 7 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN KENDALL COUNTY, ILLINOIS.

OWNER:
BETTY O'KEEFE FAMILY LIMITED
PARTNERSHIP, LLP



SURVEYORS CERTIFICATION

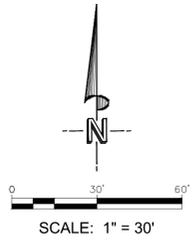
STATE OF ILLINOIS)
COUNTY OF WILL) SS
THIS IS TO CERTIFY THAT I, ROBERT F. SLUIS, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED AND SUBDIVIDED THE BELOW DESCRIBED PROPERTY:

THAT PART OF THE WEST HALF OF SECTION 9, TOWNSHIP 37 NORTH, RANGE 7 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN KENDALL COUNTY, ILLINOIS, DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHWEST CORNER OF LOT 3 IN O'KEEFE SUBDIVISION UNIT ONE, RECORDED AS DOCUMENT 200800012915, THENCE NORTH 87 DEGREES 58 MINUTES 09 SECONDS EAST ALONG THE NORTH LINE OF SAID LOT 3 55.01 FEET; THENCE SOUTH 46 DEGREES 33 MINUTES 41 SECONDS EAST ALONG THE NORTH LINE OF SAID LOT 3 70.14 FEET TO THE NORTH RIGHT OF WAY OF ROSEWINKEL STREET; THENCE NORTH 87 DEGREES 58 MINUTES 09 SECONDS EAST ALONG SAID RIGHT OF WAY 108.35 FEET TO A CURVE, CONCAVE TO THE NORTHWEST, HAVING A RADIUS OF 315.00 FEET, A CHORD OF 285.34 FEET AND A CHORD BEARING OF NORTH 61 DEGREES 00 MINUTES 09 SECONDS EAST; THENCE ALONG SAID CURVE AND SAID RIGHT OF WAY 296.12 FEET TO A CURVE, CONCAVE TO THE SOUTHEAST, HAVING A RADIUS OF 385.00 FEET, A CHORD OF 82.42 FEET, AND A CHORD BEARING OF NORTH 40 DEGREES 15 MINUTES 11 SECONDS EAST; THENCE ALONG SAID CURVE AND SAID RIGHT OF WAY 82.58 FEET; THENCE NORTH 01 DEGREES 05 MINUTES 25 SECONDS WEST 324.66 FEET; THENCE SOUTH 87 DEGREES 58 MINUTES 15 SECONDS WEST 520.00 FEET TO THE WEST RIGHT OF WAY OF ROUTE 47; THENCE SOUTH 01 DEGREES 05 MINUTES 31 SECONDS EAST ALONG SAID RIGHT OF WAY 465.06 FEET TO THE POINT OF BEGINNING.

- I DO FURTHER CERTIFY THAT:
- NO PART OF THIS SUBDIVISION IS LOCATED WITHIN A SPECIAL FLOOD HAZARD AREA AS IDENTIFIED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY, FIRM MAP NUMBER 17093C0030G, EFFECTIVE DATE FEBRUARY 4, 2009.
 - THE ACCOMPANIED PLAT IS TRUE AND CORRECT REPRESENTATION OF SAID SURVEY.
 - ALL LOTS CORNERS WILL BE STAKED WITH 5/8 INCH BY 24 INCH IRON RODS OR MONUMENTED ACCORDING TO THE PLAT ACT AS AMENDED.
 - DISTANCES ARE IN FEET AND DECIMAL PARTS THEREOF.
 - THIS SUBDIVISION CONTAINS 5.63 ACRES.
 - PART OF PIN # 02-09-100-003
 - THE PROPERTY LIES WITHIN CORPORATE LIMITS OF THE UNITED CITY OF YORKVILLE.

Dated this _____ day of _____, 2019.

Robert F. Sluis
Illinois Professional Land Surveyor #035-003558
License Expires November 30, 2020



- P.U.E. - PUBLIC UTILITY EASEMENT
- D.E. - DRAINAGE EASEMENT
- B.S.L. - BUILDING SETBACK LINE
- - CONCRETE MONUMENT

DRAFT FOR REVIEW
 REV 7 / 18 / 2019

REVISIONS		
DATE	BY	DESCRIPTION

MGA CIVIL ENGINEERING SURVEYING

M GINGERICH GEREAX & ASSOCIATES
 Professional Design Firm License # 184.005003
 P. 815-478-9680 www.mg2a.com F. 815-478-9685
 25620 S. GOUGAR RD | MANHATTAN, IL. 60442

ORDERED BY: **GAS N WASH**

DATE ISSUED: 8/14/2019
 DRAWN BY: BYNIB
 CHECKED BY: BYRFS
 FILE: 1 of 2

FINAL PLAT of GAS N WASH ROUTE 47 YORKVILLE SUBDIVISION

BEING A SUBDIVISION OF PART OF THE WEST HALF OF SECTION 9, TOWNSHIP 37 NORTH, RANGE 7 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN KENDALL COUNTY, ILLINOIS.

OWNERSHIP CERTIFICATE

STATE OF ILLINOIS)
)SS
COUNTY OF _____)

_____ IS THE OWNER OF THE LAND DESCRIBED IN THE ATTACHED PLAT HAS CAUSED THE SAME TO BE SURVEYED, SUBDIVIDED AND PLATED AS SHOWN BY THE PLAT FOR USES AND PURPOSES AS INDICATED THEREON, AND DOES HEREBY ACKNOWLEDGE AND ADOPT THE SAME UNDER THE STYLE AND TITLE THEREON INDICATED.

DATED THIS _____ DAY OF _____, A.D. 2019.

BY _____ OWNER:
(OWNER) GAS N WASH YORKVILLE LLC
8200 185TH ST. - UNIT K
TINLEY PARK, IL 60487

NOTARY CERTIFICATE

STATE OF ILLINOIS)
)SS
COUNTY OF _____)

I, _____, A NOTARY PUBLIC, IN AND FOR SAID COUNTY, IN THE STATE AFORESAID, DO HEREBY CERTIFY THAT THE PEOPLE WHOSE SIGNATURES APPEAR IN THE "OWNERSHIP CERTIFICATE" ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS WHOSE NAMES ARE SUBSCRIBED ON THE FOREGOING INSTRUMENT AS SUCH OWNERS AND THAT THEY APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT THEY SIGNED AND DELIVERED THE ANNEXED PLAT AS THEIR OWN FREE AND VOLUNTARY ACT FOR PURPOSES THEREIN SET FORTH.

GIVEN UNDER MY HAND AND NOTORIAL SEAL

THIS _____ DAY OF _____, A.D. 2019.

(NOTARY PUBLIC)

SCHOOL DISTRICT CERTIFICATE

STATE OF ILLINOIS)
)SS
COUNTY OF _____)

TO THE BEST OF THE UNDERSIGNED OWNER'S KNOWLEDGE, THE ABOVE DESCRIBED SUBDIVISION KNOWN AS _____ GAS N WASH YORKVILLE SUBDIVISION _____

LIES WITHIN _____ SCHOOL DISTRICT # _____ AND _____ HIGH SCHOOL DISTRICT # _____.

DATED THIS _____ DAY OF _____, A.D. 2019.

BY _____
(OWNER)

NOTARY CERTIFICATE

STATE OF ILLINOIS)
)SS
COUNTY OF _____)

I, _____, A NOTARY PUBLIC, IN AND FOR SAID COUNTY, IN THE STATE AFORESAID, DO HEREBY CERTIFY THAT THE PEOPLE WHOSE SIGNATURES APPEAR IN THE "SCHOOL DISTRICT CERTIFICATE" ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS WHOSE NAMES ARE SUBSCRIBED ON THE FOREGOING INSTRUMENT AS SUCH OWNERS AND THAT THEY APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT THEY SIGNED AND DELIVERED THE ANNEXED PLAT AS THEIR OWN FREE AND VOLUNTARY ACT FOR PURPOSES THEREIN SET FORTH.

GIVEN UNDER MY HAND AND NOTORIAL SEAL

THIS _____ DAY OF _____, A.D. 2019.

(NOTARY PUBLIC)

CITY CLERK CERTIFICATE

STATE OF ILLINOIS)
)SS
COUNTY OF KENDALL)

I, _____, CITY CLERK OF THE UNITED CITY OF YORKVILLE ILLINOIS, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT OR UNPAID CURRENT OR FORFEITED SPECIAL ASSESSMENTS OR ANY DEFERRED INSTALLMENTS APPORTIONED AGAINST THE TRACT INCLUDED IN THIS PLAT, DATED AT YORKVILLE, KENDALL COUNTY ILLINOIS

THIS _____ DAY OF _____, A.D., 2019.

CITY CLERK

CITY ADMINISTRATOR CERTIFICATE

STATE OF ILLINOIS)
)SS
COUNTY OF KENDALL)

APPROVED BY THE CITY ADMINISTRATOR OF THE UNITED CITY OF YORKVILLE.

ILLINOIS THIS _____ DAY OF _____, A.D., 2019.

CITY ADMINISTRATOR

PLAN COMMISSION CERTIFICATE

STATE OF ILLINOIS)
)SS
COUNTY OF KENDALL)

APPROVED BY THE PLAN COMMISSION OF THE UNITED CITY OF YORKVILLE, ILLINOIS,

AT A MEETING HELD ON THE _____ DAY OF _____, A.D., 2019.

CHAIRMAN

SECRETARY

CITY COUNCIL CERTIFICATE

STATE OF ILLINOIS)
)SS
COUNTY OF KENDALL)

APPROVED BY THE CITY COUNCIL OF THE UNITED CITY OF YORKVILLE, ILLINOIS,

AT A MEETING HELD ON THE _____ DAY OF _____, A.D., 2019.

PRESIDENT

CLERK

COUNTY CLERK'S CERTIFICATE

STATE OF ILLINOIS)
)SS
COUNTY OF KENDALL)

I, _____, COUNTY CLERK OF KENDALL COUNTY, ILLINOIS, AND CUSTODIAN OF THE RECORDS AND FILES OF SAID OFFICE, DO HEREBY CERTIFY THAT I FIND FROM SUCH RECORDS AND FILES, NO DELINQUENT GENERAL TAXES, NO UNPAID CURRENT GENERAL TAXES, NO DELINQUENT SPECIAL ASSESSMENTS ON FILE AGAINST THE TRACT OF LAND DESCRIBED ON THE ATTACHED PLAT OF _____ GAS N WASH ROUTE 47 YORKVILLE SUBDIVISION _____, AND THAT THERE ARE NO UNPAID DEFERRED INSTALLMENTS OF SPECIAL ASSESSMENTS AGAINST THE SAID PROPERTY OR ANY PART THEREOF.

DATED THIS _____ DAY OF _____, A.D. 2019.

COUNTY CLERK

SURFACE WATER STATEMENT

STATE OF ILLINOIS)
)SS
COUNTY OF WILL)

TO THE BEST OF OUR KNOWLEDGE AND BELIEF, THE DRAINAGE OF SURFACE WATERS WILL NOT BE CHANGED BY THE CONSTRUCTION OF SUCH IMPROVEMENTS OR ANY PART THEREOF, OR THAT IF SUCH SURFACE WATER DRAINAGE WILL BE CHANGED, REASONABLE PROVISION HAS BEEN MADE FOR COLLECTION AND DIVERSION OF SUCH WATERS INTO PUBLIC AREAS, OR DRAINS WHICH THE PROPERTY OWNER HAS A RIGHT TO USE, AND THAT SUCH SURFACE WATERS WILL BE PLANNED FOR IN ACCORDANCE WITH THE GENERALLY ACCEPTED ENGINEERING PRACTICES SO AS TO REDUCE THE LIKELIHOOD OF DAMAGE TO THE ADJOINING PROPERTY BECAUSE OF THE CONSTRUCTION OF THE IMPROVEMENTS.

DATED THIS _____ DAY OF _____, A.D. 2019.

ILLINOIS REGISTERED P.E. #062-054935

CITY ENGINEER CERTIFICATE

STATE OF ILLINOIS)
)SS
COUNTY OF _____)

I, _____, CITY ENGINEER FOR THE UNITED CITY OF YORKVILLE DO HEREBY CERTIFY THE REQUIRED IMPROVEMENTS HAVE BEEN INSTALLED OR THE REQUIRED GUARANTEE COLLATERAL HAS BEEN POSTED FOR THE COMPLETION OF ALL REQUIRED IMPROVEMENTS, DATED AT YORKVILLE,

ILLINOIS THIS _____ DAY OF _____, A.D., 2019.

CITY ENGINEER

DEPARTMENT OF TRANSPORTATION

STATE OF ILLINOIS)
)SS
COUNTY OF _____)

THIS PLAT HAS BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION WITH RESPECT TO ROADWAY ACCESS PURSUANT OF §2 OF "AN ACT TO REVISE THE LAW IN RELATION TO PLATS," AS AMENDED. A PLAN THAT MEETS THE REQUIREMENTS CONTAINED IN THE DEPARTMENT'S "POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS" WILL, BE REQUIRED BY THE DEPARTMENT.

_____ DAY OF _____, A.D. 2019.

ANTHONY J. QUIGLEY, P.E., REGION ONE ENGINEER

EASEMENT PROVISIONS

An easement for serving the subdivision and other property with electric and communication service is hereby reserved for and granted to:

Commonwealth Edison Company, Ameritech Illinois a.k.a. Illinois Bell Telephone Company, Grantees.

Their respective licensees, successors, and assigns, jointly and severally, to construct, operate, repair, maintain, modify, reconstruct, replace, supplement, relocate and remove, from time to time, poles, guys, anchors, wires, cables, conduits, manholes, transformers, pedestals, equipment cabinets or other facilities used in connection with overhead and underground transmission and distribution of electricity, communications, sounds and signals in, over, under, across, along and upon the surface of the property shown within the dashed or dotted lines (or similar designation) on the plat and marked "Easement", "Utility Easement", "Public Utility Easement", "P.U.E." (or similar designation), the property designated in the Declaration of Condominium and/or on this plat as "Common Elements", and the property designated on the plat as "common area or areas", and the property designated on the plat for streets and alleys, whether public or private, together with the rights to install required service connections over or under the surface of each lot and common area or areas, to serve improvements thereon, or on adjacent lots, and common area or areas, the right to cut, trim or remove trees, bushes, roots and saplings and to clear obstructions from the surface and subsurface as may be reasonably required incident to the rights herein given, and the right to enter upon the subdivided property for all such purposes. Obstructions shall not be placed over Grantees' facilities or in, upon or over the property within the dashed or dotted lines (or similar designation) marked "Easement", "Utility Easement", "Public Utility Easement", "P.U.E." (or similar designation) without the prior written consent of Grantees. After installation of any such facilities, the grade of the subdivided property shall not be altered in a manner so as to interfere with the proper operation and maintenance thereof. Further, any utility company at fault of creating or causing damage to existing field tile drainage systems, damage or disruption of other neighboring utilities, etc. shall be responsible for returning and performing repair or replacement work, at the sole expense of the utility company at fault. No expense or compensation will be provided by the developer for damage or repairs caused by the installation or maintenance of said utilities.

An easement is hereby reserved for and granted to NORTHERN ILLINOIS GAS COMPANY, its successors and assigns, in all platted "easement" areas, streets, alleys, other public ways and places shown on this plat, said easement to be for the installation, maintenance, relocation, renewal and removal of gas mains and appurtenances for the purpose of serving all areas shown on this plat as well as other property, whether or not contiguous thereto. No buildings or other structures shall be constructed or erected in any such "easement" areas, streets, alleys, or other public ways or places nor shall any other use be made thereof which will interfere with the easements reserved and granted hereby.

DRAINAGE EASEMENT (D.E.) PROVISIONS

AN EASEMENT FOR SERVING THE SUBDIVISION, AND OTHER PROPERTY WITH STORM DRAINAGE, IS HEREBY RESERVED AND GRANTED TO THE UNITED CITY OF YORKVILLE AND ITS RESPECTIVE SUCCESSORS AND ASSIGNS, JOINTLY AND SEPARATELY, TO INSTALL, OPERATE, MAINTAIN AND REMOVE, FROM TIME TO TIME, FACILITIES AND EQUIPMENT USED IN THE CONNECTION WITH THE STORM DRAINAGE SYSTEM, AND THEIR APPURTENANCES, EITHER ON, OVER, ACROSS, BELOW OR THROUGH THE GROUND SHOWN WITHIN THE DOTTED LINES ON THE PLAT MARKED "DRAINAGE EASEMENT" AND THE PROPERTY DESIGNATED ON THE PLAT FOR DRAINAGE TOGETHER WITH THE RIGHT TO CUT, TRIM OR REMOVE TREES, BUSHED AND ROOTS AS MAY BE REASONABLE REQUIRED INCIDENT TO THE RIGHTS HEREIN GIVEN, AND THE RIGHT TO ENTER UPON SUBDIVIDED PROPERTY FOR ALL SUCH PURPOSES. OBSTRUCTIONS SHALL NOT BE PLACED OVER GRANTEE'S FACILITIES OR IN, UPON OR OVER, THE PROPERTY WHICH WILL INTERFERE WITH ITS PROPOSED USE WITHIN THE DOTTED LINES MARKED "DRAINAGE EASEMENT" WITHOUT THE PRIOR WRITTEN CONSENT OF THE GRANTEE'S. AFTER THE INSTALLATION OF ANY SUCH FACILITIES, THE GRADE OF THE SUBDIVIDED PROPERTY SHALL NOT BE ALTERED IN A MANNER SO AS TO INTERFERE WITH THE PROPER OPERATION AND MAINTENANCE THEREOF.

DETENTION EASEMENT PROVISIONS

ALL EASEMENTS INDICATED AS "EASEMENT FOR DETENTION PURPOSES" ON THIS PLAT ARE RESERVED FOR AND GRANTED TO THE UNITED CITY OF YORKVILLE AND TO ITS SUCCESSORS AND ASSIGNS. NO BUILDINGS SHALL BE PLACED ON SAID DETENTION EASEMENT AREAS, BUT THE SAME MAY BE USED FOR OTHER PURPOSES THAT DO NOT ADVERSELY AFFECT THE STORAGE/FREE FLOW OF STORM WATER. EACH OWNER OR SUBSEQUENT PURCHASER SHALL BE EQUALLY RESPONSIBLE FOR MAINTAINING THE DETENTION EASEMENT AREAS AND SHALL NOT DESTROY OR MODIFY GRADES OR SLOPES WITHOUT HAVING FIRST RECEIVED PRIOR WRITTEN APPROVAL OF THE UNITED CITY OF YORKVILLE.

IN THE EVENT ANY OWNER OR SUBSEQUENT PURCHASER FAILS TO PROPERLY MAINTAIN THE EASEMENT AREAS, THE UNITED CITY OF YORKVILLE, ILLINOIS, OR ANY OTHER UNIT OF LOCAL GOVERNMENT HAVING JURISDICTION OVER DRAINAGE, SHALL UPON TEN (10) DAYS PRIOR WRITTEN NOTICE, RESERVE THE RIGHT TO PERFORM, OR HAVE PERFORMED ON ITS BEHALF, ANY MAINTENANCE WORK TO OR UPON THE WATER DETENTION AREA REASONABLY NECESSARY TO INSURE ADEQUATE STORM WATER STORAGE AND FREE FLOW OF STORM WATER THROUGH THE DETENTION EASEMENT AREAS.

PUBLIC UTILITY EASEMENT (P.U.E.) PROVISIONS

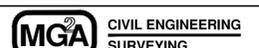
AN EASEMENT FOR SERVING THE SUBDIVISION, AND OTHER PROPERTY WITH STORM DRAINAGE, SANITARY SEWER, STREET LIGHTING, PORTABLE WATER SERVICE AND OTHER PUBLIC UTILITIES, IS HEREBY RESERVED AND GRANTED TO THE UNITED CITY OF YORKVILLE AND ITS RESPECTIVE SUCCESSORS AND ASSIGNS, JOINTLY AND SEPARATELY, TO INSTALL, OPERATE, MAINTAIN AND REMOVE, FROM TIME TO TIME, FACILITIES AND EQUIPMENT USED IN THE CONNECTION WITH THE PUBLIC WATER SUPPLY, TRANSMISSION LINES, SANITARY SEWERS, STORM DRAINAGE SYSTEM, STREET LIGHTING SYSTEM, OR OTHER PUBLIC UTILITY SERVICE, AND THEIR APPURTENANCES, EITHER ON, OVER, ACROSS, BELOW OR THROUGH THE GROUND SHOWN WITHIN THE DOTTED LINES ON THE PLAT MARKED "PUBLIC UTILITY EASEMENT" AND THE PROPERTY DESIGNATED ON THE PLAT FOR STREETS AND ALLEYS TOGETHER WITH THE RIGHT TO CUT, TRIM OR REMOVE TREES, BUSHED AND ROOTS AS MAY BE REASONABLE REQUIRED INCIDENT TO THE RIGHTS HEREIN GIVEN, AND THE RIGHT TO ENTER UPON SUBDIVIDED PROPERTY FOR ALL SUCH PURPOSES. OBSTRUCTIONS SHALL NOT BE PLACED OVER GRANTEE'S FACILITIES OR IN, UPON OR OVER, THE PROPERTY WHICH WILL INTERFERE WITH ITS PROPOSED USE WITHIN THE DOTTED LINES MARKED "PUBLIC UTILITY EASEMENT" WITHOUT THE PRIOR WRITTEN CONSENT OF THE GRANTEE'S. AFTER THE INSTALLATION OF ANY SUCH FACILITIES, THE GRADE OF THE SUBDIVIDED PROPERTY SHALL NOT BE ALTERED IN A MANNER SO AS TO INTERFERE WITH THE PROPER OPERATION AND MAINTENANCE THEREOF.

INGRESS/EGRESS EASEMENT PROVISIONS

INGRESS/EGRESS EASEMENT IS HEREBY CREATED AND GRANTED FOR THE BENEFIT OF THE UNITED CITY OF YORKVILLE, THE PUBLIC AND THE OWNERS OF ALL LOTS WITHIN THE SUBDIVISION, THEIR SUCCESSORS AND OR ASSIGNS AND CUSTOMERS, THE RIGHT OF PEDESTRIAN AND VEHICULAR ACCESS ACROSS AND OVER ALL PARKING, ROAD(S), ALLEY(S), STREETS AND OTHER INGRESS AND EGRESS EASEMENTS SHOWN ON THE PLAT DESIGNATED AS "INGRESS/EGRESS EASEMENT".

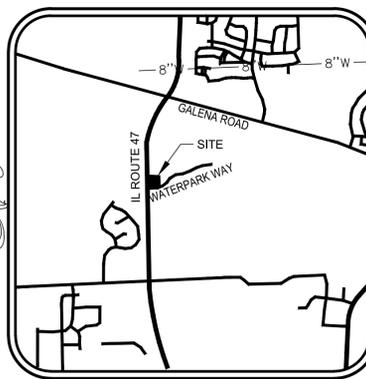
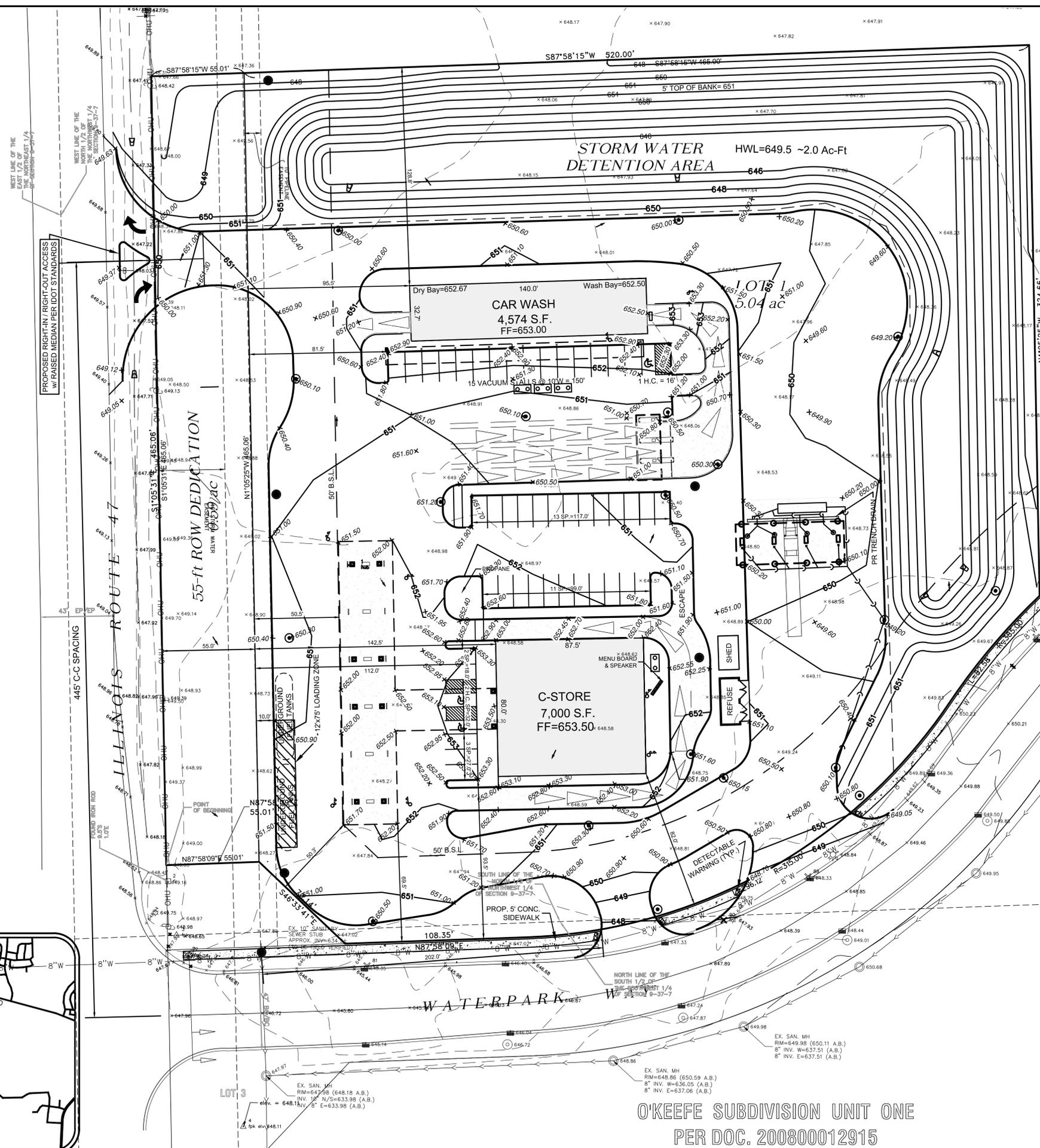
REVISIONS

DATE	BY	DESCRIPTION



M GINGERICH GEREUX & ASSOCIATES
Professional Design Firm License # 184.005003
P. 815-478-9680 www.mg2a.com F. 815-478-9685
25620 S. GOUGAR RD | MANHATTAN, IL. 60442

ORDERED BY: **GAS N WASH**
DATE ISSUED: 8/14/2019
JOB NO.: 19-451
DRAWN BY: **BYNIB**
CHECKED BY: **BYRFS**
PAGE: 2 of 2



SITE DATA

Area: 219,693.83 sq.ft. (5.04 ac)
 Current Zoning: B-3
 Building Area: 11,813 sq.ft.
 Building Coverage: 5.4%
 Pavement Area*: 104,505 sq.ft.
 Pavement Coverage*: 47.6%
 Landscaping Area: **TBD sq.ft.±
 Landscaping Coverage: **TBD %±
 Impervious Coverage: 67%

* Fueling Canopies are included in Pavement Area.
 **To be verified with Landscape Architect Plan

Height of Proposed C-Store: 34'
 Height of Proposed Car Wash: 21'-2"

PARKING DATA

Gas Station & Convenience Store (7,000 sf):
Convenience Store
 Number of Parking Stalls Provided: 31
 Number of Parking Stalls Req'd: 21
 (3 spaces /1,000 s.f.)

Drive Thru / Behind Menu Board
 Number of Stacking Stalls Provided: 7
 Number of Stacking Stalls Req'd: 3

Drive Thru / Menu to 1st Window
 Number of Stacking Stalls Provided: 3
 Number of Stacking Stalls Req'd: 3

Automatic Car Wash:
Entrance
 Number of Stacking Stalls Provided: 5
 Number of Stacking Stalls Req'd: 5
 (5 stalls per bay)

Exit
 Number of Stacking Stalls Provided: 2
 Number of Stacking Stalls Req'd: 2
 (2 stalls per bay)

Pay Station
 Number of Stacking Stalls Provided: 15
 Number of Stacking Stalls Req'd: n/a

Vacuum Stalls / Parking
 Number of Parking Stalls Provided: 16
 Number of Parking Stalls Req'd: n/a

LEGAL DESCRIPTION

THAT PART OF THE WEST HALF OF SECTION 9, TOWNSHIP 37 NORTH, RANGE 7 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN KENDALL COUNTY, ILLINOIS, DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHWEST CORNER OF LOT 3 IN O'KEEFE SUBDIVISION UNIT ONE, RECORDED AS DOCUMENT 200800012915, THENCE NORTH 87 DEGREES 58 MINUTES 09 SECONDS EAST ALONG THE NORTH LINE OF SAID LOT 3 55.01 FEET; THENCE SOUTH 46 DEGREES 33 MINUTES 41 SECONDS EAST ALONG THE NORTH LINE OF SAID LOT 3 70.14 FEET TO THE NORTH RIGHT OF WAY OF ROSEWINKEL STREET; THENCE NORTH 87 DEGREES 58 MINUTES 09 SECONDS EAST ALONG SAID RIGHT OF WAY 108.35 FEET TO A CURVE, CONCAVE TO THE NORTHWEST, HAVING A RADIUS OF 315.00 FEET, A CHORD OF 285.34 FEET AND A CHORD BEARING OF NORTH 61 DEGREES 00 MINUTES 09 SECONDS EAST; THENCE ALONG SAID CURVE AND SAID RIGHT OF WAY 296.12 FEET TO A CURVE, CONCAVE TO THE SOUTHEAST, HAVING A RADIUS OF 385.00 FEET, A CHORD OF 82.42 FEET, AND A CHORD BEARING OF NORTH 40 DEGREES 15 MINUTES 11 SECONDS EAST; THENCE ALONG SAID CURVE AND SAID RIGHT OF WAY 82.58 FEET; THENCE NORTH 01 DEGREES 05 MINUTES 25 SECONDS WEST 324.66 FEET; THENCE SOUTH 87 DEGREES 58 MINUTES 15 SECONDS WEST 520.00 FEET TO THE WEST RIGHT OF WAY OF ROUTE 47; THENCE SOUTH 01 DEGREES 05 MINUTES 31 SECONDS EAST ALONG SAID RIGHT OF WAY 465.06 FEET TO THE POINT OF BEGINNING.

O'KEEFE SUBDIVISION UNIT ONE
 PER DOC. 200800012915

PLAN EDITION	DATE	DESCRIPTION
1	6/20/19	ISSUE FOR REVIEW
2	7/18/19	REV

CIVIL ENGINEERING SURVEYING

MGA

M GINGERICH GEREUX & ASSOCIATES
 Professional Design Firm License # 184.0050003
 P. 815-478-9680 www.mga2a.com F. 815-478-9685
 25620 S. GOUGAR RD. | MANHATTAN, IL. 60442

OWNER DATA
 Gas N Wash - Yorkville LLC
 8200 185th Street, Unit K
 Tinley Park, IL 60487

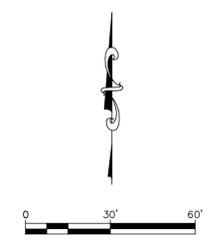
DESIGN:	BPH
DRAWING:	NB
CHECKED:	BPH/RFV
APPROVED:	BPH

GAS n WASH
 NE CORNER IL RTE 47 & WATERPARK WAY
 YORKVILLE, ILLINOIS

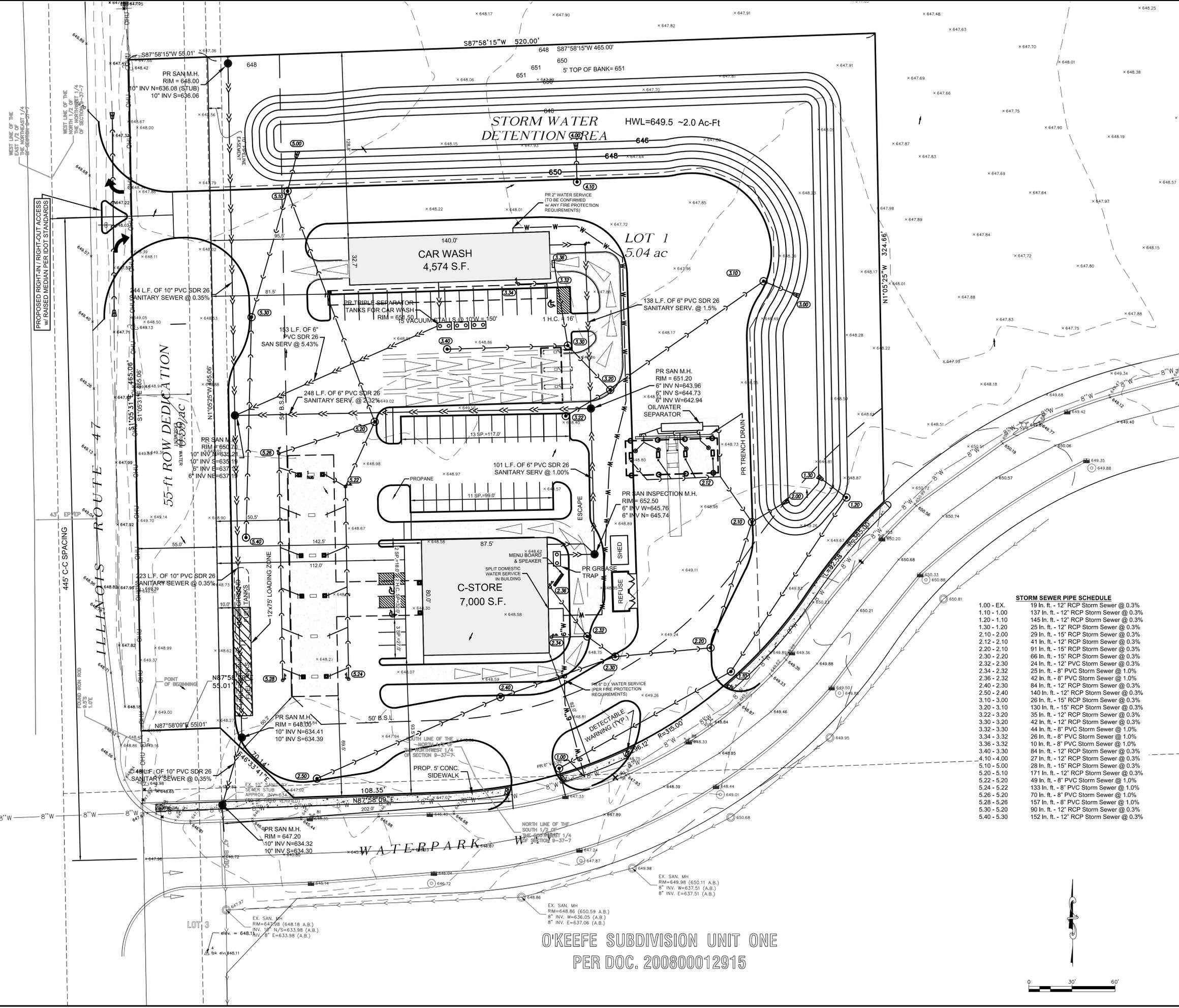
PRELIMINARY SITE PLAN (GEOMETRIC & GRADING)

SHEET NO.
1 OF **4**

JOB NO. 19-451
 © 2018 M. GINGERICH, GEREUX & ASSOCIATES



- 1.30 F.E.S.
12" INV SE=646.00
- 1.20 CB Ty A, 6" Dia. w/ Restrictor
w/ EJIW 1050 Fr / M1 Gr
RIM=650.00
12" INV NW=645.94
2.5" Restr. INV = 646.00
3.5" Restr. INV = 647.10
12" INV SW = 645.66
- 1.10 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=651.00
12" INV NE=645.22
12" INV SW=645.20
- 1.00 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.00
12" INV NE=644.78
12" INV S=644.76
- EX. EX. INLET - PROP. INV.
NORTH
12" INV N=644.70
EX. INV. S=644.64
- 2.50 INL A, 2" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.50
12" INV E=647.51
- 2.40 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.30
12" INV NE=647.09
12" INV E=647.07
- 2.36 D.S. CONNECTION
FIG = 653.00
12" INV E=647.07
- 2.34 D.S. CONNECTION
FIG = 653.00
12" INV S=646.99
- 2.32 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=651.50
12" INV N=646.91
12" INV S=646.89
- 2.30 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.15
12" INV N=646.81
12" INV S=646.79
- 2.20 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.10
15" INV SW=646.59
15" INV NE=646.57
- 2.12 TRENCH DRAIN
w/ EJIW V-7384 Gate or similar
RIM = 650.10
12" INV SE=646.24
- 2.10 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=649.50
12" INV NW=646.11
15" INV SW=646.11
15" INV NE=646.09
- 2.00 F.E.S.
15" INV SW=646.00
- 3.40 INL A, 2" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.10
12" INV E=646.79
- 3.36 TRENCH DRAIN
w/ EJIW V-7384 Gate or similar
RIM = 652.20
12" INV SE=647.28
- 3.34 D.S. CONNECTION
FIG = 652.90
12" INV E=647.33
- 3.32 CLEANOUT
RIM = 652.80
2.5" Restr. INV = 647.25
3.5" Restr. INV = 647.25
12" INV S=647.23
- 3.30 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=651.00
12" INV NE=646.79
12" INV S=646.77
- 3.22 INL A, 2" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.50
12" INV E=646.67
- 3.20 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.30
12" INV NW=646.56
12" INV SW=646.56
15" INV NE=646.54
- 3.10 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=649.20
15" INV SW=646.15
15" INV E=646.13
- 3.00 F.E.S.
15" INV E=646.00
- 4.10 INL A, 2" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.00
15" INV N=646.14
- 4.00 F.E.S.
12" INV S=646.00
- 5.40 INL A, 2" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.40
12" INV N=647.34
- 5.30 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.10
12" INV S=646.94
12" INV NE=646.92
- 5.28 CLEANOUT
RIM = 651.70
12" INV N=648.03
- 5.26 CLEANOUT
RIM = 651.40
12" INV S=647.24
12" INV NE=647.22
- 5.24 CLEANOUT
FIG = 652.10
12" INV N=647.81
- 5.22 CLEANOUT
RIM = 652.00
12" INV S=647.14
12" INV NE=647.12
- 5.20 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=651.20
8" INV SW=646.87
8" INV S=646.87
15" INV NW=646.85
- 5.10 MH Ty A, 4" Dia
w/ EJIW 1050 Fr / M1 Gr
RIM=650.00
12" INV SW=646.16
15" INV N=646.14
- 5.00 F.E.S.
12" INV S=646.00



PLAN EDITION MILESTONES	
DATE	DESCRIPTION
6/20/19	ISSUE FOR REVIEW
7/18/19	REV
	CITY COMMENTS

CIVIL ENGINEERING SURVEYING

MGA

M GINGERICH GEREUX & ASSOCIATES
Professional Design Firm License # 184.0050003
P. 815-478-9680 www.mg2a.com F. 815-478-9685
25620 S. GOUGAR RD. | MANHATTAN, IL. 60442

OWNER DATA
Gas N Wash - Yorkville LLC
8200 185th Street, Unit K
Tinley Park, IL 60487

DESIGN:	BPH
DRAWING:	NB
CHECKED:	BPH/RFV
APPROVED:	BPH

GAS n WASH
NE CORNER IL RTE 47 & WATERPARK WAY
YORKVILLE, ILLINOIS

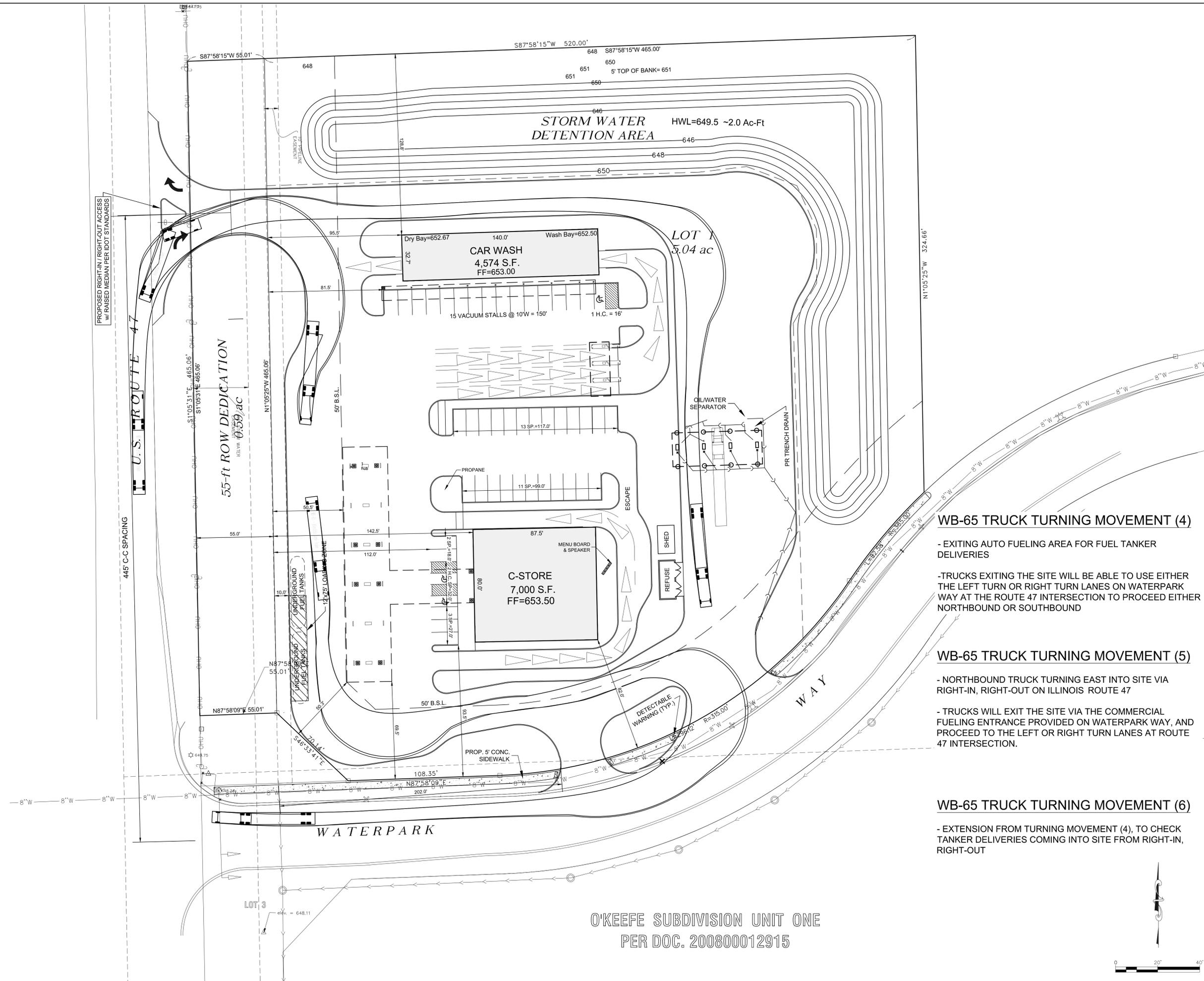
PRELIMINARY SITE PLAN (UTILITY & STORMWATER)

SHEET NO.
2 OF **4**

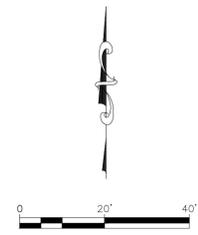
JOB NO. 19-451
© 2018 M. GINGERICH, GEREUX & ASSOCIATES

O'KEEFE SUBDIVISION UNIT ONE
PER DOC. 200800012915





O'KEEFE SUBDIVISION UNIT ONE
PER DOC. 200800012915



PLAN EDITION MILESTONES	
DATE	DESCRIPTION
6/20/19	ISSUE FOR REVIEW
7/18/19	REV
	CITY COMMENTS

MGA CIVIL ENGINEERING SURVEYING

M GINGERICH GEREUX & ASSOCIATES
Professional Design Firm License # 184.005003
P. 815-478-9680 www.mg2a.com F. 815-478-9685
25620 S. GOUGAR RD. | MANHATTAN, IL. 60442

OWNER DATA
Gas N Wash - Yorkville LLC
8200 185th Street, Unit K
Tinley Park, IL 60487

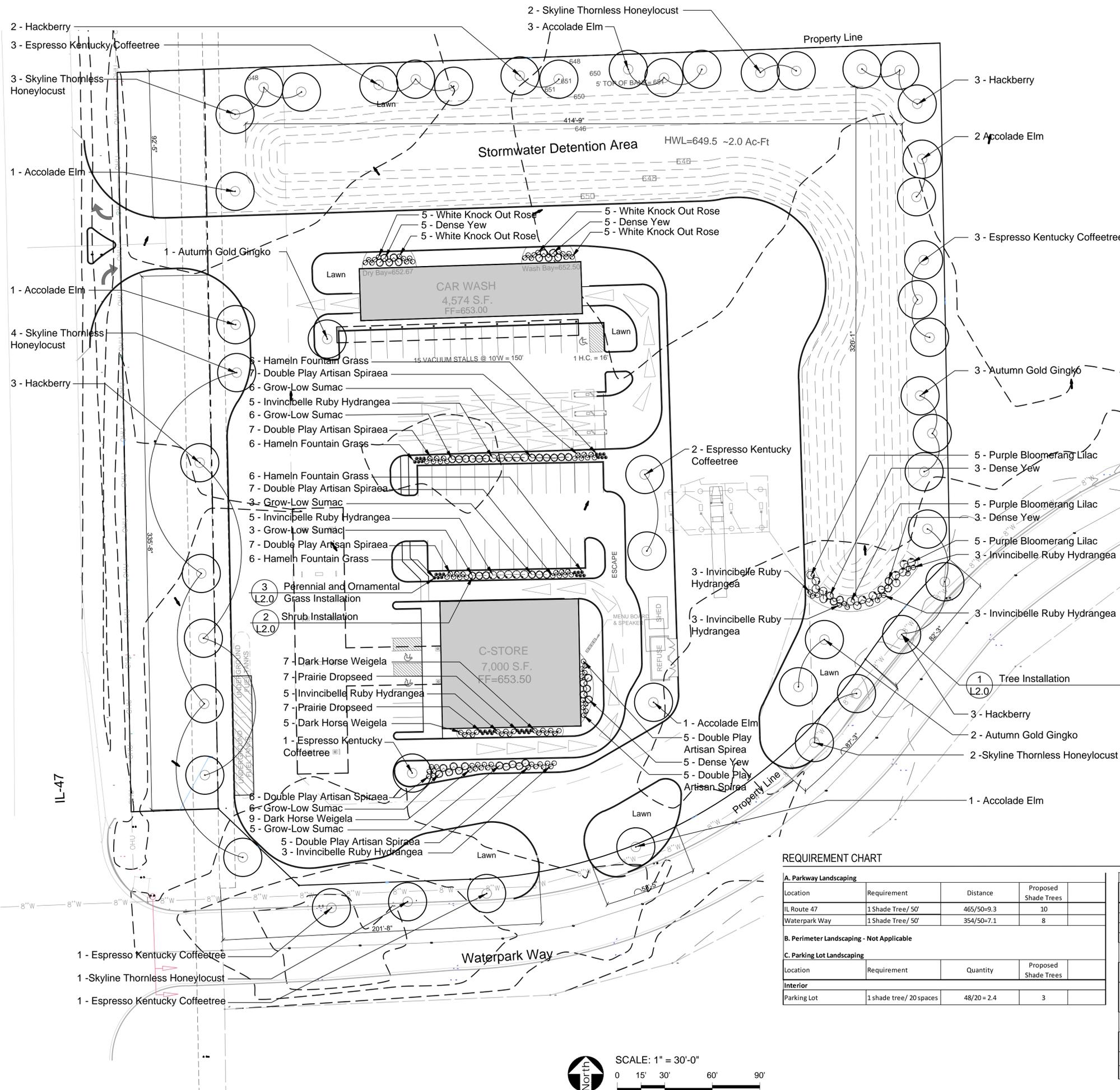
DESIGN:	BPH
DRAWING:	NB
CHECKED:	BPH
APPROVED:	BPH

GAS N WASH
YORKVILLE, ILLINOIS

TRUCK TURNING PLAN

SHEET NO.
4 OF **4**

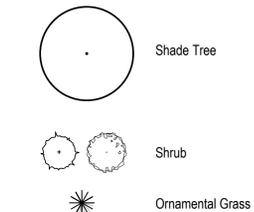
JOB NO. 19-451
© 2018 M. GINGERICH, GEREUX & ASSOCIATES



GENERAL NOTES: LANDSCAPE

- Notes indicated on grading plans shall pertain to landscape plans. Final grade of planting beds shall be as per grading plan.
- The landscape contractor shall be responsible for making themselves familiar with all underground utilities and structures.
- All existing plant material and trees shall be saved and protected unless otherwise noted. Contractor to protect new and existing trees and landscaping from damage and shall restore all areas disturbed as a result of construction.
- Plant material shall be supplied from Northern Illinois nursery stock, shall be dug the planting season it is installed, and shall conform to the American Association of Nurseryman's standards.
- Plant material shall be size and type specified. Substitution of plant material shall be on a case by case basis and approved in writing by the Owner's Representative. In no case shall plant material be smaller than indicated in the plans.
- Do not willfully proceed with plantings as designed when it is obvious that obstructions and/or grade differences exist that may not have been known during the design process. Such conditions shall be immediately brought to the attention of the Owner's Representative.
- All plant material shall be inspected and approved by the Owner's Representative prior to the installation of any and all plant material.
- Plant locations shall be flagged in field with Owner's Rep. Final location of all plant material shall be subject to approval of the Owner's Representative prior to digging any holes. The landscape contractor is responsible for providing Owner's Representative with 48 hour minimum advance notice prior to planting.
- Plants shall be watered on the day they are planted and maintained with watering until final acceptance of the project.
- Apply a pre-emergent as per manufacturer's specification prior to installing mulch.
- Beds and tree rings (6' diameter) shall have 3" of hardwood shredded mulch applied and a 4" deep spade edge at lawn. Trees that are not located in beds, shall have a tree ring.
- Landscape plant material shall be guaranteed for 12 months from final acceptance. Any plant 1/3 dead or more shall be replaced under the guarantee.
- Contractor to prepare landscape beds by roto-tilling 2" of Mushroom Compost into new beds. Do not add compost nor roto-till within drip line of existing trees.
- Lawn Seeding shall be under favorable weather conditions, and shall follow dates in specification.
- Turf mixes shall be installed and lawn established at all disturbed areas.
- Do not overseed into mulch beds and paving.
- Contractor shall restore all areas disturbed as a result of construction.

LEGEND



PLANT LIST

Shade Trees - Balled and Burlap			
11	2.5' cal	<i>Celtis occidentalis</i>	Hackberry
6	2.5' cal	<i>Ginkgo biloba 'Autumn Gold'</i>	Autumn Gold Ginkgo
12	2.5' cal	<i>Gleditsia triacanthos var. inermis 'Skycole'</i>	Skyline Honeylocust
11	2.5' cal	<i>Gymnocladus dioica 'Espresso'</i>	Espresso Kentucky Coffeetree
9	2.5' cal	<i>Ulmus 'Morton'</i>	Accolade Elm
49	Total		

Evergreen Shrubs - Balled and Burlap or Pot			
21	24" Ht. x 24" Spr.	<i>Taxus x media 'Densaformis'</i>	Dense Yew
21	Total		

Deciduous Shrubs - Balled and Burlap or Pot			
30	24" Ht. x 24" Spr.	<i>Hydrangea arborescens 'NCHA3'</i>	Invincibelle Ruby Hydrangea
29	24" Ht. x 36" Spr.	<i>Rhus aromatica 'Grow-Low'</i>	Grow-Low Sumac
20	24" Ht. x 24" Spr.	<i>Rosa 'Radwile'</i>	White Knock Out Rose
49	24" Ht. x 24" Spr.	<i>Spiraea japonica 'Galen'</i>	Spiraea Double Play Artisan
15	36" Ht. x 24" Spr.	<i>Syringa x 'Penda'</i>	Purple Bloomerang Lilac
21	18" Ht. x 24" Spr.	<i>Weigela x 'Dark Horse'</i>	Dark Horse Weigela
164	Total		

Perennials, Ornamental Grasses, and Groundcovers			
24	#/cont.	<i>Panicum alopurpureoides 'Hameln'</i>	Hameln Fountain Grass
14	#/cont.	<i>Sporobolus heterolepis</i>	Prairie Dropseed
38	Total		

REQUIREMENT CHART

A. Parkway Landscaping			
Location	Requirement	Distance	Proposed Shade Trees
IL Route 47	1 Shade Tree / 50'	465/50=9.3	10
Waterpark Way	1 Shade Tree / 50'	354/50=7.1	8

B. Perimeter Landscaping - Not Applicable			
Location	Requirement	Quantity	Proposed Shade Trees

C. Parking Lot Landscaping			
Location	Requirement	Quantity	Proposed Shade Trees
Interior			
Parking Lot	1 shade tree / 20 spaces	48/20 = 2.4	3

Perimeter Parking Lot Landscape				
Location	Requirement	Quantity	Proposed Shade Trees	Proposed Shrubs
North	N/A	N/A	N/A	N/A
South - Waterpark Way	N/A	N/A	N/A	N/A
East - Adjacent Property	N/A	N/A	N/A	N/A
West - IL Route 47	N/A	N/A	N/A	N/A

Lot Landscaping				
Location	Requirement	Distance	Proposed Shade Trees	Proposed Shrubs
Entire Lot	2 trees & 15 shrubs / 20,000 sf of lot	219,542/20000=10.9 10.9*2=21.8 10.9*15=163.5	49	164

Stormwater Detention Landscaping				
Location	Requirement	Distance	Proposed Shade Trees	Proposed Shrubs
North Property Edge	1 tree / 30 LF	414.75/30=13.83	14	
East Property Edge	1 tree / 30 LF	320.08/30 = 10.67	11	

PROJECT
Gas N Wash
Yorkville, Illinois

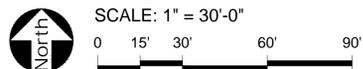


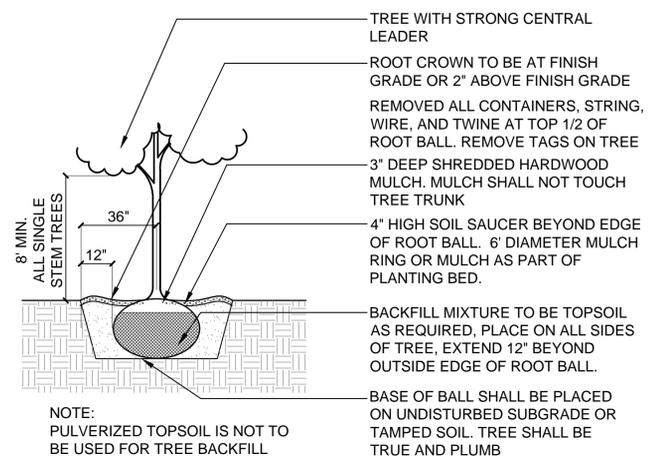
SHEET TITLE
Landscape Plan

SHEET NUMBER L1.0

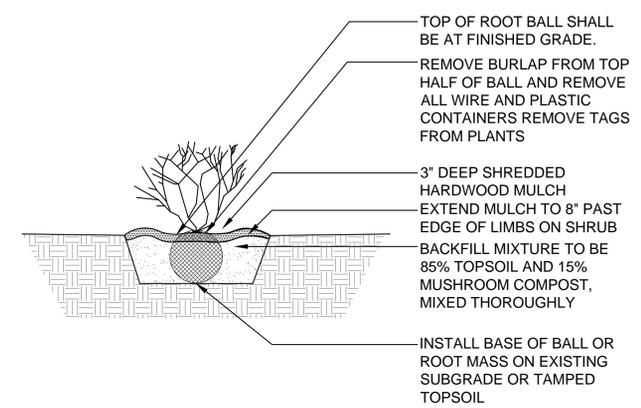
DRAW / REVISION

LD/VT	Review	21JUN2019

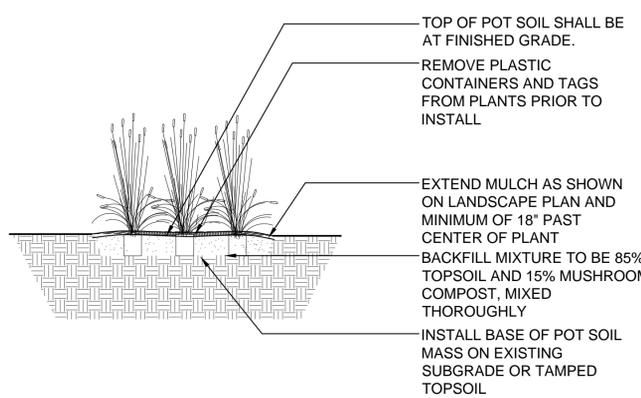




1 Tree Installation
SCALE: N.T.S. d-plant-tree_12



2 Shrub Installation
SCALE: N.T.S. d-plant-shrub_12



3 Perennial & Ornamental Grass Installation
SCALE: 1" = 1'-0" d-perennials

PROJECT
Gas N Wash
Yorkville, Illinois

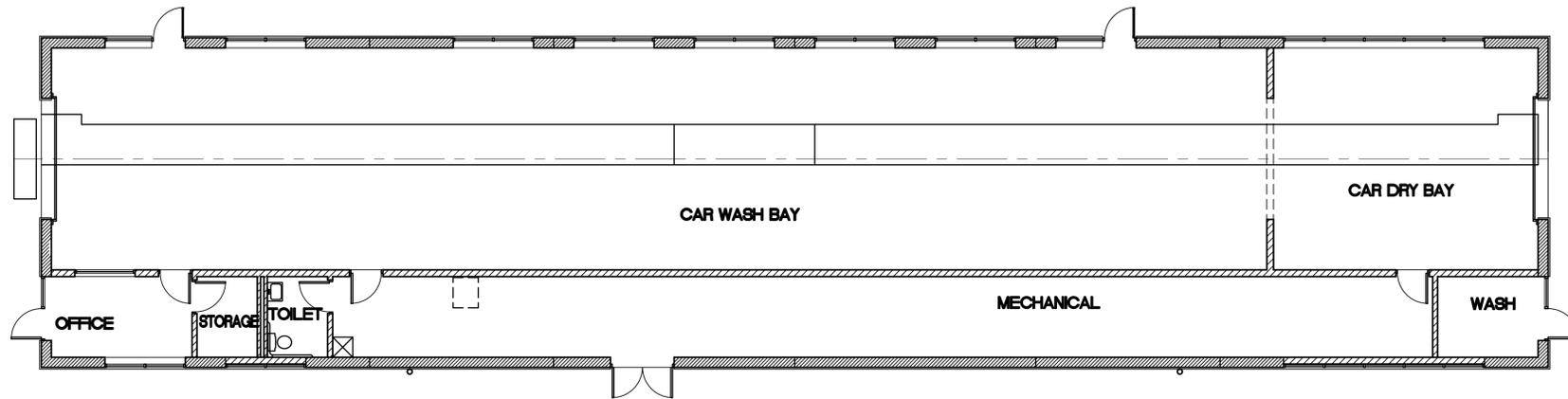


SHEET TITLE
Landscape Plan

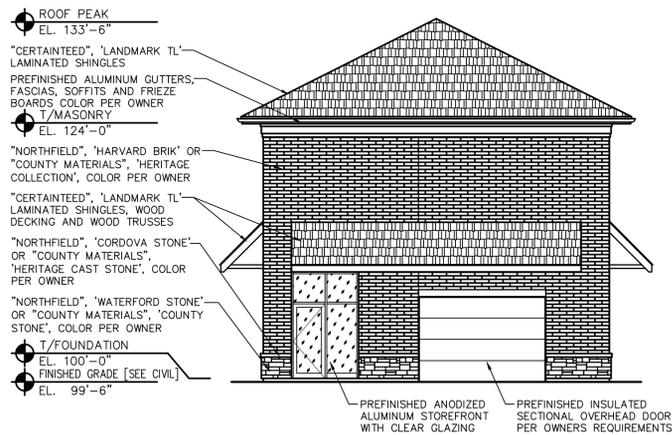
SHEET NUMBER **L2.0**

DRAW / REVISION

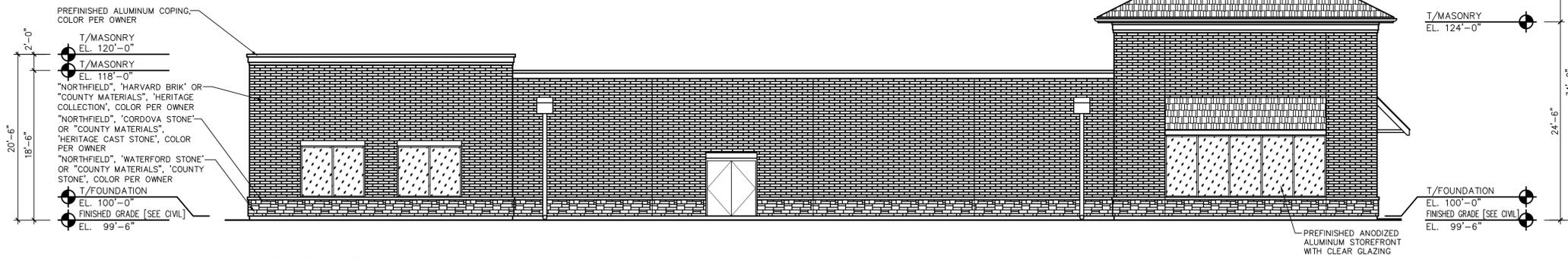
LD/VT	Review	21JUN2019



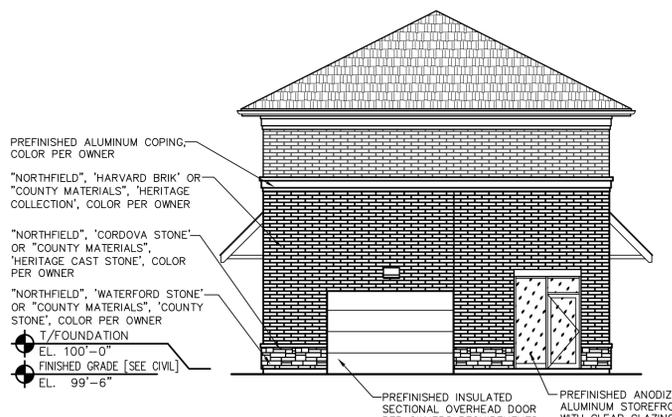
CARWASH FLOOR PLAN
SCALE: 1/8" = 1'-0"



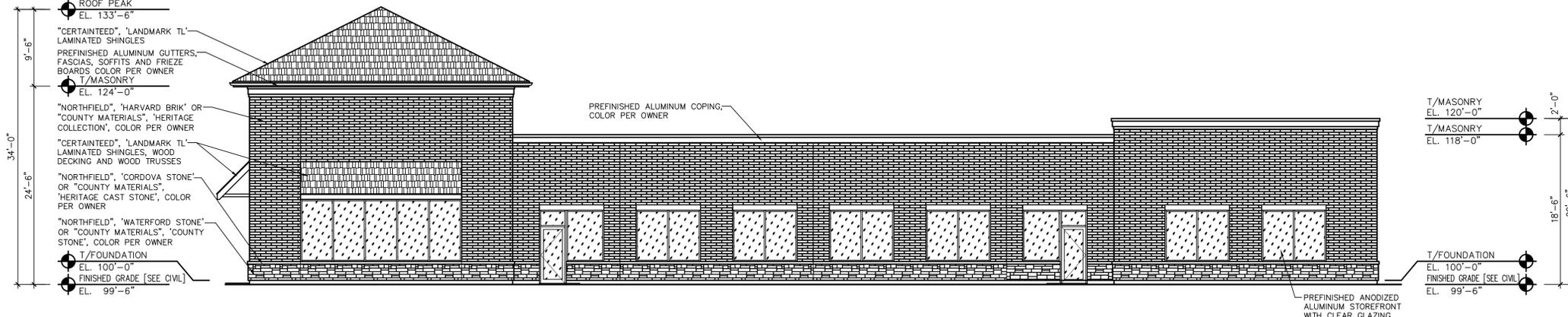
CARWASH WEST ELEVATION
SCALE: 1/8" = 1'-0"



CARWASH NORTH ELEVATION
SCALE: 1/8" = 1'-0"



CARWASH EAST ELEVATION



CARWASH SOUTH ELEVATION
SCALE: 1/8" = 1'-0"

GAS AND WASH

NEC STATE ROUTE 47 AND WATERPARK WAY
YORKVILLE, ILLINOIS

SIGN
A



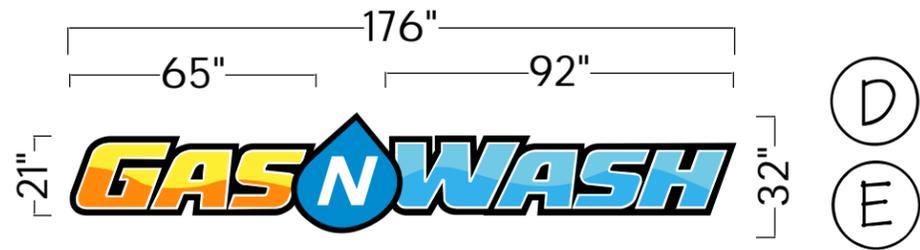
13401 SOUTHWEST HWY.,
ORLAND PARK, ILLINOIS
708-448-0826 FAX 708-448-9092

Notes _____

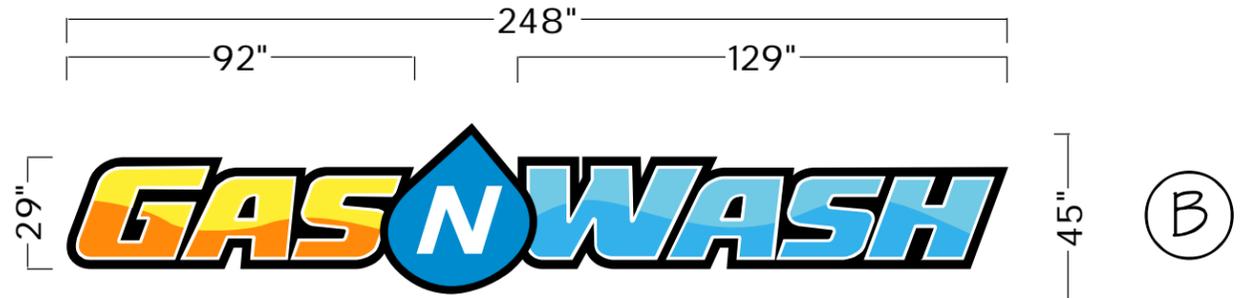
Approved _____ Date _____

© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
This rendering is the exclusive property of Van Bruggen Signs, Inc.,
for the sole purpose of consideration to purchase a sign or design from
Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
in a compensation fee of \$500.00 for the time and effort entailed in
creating these drawings.

Scale	1/2"	Title	GAS N WASH - YORKVILLE			
Date	4-18-19	Description	MONUMENT SIGN 16"			
Drawn By	ED	Revisions By	ED		Drawing No.	19-109.2CA
		Date	6-13-19			



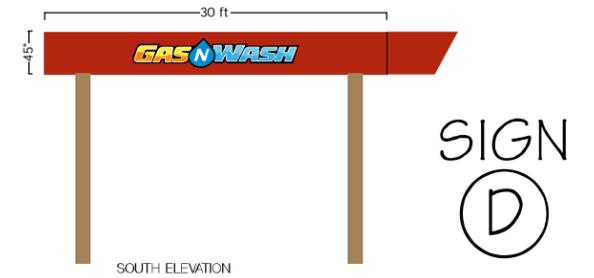
1/4" SCALE (x2 SETS) ILLUMINATED CHANNEL LETTERS - 28 SQ. FT.



1/4" SCALE ILLUMINATED CHANNEL LETTERS - 57.5 SQ. FT.

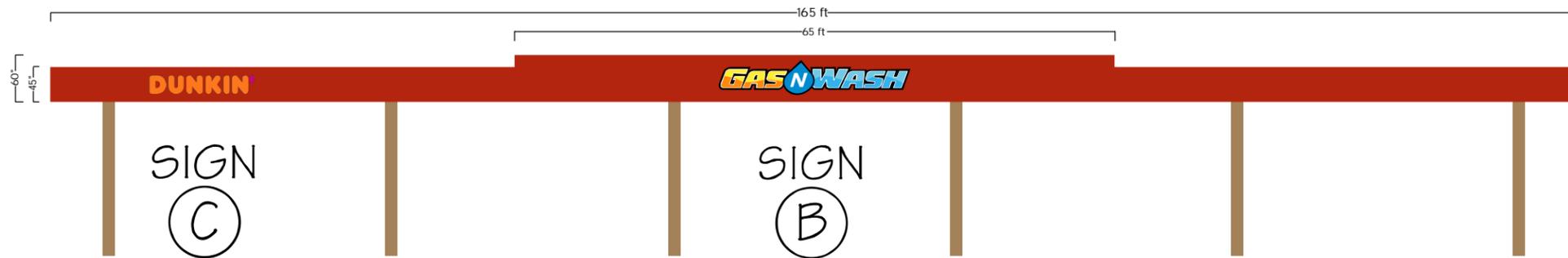


1/4" SCALE ILLUMINATED CHANNEL LETTERS - 23 SQ. FT.

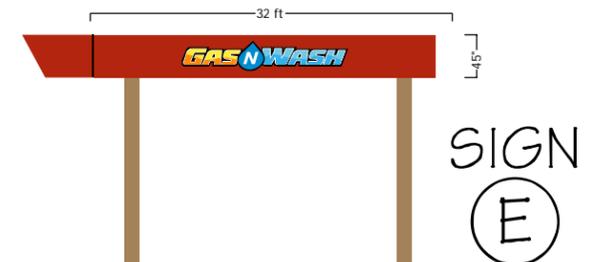


SOUTH ELEVATION

SIGN
D



WEST ELEVATION 1/16" SCALE



NORTH ELEVATION 1/16" SCALE

SIGN
E



13401 SOUTHWEST HWY.,
ORLAND PARK, ILLINOIS

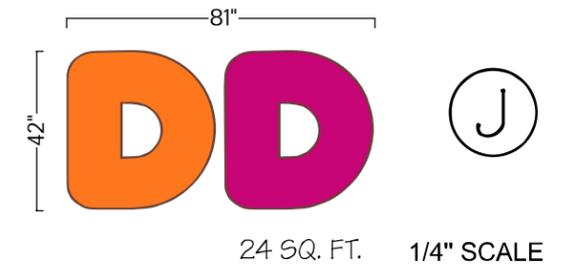
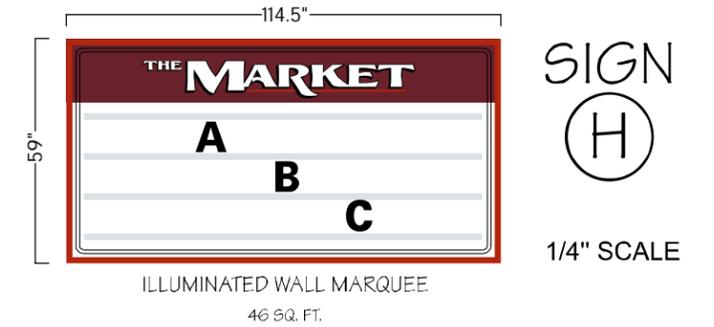
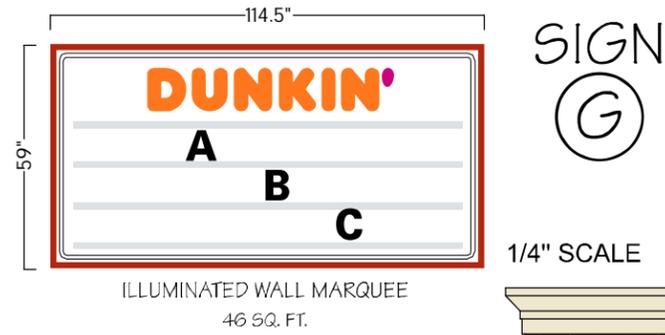
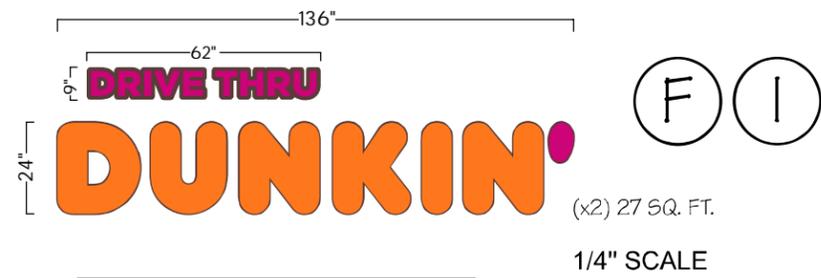
708-448-0826 FAX 708-448-9092

Notes _____

Approved _____ Date _____

© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
This rendering is the exclusive property of Van Bruggen Signs, Inc.,
for the sole purpose of consideration to purchase a sign or design from
Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
in a compensation fee of \$500.00 for the time and effort entailed in
creating these drawings.

Scale	NOTED	Title				GAS N WASH - YORKVILLE			
Date	4-23-19	Description				CAR CANOPY			
Drawn By	ED	Revisions By	ED			Drawing No.	19-109.6C		
		Date	6-13-19						



VanBruggen SIGNS
INC. SINCE 1925
13401 SOUTHWEST HWY.,
ORLAND PARK, ILLINOIS
708-448-0826 FAX 708-448-9092

Notes _____

Approved _____ Date _____
© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
This rendering is the exclusive property of Van Bruggen Signs, Inc.,
for the sole purpose of consideration to purchase a sign or design from
Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
in a compensation fee of \$500.00 for the time and effort entailed in
creating these drawings.

Scale	NOTED	Title	GAS N WASH - YORKVILLE				
Date	4-22-19	Description	C-STORE SIGNAGE				
Drawn By	ED	Revisions By	ED			Drawing No.	19-109.3C
		Date	6-13-19				

SIGN
 (K)



SINGLE FACE ILLUMINATED CANOPY SIGN
 WEST ELEVATION
 35.7 SQ. FT.



13401 SOUTHWEST HWY.,
 ORLAND PARK, ILLINOIS

708-448-0826 FAX 708-448-9092

Notes _____

Approved _____ Date _____

© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
 This rendering is the exclusive property of Van Bruggen Signs, Inc.,
 for the sole purpose of consideration to purchase a sign or design from
 Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
 in a compensation fee of \$500.00 for the time and effort entailed in
 creating these drawings.

Scale	3/8"	Title	GAS N WASH - YORKVILLE		
Date	4-23-19	Description	CAR WASH ENTRANCE		
Drawn By	ED	Revisions By	ED	_____	Drawing No.
		Date	6-13-19	_____	19-109.7C

215.5"
CAR WASH
 25"
 27.5"

1/4" SCALE ILLUMINATED CHANNEL LETTERS - 41 SQ. FT.

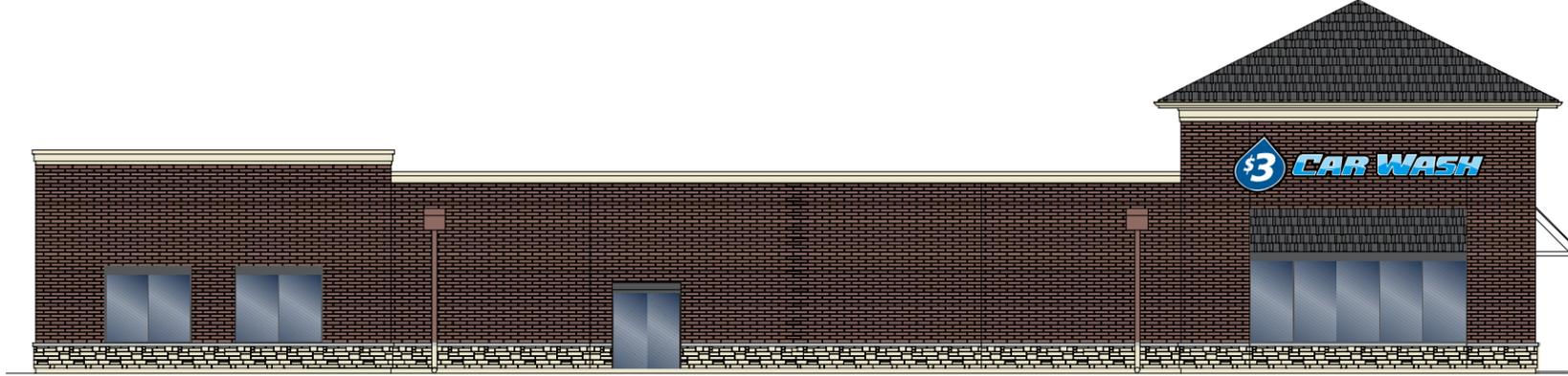
(L)

60.5" (M)
 215.5"
 65.5"
\$3 CAR WASH
 25"
 27.5"
 1/4" SCALE ILLUMINATED CHANNEL LETTERS - 68 SQ. FT.

SIGN
 (L)

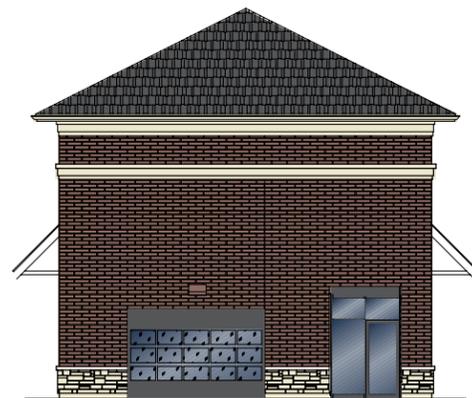


CARWASH WEST ELEVATION



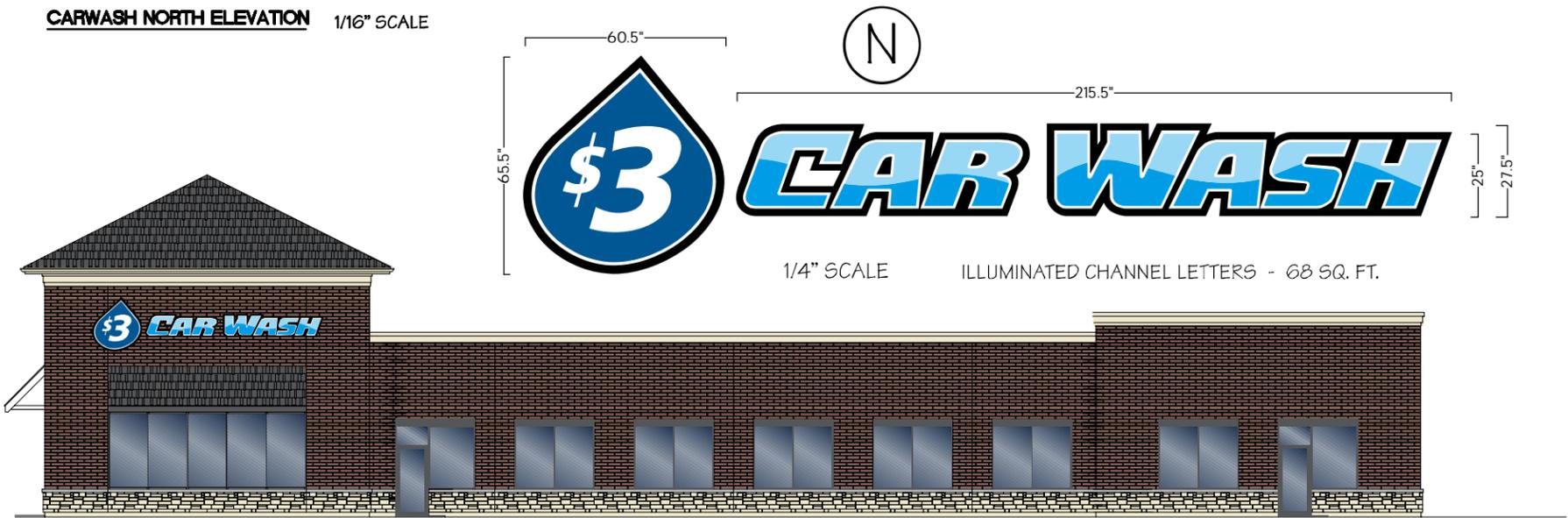
CARWASH NORTH ELEVATION 1/16" SCALE

SIGN
 (M)



CARWASH EAST ELEVATION

SIGN
 (N)



CARWASH SOUTH ELEVATION 1/16" SCALE



13401 SOUTHWEST HWY.,
 ORLAND PARK, ILLINOIS

708-448-0826 FAX 708-448-9092

Notes _____

Approved _____ Date _____

© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
 This rendering is the exclusive property of Van Bruggen Signs, Inc.,
 for the sole purpose of consideration to purchase a sign or design from
 Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
 in a compensation fee of \$500.00 for the time and effort entailed in
 creating these drawings.

Scale	NOTED	Title	GAS N WASH - YORKVILLE				
Date	4-22-19	Description	CAR WASH BUILDING SIGNAGE				
Drawn By	ED	Revisions By	ED			Drawing No.	19-109.4C
		Date	6-13-19				

0

141"

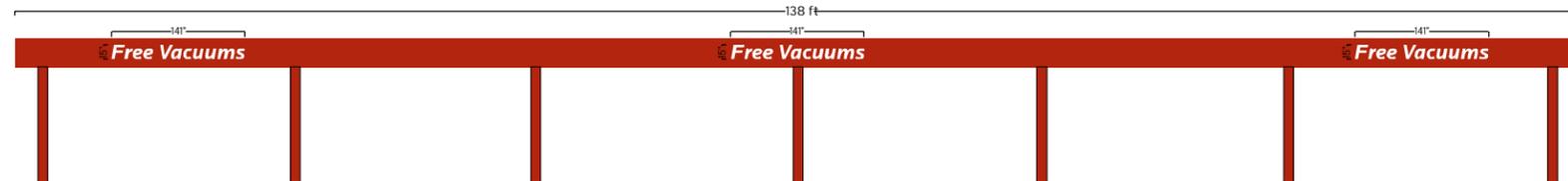
15"

Free Vacuums

1/2" SCALE

SIGN

0



SOUTH ELEVATION - VACUUM CANOPY - YORKVILLE 1/16" SCALE



13401 SOUTHWEST HWY.,
ORLAND PARK, ILLINOIS

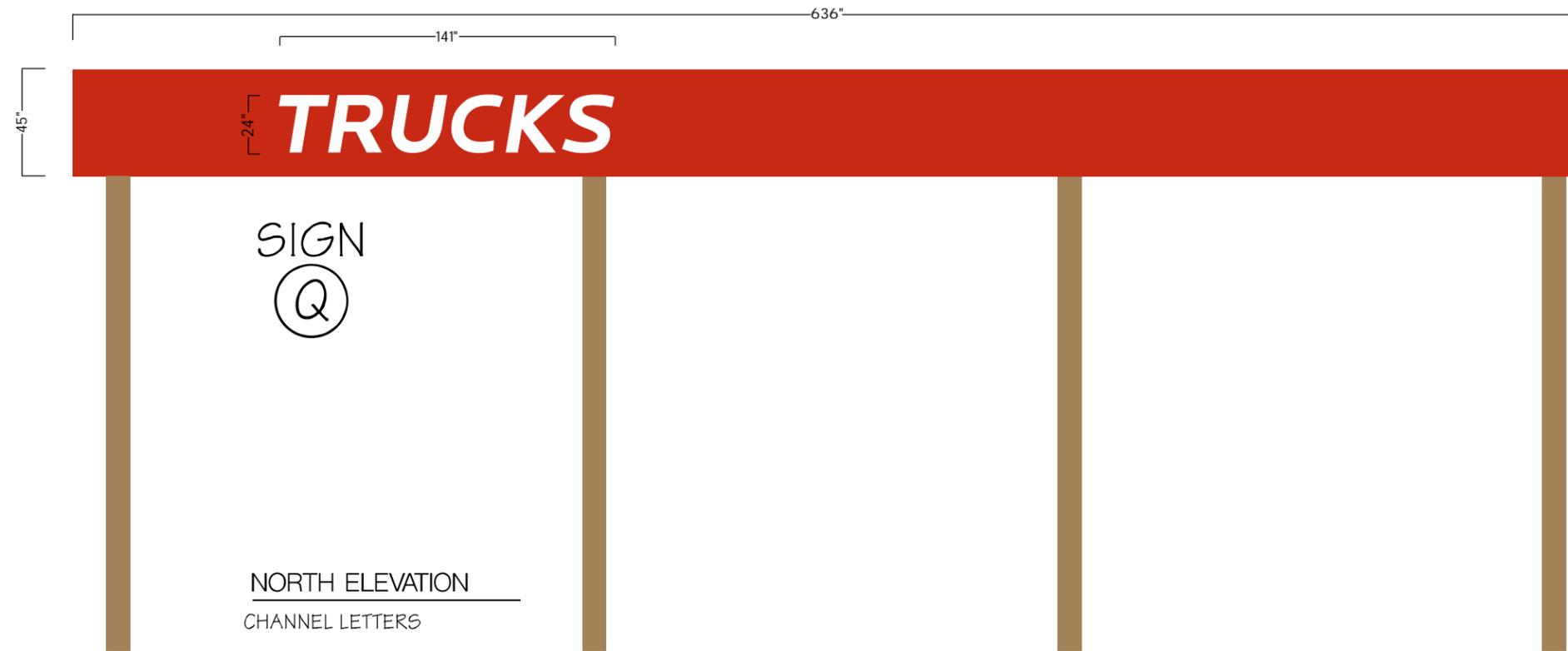
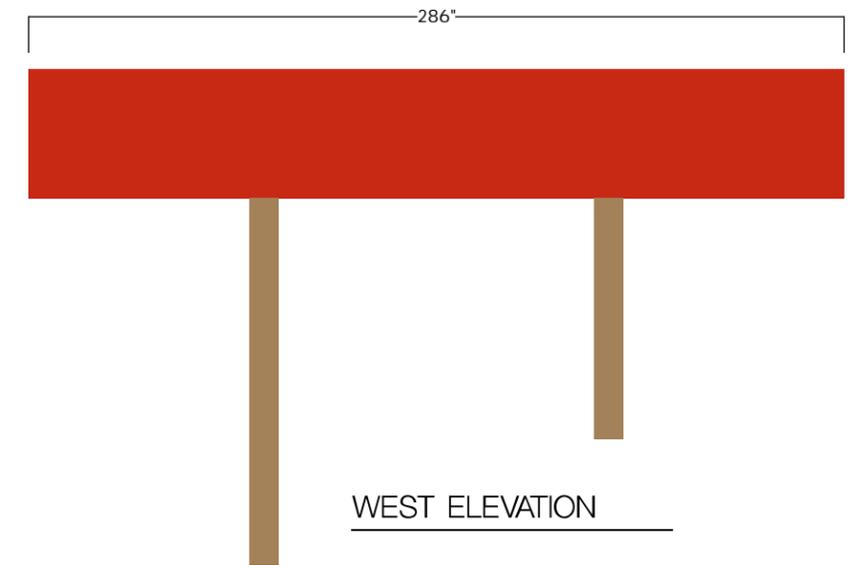
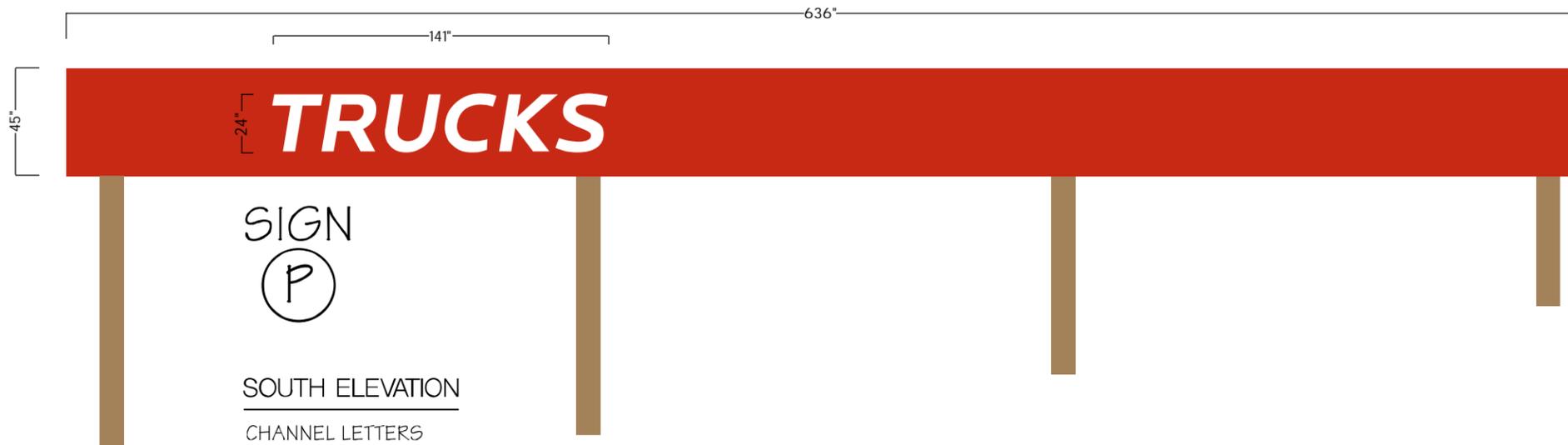
708-448-0826 FAX 708-448-9092

Notes _____

Approved _____ Date _____

© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
 This rendering is the exclusive property of Van Bruggen Signs, Inc.,
 for the sole purpose of consideration to purchase a sign or design from
 Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
 in a compensation fee of \$500.00 for the time and effort entailed in
 creating these drawings.

Scale	NOTED	Title	GAS N WASH - YORKVILLE			
Date	4-23-19	Description	VACUUM CANOPY			
Drawn By	ED	Revisions By	ED			Drawing No.
		Date	6-13-19			19-020.8C



13401 SOUTHWEST HWY.,
ORLAND PARK, ILLINOIS

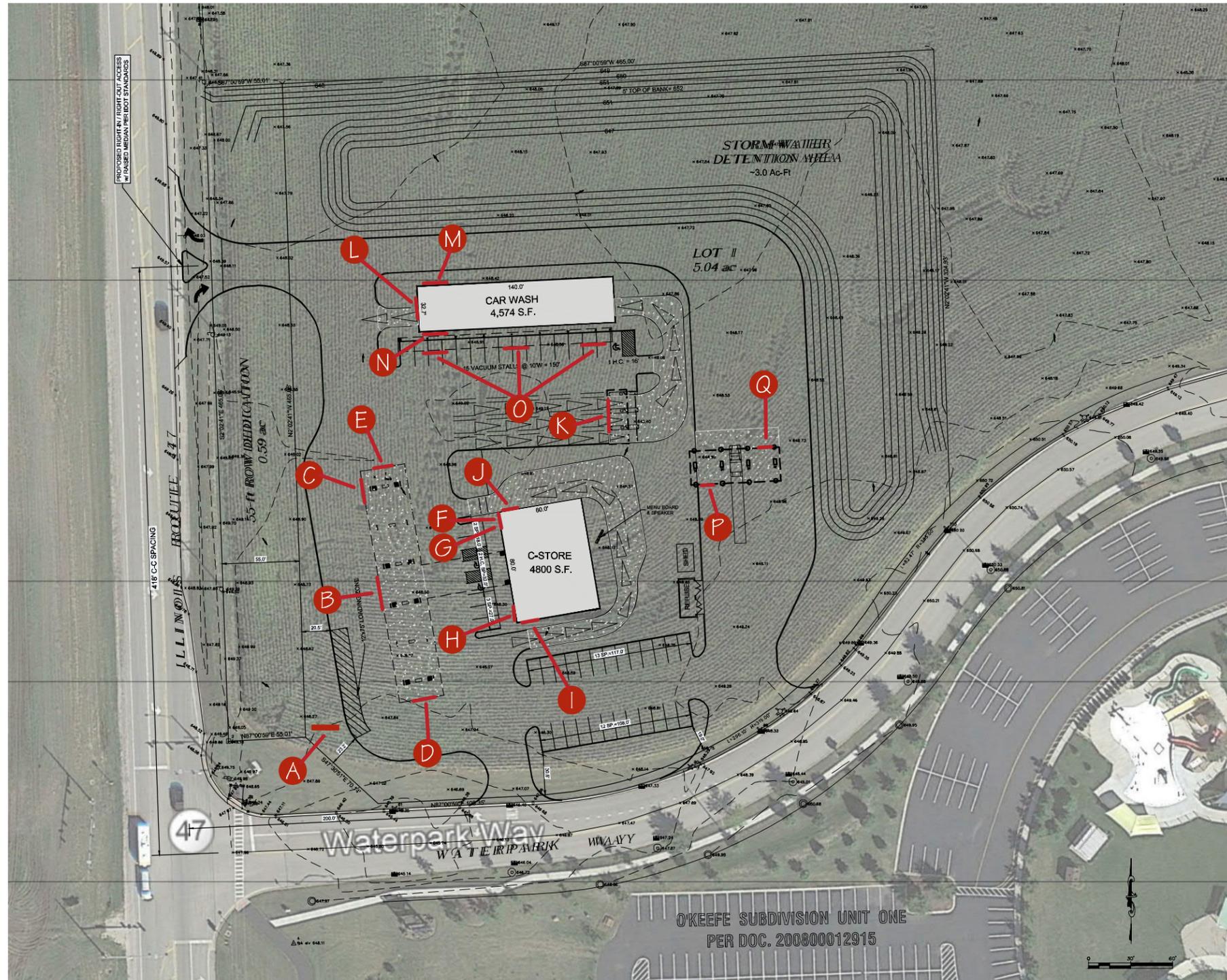
708-448-0826 FAX 708-448-9092

Notes _____

Approved _____ Date _____

© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
 This rendering is the exclusive property of Van Bruggen Signs, Inc.,
 for the sole purpose of consideration to purchase a sign or design from
 Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
 in a compensation fee of \$500.00 for the time and effort entailed in
 creating these drawings.

Scale	3/16"	Title	GAS N WASH - YORKVILLE				
Date	4-23-19	Description	TRUCK CANOPY				
Drawn By	ED	Revisions By	ED			Drawing No.	19-109.5C
		Date	6-13-19				



13401 SOUTHWEST HWY.,
ORLAND PARK, ILLINOIS

708-448-0826 FAX 708-448-9092

Notes _____

Approved _____ Date _____

© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
 This rendering is the exclusive property of Van Bruggen Signs, Inc.,
 for the sole purpose of consideration to purchase a sign or design from
 Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
 in a compensation fee of \$500.00 for the time and effort entailed in
 creating these drawings.

Scale	NTS	Title	GAS N WASH - YORKVILLE		
Date	6-13-19	Description	SITE PLAN		
Drawn By	ED	Revisions By			Drawing No.
		Date			19-109.9C

SIGN
A



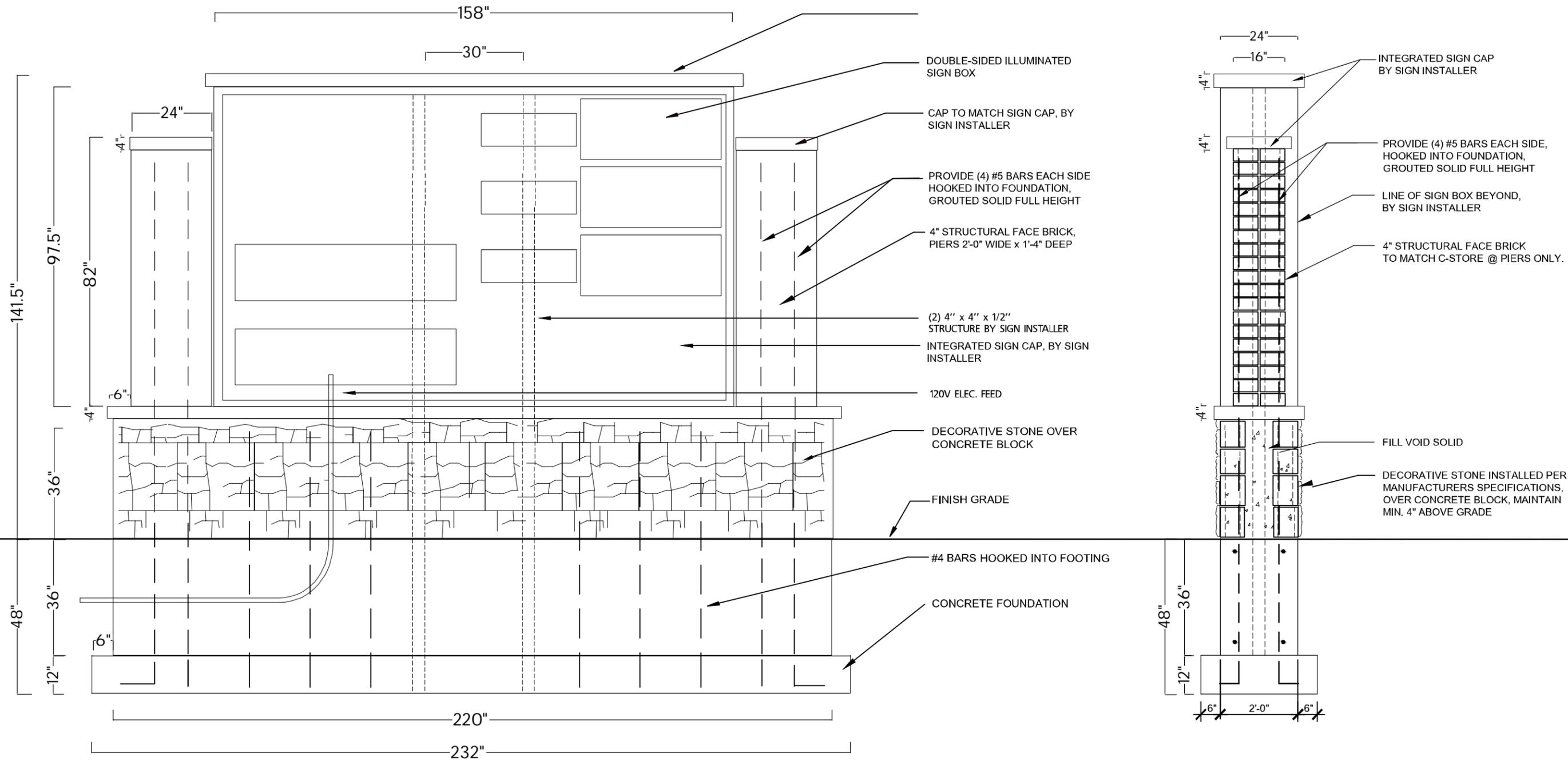
13401 SOUTHWEST HWY.,
ORLAND PARK, ILLINOIS
708-448-0826 FAX 708-448-9092

Notes _____

Approved _____ Date _____

© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
This rendering is the exclusive property of Van Bruggen Signs, Inc.,
for the sole purpose of consideration to purchase a sign or design from
Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
in a compensation fee of \$500.00 for the time and effort entailed in
creating these drawings.

Scale	1/2"	Title	GAS N WASH - YORKVILLE			
Date	4-18-19	Description	MONUMENT SIGN 16"			
Drawn By	ED	Revisions By	ED		Drawing No.	19-109.2CA
		Date	6-13-19			



13401 SOUTHWEST HWY.,
ORLAND PARK, ILLINOIS

708-448-0826 FAX 708-448-9092

Notes _____

Approved _____ Date _____

© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
 This rendering is the exclusive property of Van Bruggen Signs, Inc.,
 for the sole purpose of consideration to purchase a sign or design from
 Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
 in a compensation fee of \$500.00 for the time and effort entailed in
 creating these drawings.

Scale	3/8"	Title	GAS N WASH - YORKVILLE		
Date	4-18-19	Description	MONUMENT SIGNS FOUNDATION		
Drawn By	ED	Revisions By	ED		Drawing No.
		Date	6-13-19		19-109.2C



13401 SOUTHWEST HWY.,
ORLAND PARK, ILLINOIS

708-448-0826 FAX 708-448-9092

Notes _____

Approved _____ Date _____

© COPYRIGHT 2018 by Van Bruggen Signs, Inc., Orland Park, IL.
 This rendering is the exclusive property of Van Bruggen Signs, Inc.,
 for the sole purpose of consideration to purchase a sign or design from
 Van Bruggen Signs, Inc. Any unauthorized use of this drawing will result
 in a compensation fee of \$500.00 for the time and effort entailed in
 creating these drawings.

Scale	NTS	Title	GAS N WASH - YORKVILLE		
Date	6-13-19	Description	MONUMENT SIGN - SITE PLAN		
Drawn By	ED	Revisions By			Drawing No.
		Date			19-109.9C



Yorkville IL

Jul 17, 2019





Yorkville IL

Jul 17, 2019

1411 W. Peterson Avenue, Suite 203 • Park Ridge, IL 60068 • tel 847-698-4438 / fax 847-698-9889





Yorkville IL

Jul 17, 2019

1411 W. Peterson Avenue, Suite 203 • Park Ridge, IL 60068 • tel 847-698-4438 / fax 847-698-9889





Yorkville IL

Jul 17, 2019

1411 W. Peterson Avenue, Suite 203 • Park Ridge, IL 60068 • tel 847-698-4438 / fax 847-698-9889





Yorkville IL

Jul 17, 2019

1411 W. Peterson Avenue, Suite 203 • Park Ridge, IL 60068 • tel 847-698-4438 / fax 847-698-9889





Yorkville IL

Jul 17, 2019

1411 W. Peterson Avenue, Suite 203 • Park Ridge, IL 60068 • tel 847-698-4438 / fax 847-698-9889





Yorkville IL

Jul 17, 2019

1411 W. Peterson Avenue, Suite 203 • Park Ridge, IL 60068 • tel 847-698-4438 / fax 847-698-9889





Yorkville IL

Jul 17, 2019

1411 W. Peterson Avenue, Suite 203 • Park Ridge, IL 60068 • tel 847-698-4438 / fax 847-698-9889





Reviewed By:	
Legal	<input checked="" type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input checked="" type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #6

Tracking Number

EDC 2019-64

Agenda Item Summary Memo

Title: 208 E. Main Street – Variance Request

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: Reduction in front yard setback from 30 feet to 20 feet for the purpose of rebuilding a previously existing porch.

Council Action Previously Taken:

Date of Action: _____ Action Taken: _____

Item Number: _____

Type of Vote Required: Majority

Council Action Requested: Vote

Submitted by: Krysti Barksdale-Noble, AICP Community Development
Name Department

Agenda Item Notes:



Memorandum

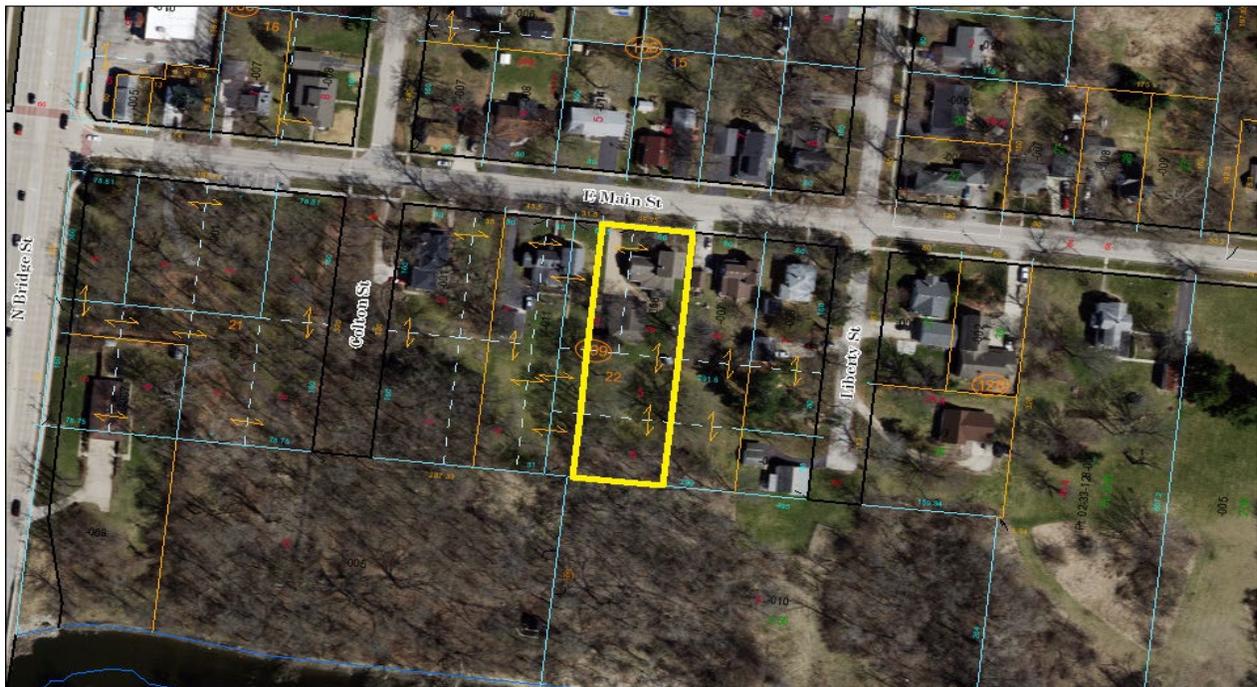
To: Economic Development Committee
From: Krysti Barksdale-Noble, Community Development Director
CC: Bart Olson, City Administrator
Jason Engberg, Senior Planner
Date: July 10, 2019
Subject: **PZC 2019-20 208 E. Main Street – Front Yard Setback Variance**

SUMMARY:

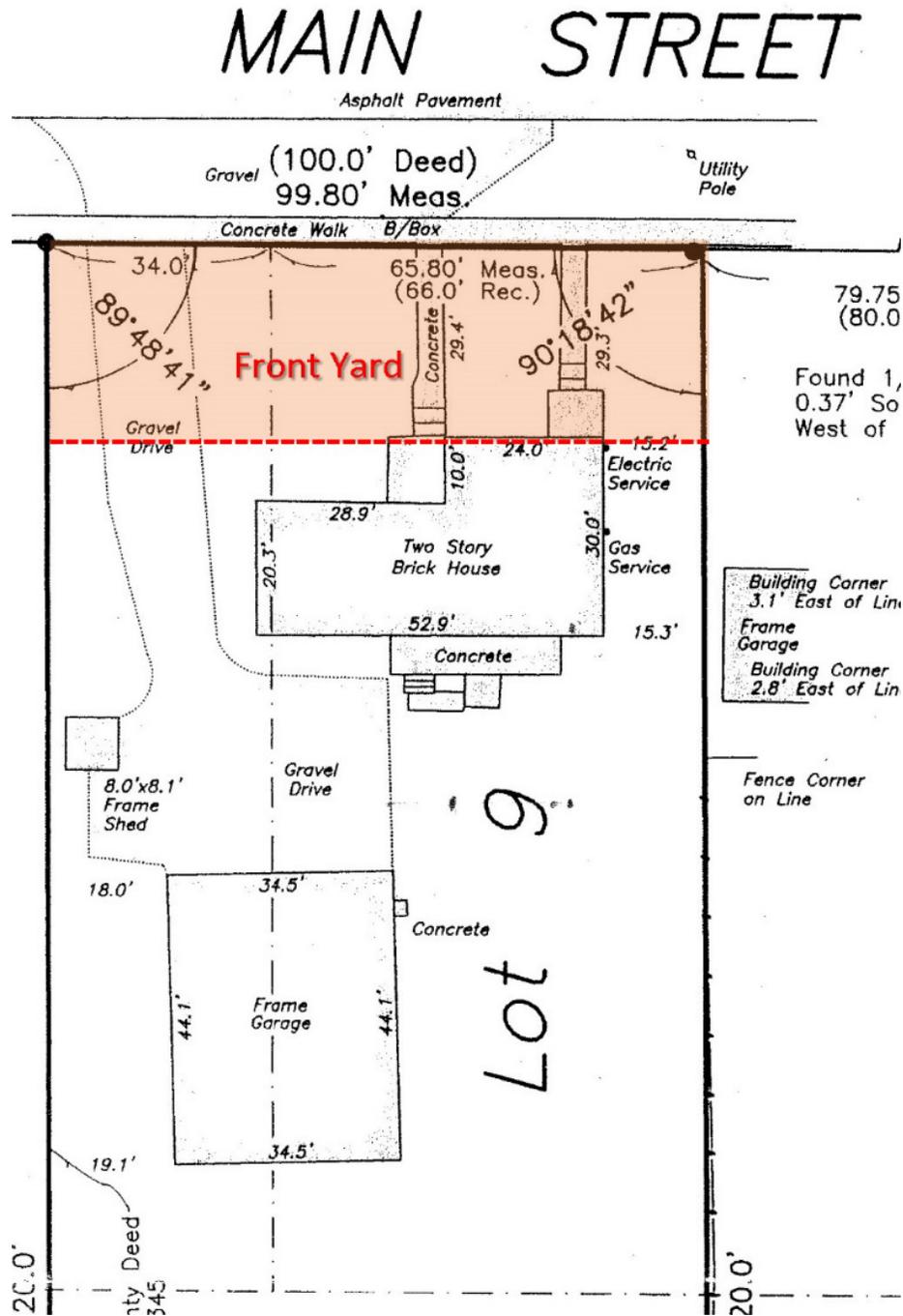
The petitioners, Charles and Gayle Ashley, have filed an application with the United City of Yorkville, Kendall County, Illinois, requesting a reduction in the front yard setback from the required minimum 30 feet from the property line to the building to 20 feet for a residential parcel located at 208 E. Main Street. The property is zoned R-2 Traditional Family Residence District and purpose of the request is to allow for the reconstruction of an attached covered porch, as was part of the original home, to be located within the required front yard. The proposed porch will reflect the established neighborhood character and forward the goals of the recently adopted Neighborhood Design Manual.

PROPERTY BACKGROUND:

The property, a two-story is located approximately 2 blocks east of IL Rte. 47 (N. Bridge Street) on Main Street surrounded by similar single-family homes in the older residential part of Yorkville. This parcel, as well as the adjacent parcels, was platted as a narrow, deep lot which backs up to wooded open space and the Fox River. This area of the City was developed prior to any modern zoning regulations existed in Yorkville and therefore the structures and parcels in the area may or may not conform to current code standards.



As currently situated, the nearest distance from the property line to the edge of the home is approximately 29.4'. However, the existing front stoop extends an additional eight feet (8') into the yard, effectively rendering the front setback at 21.4', less than the required minimum 30 feet for the R-2 zoning district. Therefore, this property is considered "legally non-conforming" since its non-conformity existed prior to the adoption of the current ordinance. The exhibit below illustrates where the house is located on the parcel. The required front yard is shaded and shows the existing stoop.



PROJECT DESCRIPTION:

The two-story brick residence, as seen in the picture to the right, was originally built circa 1860 with architectural features common to Victoria Stick Style homes. These features include steep gable and side gable rooflines; rectangular windows in a simple pattern of one-over-one; and unusually announced porches with large brackets that form curved diagonal braces. At some point, the front façade of the home was altered to remove the covered porch and the current homes exterior is depicted in the images below.



As proposed, the petitioners are seeking to rebuild the one-story porch to the existing front façade of the home in its original location. The porch would extend approximately thirty-two (32) feet across the front plane of the house and project out about eight (8) feet into the front yard, following the line of the existing front stoop. The covered roof of the porch would be approximately ten (10) feet in height and the floor material will be a one inch (1”) thick composite deck. The overall setback will essentially remain at its current distance of 21’-4” if the variance is approved.

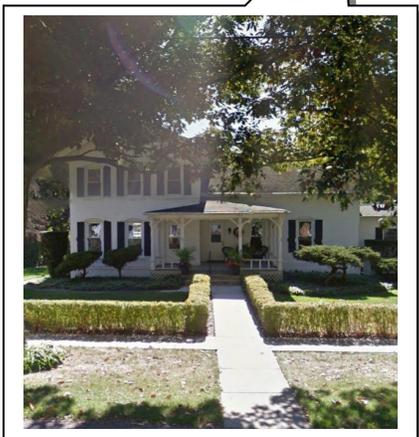
VARIANCE REQUEST:

Section 10-7-1 of the City’s Zoning Ordinance requires all buildings located in the R-2 Single-Family Traditional Residence District to maintain a minimum front yard setback of thirty feet (30’). Although the current home does not maintain this setback, it is non-conforming to this standard, Section 10-15-3-B-3 states the non-conforming structure can be maintained, repaired or altered but cannot expand or create any additional nonconformities. Therefore, the petitioner is seeking the variance so that they are legally allowed to encroach, expand and alter their home to the newly requested setback of 20 feet and not constrained, now or in the future, by the current non-conforming status.

NEIGHBORHOOD DESIGN:

In February 2019, the City Council adopted the Neighborhood Design Manual, which is geared towards retaining and enhancing the character and livability of Yorkville’s traditional neighborhoods. As part of this non-binding manual, guidelines were established for renovation and new construction with the goal of any repair or reconstruction blending into the fabric of the surrounding neighborhood. Specifically, regarding porches, the Neighborhood Design Manual acknowledges that “the front porch can be a great place to relax and enjoy the breeze on a cool summer evening” and therefore recommends when renovating an existing home in the older parts of town, “care should be taken to preserve or replicate the styling of historic elements...into the new design.” Additionally, the Comprehensive Plan designates this area as “Traditional Neighborhood Residential” which encourages the current residential character.

The petitioners main purpose is to replace the previous porch feature onto their home as it was originally built. They also desire to enhance the character of their home with the addition of this architectural element and “bring back some old-time touches” to the neighborhood, as other homes also have front porches. Furthermore, the variance for the front yard setback is consistent with the established neighborhood frontage and the addition of the requested porch would maintain that line, as illustrated in the aerial below.



STAFF COMMENTS

Staff is supportive of the request for front yard setback reduction as proposed by the petitioners. The original architecture of the home was designed with a Victorian Stick Style porch, similar to the surrounding residences. Additionally, the property, and most properties within the neighborhood, are legally non-conforming with regards to front yard setbacks, as they were built prior to the modern zoning code. Finally, the proposed porch would not change the character of the street line and is consistent with the recently adopted Neighborhood Design Manual and Comprehensive Plan.

Staff is seeking preliminary feedback from the Economic Development Committee in regard to this proposed variance. A public hearing for the variance is tentatively scheduled for the Planning and Zoning Commission meeting on September 11, 2019.

ATTACHMENTS:

1. Petitioner Application (with attachments)
2. Public Hearing Notice



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR VARIANCE

INTENT AND PURPOSE:

The purpose of a variance is to provide relief from certain regulations of the zoning ordinance to permit the use of land in a way that is not otherwise permitted under the ordinance. A variance is granted when the terms of the zoning ordinance, if literally applied, would create an unreasonable hardship on the landowner, making the property virtually useless.

This packet explains the process to successfully submit and complete an Application for a Variance Request. It includes a detailed description of the process and the actual application itself. Please type the required information in the application on your computer. The application will need to be printed and be signed by the petitioner. The only item that needs to be submitted to the city from this packet is the application. The rest of the packet is to help guide you through the process unto completion.

For a complete explanation of what is legally required throughout the Variance Request process, please refer to "Title 10, Chapter 4, Section 7 Variations" of the Yorkville, Illinois City Code.

APPLICATION PROCEDURE:



STAGE 1: APPLICATION SUBMITTAL

The following must be submitted to the Community Development Department:

- One (1) original signed application with legal description.
- Three (3) 11" x 17" copies each of the exhibits, proposed drawings, location map, and site plan. Large items must be folded to fit in a 10" x 13" envelope.
- Appropriate filing fee.
- One (1) CD or portable USB drive containing one (1) electronic copy (pdf) of each of the following: signed application (complete with exhibits), proposed drawings, location map, and site plan. A Microsoft Word document with the legal description is also required.

Within one (1) week of submittal, the Community Development Department will determine if the application is complete or if additional information is needed. These materials must be submitted a minimum of forty five (45) days prior to the targeted Planning and Zoning Commission meeting. An incomplete submittal could delay the scheduling of the project. The Kendall County Soil and Water Conservation District requires Natural Resource Inventory (NRI) applications from petitioners seeking variances. A copy of this application can be found on the District's website (www.KendallSWCD.org).

Petitioner will be responsible for payment of recording fees and public hearing costs, including written transcripts of the public hearing and outside consultant costs (i.e. legal review, land planner, zoning coordinator, environmental, etc.). The petitioner will be required to establish a deposit account with the city to cover these fees. The Petitioner Deposit Account/Acknowledgment of Financial Responsibility form is attached to this document and must be submitted with the application.



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR VARIANCE

STAGE 2: PLANNING AND ZONING COMMISSION

Petitioner will attend a public hearing conducted by the Planning and Zoning Commission. The Planning and Zoning Commission meets on the 2nd Wednesday of the Month at 7:00pm. Notice will be placed in the Kendall County Record by the United City of Yorkville. The petitioner is responsible for sending certified public hearing notices to adjacent property owners within five hundred (500) feet of the subject property no less than fifteen (15) days and no more than thirty (30) days prior to the public hearing date. Twenty four (24) hours prior to the public hearing, a certified affidavit must be filed by the petitioner with the Community Development Department containing the names, addresses and permanent parcel numbers of all parties that were notified. The Certified Mailing Affidavit form is attached to this document and must be submitted prior to the scheduled Planning and Zoning Commission meeting.

If the Planning and Zoning Commission decides that the requested variance adheres to the standards set forth by the Illinois municipal code, then the variance may be granted under these instances and no others:

- To permit any yard or setback less than the yard or setback required by the applicable regulations, but by no more than twenty-five percent (25%).
- To permit the use of a lot or lots for a use otherwise prohibited solely because of insufficient area or widths of the lot or lots but in no event shall the respective area and width of the lot or lots be less than ninety percent (90%) of the required area and width. The percentage set forth in this subsection is not to be reduced by any other percentage for minimum lot width and area set forth in this title.
- To permit the same off street parking facility to qualify as required facilities for two (2) or more uses provided the substantial use of such facility by each use does not take place at approximately the same hours of the same days of the week.
- To reduce the applicable off street parking or loading facilities required by not more than one (1) parking space or loading space, or twenty percent (20%) of the applicable regulations, whichever number is greater.
- To increase by not more than twenty five percent (25%) the maximum distance that required parking spaces are permitted to be located from the use served.
- To allow for the deferment, or land banking, of required parking facilities for a reasonable period of time, such period of time to be specified in the variance.
- To increase by not more than ten percent (10%) the maximum gross floor area of any use so limited by the applicable regulations.
- To exceed any of the authorized variations allowed under this subsection when a lot of record or a zoning lot, vacant or legally used on the effective date hereof, is, by reason of the exercise of the right of eminent domain by any authorized governmental domain proceeding, reduced in size so that the remainder of said lot of record or zoning lot or structure on said lot does not conform with one or more of the regulations of the district in which said lot of record or zoning lot or structure is located.

If the Planning and Zoning Commission decides the requested variance follows the Illinois standards but is not included in the list of instances above, then the variance must be approved by City Council.

STAGE 3: CITY COUNCIL

If necessary, the petitioner will attend a City Council public hearing where the Planning and Zoning Commission decision will be discussed and reviewed. The City Council meets on the 2nd and 4th Tuesdays of the month at 7:00pm. Variations other than those listed above may be granted by the City Council, but only after a public hearing for an authorized variation. The concurring vote of two-thirds (2/3) of all members of the City Council shall be necessary to reverse the recommendations of the Planning and Zoning Commission.



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR VARIANCE

DORMANT APPLICATIONS

The Community Development Director shall determine if an application meets or fails to meet the requirements stated above. If the Director determines that the application is incomplete it will become dormant under these circumstances:

- The petitioner has been notified of such deficiencies and has not responded or provided a time line for completing the application within ninety (90) days from the time of notification.
- The petitioner has not responded in writing to a request for information or documentation from the initial plan commission review within six (6) months from the date of that request.
- The petitioner has not responded to a request for legal or engineering deposit replenishment for city incurred costs and fees within ninety (90) days from the date of the request.

If the Community Development Director has sent the required notice and the petitioner has not withdrawn their application or brought it into compliance, then the director shall terminate the application. After termination, the application shall not be reconsidered except after the filing of a completely new application.

Withdrawal or termination of an application shall not affect the petitioner's responsibility for payment of any costs and fees, or any other outstanding debt owed to the city. The balance of any funds deposited with the city that is not needed to pay for costs and fees shall be returned to the petitioner. (Ord. 2011-34, 7-26-2011)



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR VARIANCE

INVOICE & WORKSHEET PETITION APPLICATION			
CONCEPT PLAN REVIEW	<input type="checkbox"/> Engineering Plan Review deposit	\$500.00	Total: \$
AMENDMENT	<input type="checkbox"/> Annexation <input type="checkbox"/> Plan <input type="checkbox"/> Plat <input type="checkbox"/> P.U.D.	\$500.00 \$500.00 \$500.00 \$500.00	Total: \$
ANNEXATION	<input type="checkbox"/> \$250.00 + \$10 per acre for each acre over 5 acres _____ - 5 = _____ x \$10 = _____ + \$250 = \$ _____ <small># of Acres Acres over 5 Amount for Extra Acres Total Amount</small>		Total: \$
REZONING	<input type="checkbox"/> \$200.00 + \$10 per acre for each acre over 5 acres <i>If annexing and rezoning, charge only 1 per acre fee; if rezoning to a PUD, charge PUD Development Fee - not Rezoning Fee</i> _____ - 5 = _____ x \$10 = _____ + \$200 = \$ _____ <small># of Acres Acres over 5 Amount for Extra Acres Total Amount</small>		Total: \$
SPECIAL USE	<input type="checkbox"/> \$250.00 + \$10 per acre for each acre over 5 acres _____ - 5 = _____ x \$10 = _____ + \$250 = \$ _____ <small># of Acres Acres over 5 Amount for Extra Acres Total Amount</small>		Total: \$
ZONING VARIANCE	<input checked="" type="checkbox"/> \$85.00 + \$500.00 outside consultants deposit		Total: \$ 585
PRELIMINARY PLAN FEE	<input type="checkbox"/> \$500.00		Total: \$
PUD FEE	<input type="checkbox"/> \$500.00		Total: \$
FINAL PLAT FEE	<input type="checkbox"/> \$500.00		Total: \$
ENGINEERING PLAN REVIEW DEPOSIT	<input type="checkbox"/> Less than 1 acre <input type="checkbox"/> Over 1 acre, less than 10 acres <input type="checkbox"/> Over 10 acres, less than 40 acres <input type="checkbox"/> Over 40 acres, less than 100 acres <input type="checkbox"/> Over 100 acres	\$5,000.00 \$10,000.00 \$15,000.00 \$20,000.00 \$25,000.00	Total: \$
OUTSIDE CONSULTANTS DEPOSIT	<i>Legal, land planner, zoning coordinator, environmental services</i> For Annexation, Subdivision, Rezoning, and Special Use: <input type="checkbox"/> Less than 2 acres <input type="checkbox"/> Over 2 acres, less than 10 acres <input type="checkbox"/> Over 10 acres		Total: \$
TOTAL AMOUNT DUE:			\$ 585



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR VARIANCE

DATE: 6-20-19	PZC NUMBER:	DEVELOPMENT NAME:
PETITIONER INFORMATION		
NAME: CHUCK & BOYLE ASHLEY	COMPANY:	
MAILING ADDRESS: 208 E. MAIN		
CITY, STATE, ZIP: YORKVILLE, IL 60560	TELEPHONE: 630-553-6414	
EMAIL: ASHLEY4649@SBCGLOBAL.NET	FAX:	
PROPERTY INFORMATION		
NAME OF HOLDER OF LEGAL TITLE: CHARLES & BOYLE ASHLEY		
IF LEGAL TITLE IS HELD BY A LAND TRUST, LIST THE NAMES OF ALL HOLDERS OF ANY BENEFICIAL INTEREST THEREIN: N/A		
PROPERTY STREET ADDRESS: 208 E. MAIN YORKVILLE		
DESCRIPTION OF PROPERTY'S PHYSICAL LOCATION: SAME AS ABOVE		
CURRENT ZONING CLASSIFICATION: R2		
ZONING AND LAND USE OF SURROUNDING PROPERTIES		
NORTH: R2		
EAST: R2		
SOUTH: R1		
WEST: R2		
KENDALL COUNTY PARCEL IDENTIFICATION NUMBER(S)		
0233109000		



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR VARIANCE

ATTORNEY INFORMATION	
NAME:	COMPANY:
MAILING ADDRESS: <i>N/A</i>	
CITY, STATE, ZIP:	TELEPHONE:
EMAIL:	FAX:
ENGINEER INFORMATION	
NAME:	COMPANY:
MAILING ADDRESS: <i>N/A</i>	
CITY, STATE, ZIP:	TELEPHONE:
EMAIL:	FAX:
LAND PLANNER/SURVEYOR INFORMATION	
NAME:	COMPANY:
MAILING ADDRESS: <i>N/A</i>	
CITY, STATE, ZIP:	TELEPHONE:
EMAIL:	FAX:
ATTACHMENTS	
Petitioner must attach a legal description of the property to this application and title it as "Exhibit A".	
Petitioner must list the names and addresses of any adjoining or contiguous landowners within 500 feet of the property that are entitled notice of application under any applicable City Ordinance or State Statute. Attach a separate list to this application and title it as "Exhibit B".	
VARIANCE STANDARDS	
PLEASE CONFIRM THE PROPOSED VARIATION IS CONSISTENT WITH THE OFFICIAL COMPREHENSIVE PLAN AND OTHER DEVELOPMENT STANDARDS AND POLICIES OF THE CITY.	
<i>YES</i>	



United City of Yorkville
800 Game Farm Road
Yorkville, Illinois, 60560
Telephone: 630-553-4350
Fax: 630-553-7575
Website: www.yorkville.il.us

APPLICATION FOR VARIANCE

VARIANCE STANDARDS

PLEASE STATE THE VARIANCE REQUESTED AND THE CITY ORDINANCE INCLUDING THE SECTION NUMBERS TO BE VARIED:

REQUEST A VARIANCE OF THE FRONT YARD SET BACK IN THE R-2 DISTRICT
FROM 30' TO 20' TO ACCOMMODATE A FRONT PORCH
TITLE 10, CH. 11 - ARTICLE C - SECTION 10-11 C-3.

PLEASE STATE HOW THE PARTICULAR SURROUNDINGS, SHAPE OR TOPOGRAPHICAL CONDITIONS OF THE SPECIFIC PROPERTY INVOLVED, A PARTICULAR HARDSHIP TO THE OWNER WOULD RESULT, AS DISTINGUISHED FROM A MERE INCONVENIENCE, IF THE STRICT LETTER OF REGULATIONS WAS CARRIED OUT:

WE WOULD NOT BE ABLE TO CONSTRUCT A FRONT PORCH AS WAS ORIGINALLY
PART OF THE HOME

PLEASE STATE HOW THE CONDITIONS UPON WHICH THE APPLICATION FOR A VARIATION IS BASED ARE UNIQUE TO THE PROPERTY FOR WHICH THE VARIATION IS SOUGHT AND ARE NOT APPLICABLE, GENERALLY, TO OTHER PROPERTY WITHIN THE SAME ZONING CLASSIFICATION:

HOME WAS CONSTRUCTED BEFORE THESE RULES WERE ESTABLISHED
CREATING SET BACKS.

PLEASE STATE HOW THE ALLEGED DIFFICULTY OR HARDSHIP IS CAUSED BY THIS TITLE AND HAS NOT BEEN CREATED BY ANY PERSON PRESENTLY HAVING AN INTEREST IN THE PROPERTY:

HOME WAS CONSTRUCTED BEFORE THESE RULES WERE CREATED.
MANY OTHER HOMES IN THIS NEIGHBORHOOD HAVE PORCHES THAT WOULD
NOT MEET TODAY'S STANDARDS.



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

APPLICATION FOR VARIANCE

VARIANCE STANDARDS

PLEASE STATE HOW THE GRANTING OF THE VARIATION WILL NOT BE DETRIMENTAL TO THE PUBLIC WELFARE OR INJURIOUS TO OTHER PROPERTY OR IMPROVEMENTS IN THE NEIGHBORHOOD IN WHICH THE PROPERTY IS LOCATED:

THIS PORCH WILL MATCH OTHERS IN THE NEIGHBORHOOD, ENHANCE THE HOME, AND BRING BACK SOME OLD TIME TOUCHES TO THE NEIGHBORHOOD

PLEASE STATE HOW THE PROPOSED VARIATION WILL NOT IMPAIR AN ADEQUATE SUPPLY OF LIGHT AND AIR TO ADJACENT PROPERTY, OR SUBSTANTIALLY INCREASE THE CONGESTION IN THE PUBLIC STREETS, OR INCREASE THE DANGER TO THE PUBLIC SAFETY, OR SUBSTANTIALLY DIMINISH OR IMPAIR PROPERTY VALUES WITHIN THE NEIGHBORHOOD:

THIS PORCH WILL HAVE NO ADVERSE EFFECT ON ANY SURROUNDING PROPERTY

AGREEMENT

I VERIFY THAT ALL THE INFORMATION IN THIS APPLICATION IS TRUE TO THE BEST OF MY KNOWLEDGE. I UNDERSTAND AND ACCEPT ALL REQUIREMENTS AND FEES AS OUTLINED AS WELL AS ANY INCURRED ADMINISTRATIVE AND PLANNING CONSULTANT FEES WHICH MUST BE CURRENT BEFORE THIS PROJECT CAN PROCEED TO THE NEXT SCHEDULED COMMITTEE MEETING.

I UNDERSTAND ALL OF THE INFORMATION PRESENTED IN THIS DOCUMENT AND UNDERSTAND THAT IF AN APPLICATION BECOMES DORMANT IT IS THROUGH MY OWN FAULT AND I MUST THEREFORE FOLLOW THE REQUIREMENTS OUTLINED ABOVE.

Charles R. Ashley

 PETITIONER SIGNATURE

6-20-19

 DATE

OWNER HEREBY AUTHORIZES THE PETITIONER TO PURSUE THE APPROPRIATE ENTITLEMENTS ON THE PROPERTY.

Charles R. Ashley

 OWNER SIGNATURE

6-20-19

 DATE

THIS APPLICATION MUST BE NOTARIZED PLEASE NOTARIZE HERE:



United City of Yorkville
 800 Game Farm Road
 Yorkville, Illinois, 60560
 Telephone: 630-553-4350
 Fax: 630-553-7575
 Website: www.yorkville.il.us

PETITIONER DEPOSIT ACCOUNT/ ACKNOWLEDGMENT OF FINANCIAL RESPONSIBILITY

PROJECT NUMBER:	FUND ACCOUNT NUMBER:	PROPERTY ADDRESS:
-----------------	----------------------	-------------------

APPLICATION/APPROVAL TYPE (check appropriate box(es) of approval requested):

<input type="checkbox"/> CONCEPT PLAN REVIEW	<input type="checkbox"/> AMENDMENT (TEXT)	<input type="checkbox"/> ANNEXATION	<input type="checkbox"/> REZONING
<input type="checkbox"/> SPECIAL USE	<input type="checkbox"/> MILE AND 1/2 REVIEW	<input checked="" type="checkbox"/> ZONING VARIANCE	<input type="checkbox"/> PRELIMINARY PLAN
<input type="checkbox"/> FINAL PLANS	<input type="checkbox"/> PLANNED UNIT DEVELOPMENT	<input type="checkbox"/> FINAL PLAT	

PETITIONER DEPOSIT ACCOUNT FUND:
 It is the policy of the United City of Yorkville to require any petitioner seeking approval on a project or entitlement request to establish a Petitioner Deposit Account Fund to cover all actual expenses occurred as a result of processing such applications and requests. Typical requests requiring the establishment of a Petitioner Deposit Account Fund include, but are not limited to, plan review of development approvals/engineering permits. Deposit account funds may also be used to cover costs for services related to legal fees, engineering and other plan reviews, processing of other governmental applications, recording fees and other outside coordination and consulting fees. Each fund account is established with an initial deposit based upon the estimated cost for services provided in the **INVOICE & WORKSHEET PETITION APPLICATION**. This initial deposit is drawn against to pay for these services related to the project or request. Periodically throughout the project review/approval process, the Financially Responsible Party will receive an invoice reflecting the charges made against the account. At any time the balance of the fund account fall below ten percent (10%) of the original deposit amount, the Financially Responsible Party will receive an invoice requesting additional funds equal to one-hundred percent (100%) of the initial deposit if subsequent reviews/fees related to the project are required. In the event that a deposit account is not immediately replenished, review by the administrative staff, consultants, boards and commissions may be suspended until the account is fully replenished. If additional funds remain in the deposit account at the completion of the project, the city will refund the balance to the Financially Responsible Party. A written request must be submitted by the Financially Responsible Party to the city by the 15th of the month in order for the refund check to be processed and distributed by the 15th of the following month. All refund checks will be made payable to the Financially Responsible Party and mailed to the address provided when the account was established.

ACKNOWLEDGMENT OF FINANCIAL RESPONSIBILITY

NAME: <u>CHUCK & GAYLE ASHLEY</u>	COMPANY:
MAILING ADDRESS: <u>208 E. MAIN ST</u>	
CITY, STATE, ZIP: <u>YORKVILLE, IL 60560</u>	TELEPHONE: <u>630-553-6414</u>
EMAIL:	FAX:

FINANCIALLY RESPONSIBLE PARTY:
 I acknowledge and understand that as the Financially Responsible Party, expenses may exceed the estimated initial deposit and, when requested by the United City of Yorkville, I will provide additional funds to maintain the required account balance. Further, the sale or other disposition of the property does not relieve the individual or Company/ Corporation of their obligation to maintain a positive balance in the fund account, unless the United City of Yorkville approves a Change of Responsible Party and transfer of funds. Should the account go into deficit, all City work may stop until the requested replenishment deposit is received.

PRINT NAME _____	TITLE _____
SIGNATURE _____	DATE _____

ACCOUNT CLOSURE AUTHORIZATION

DATE REQUESTED: _____

PRINT NAME: _____

SIGNATURE: _____

DEPARTMENT ROUTING FOR AUTHORIZATION:

<input type="checkbox"/> COM. DEV.	<input type="checkbox"/> BUILDING	<input type="checkbox"/> ENGINEERING	<input type="checkbox"/> FINANCE	<input type="checkbox"/> ADMIN.
------------------------------------	-----------------------------------	--------------------------------------	----------------------------------	---------------------------------

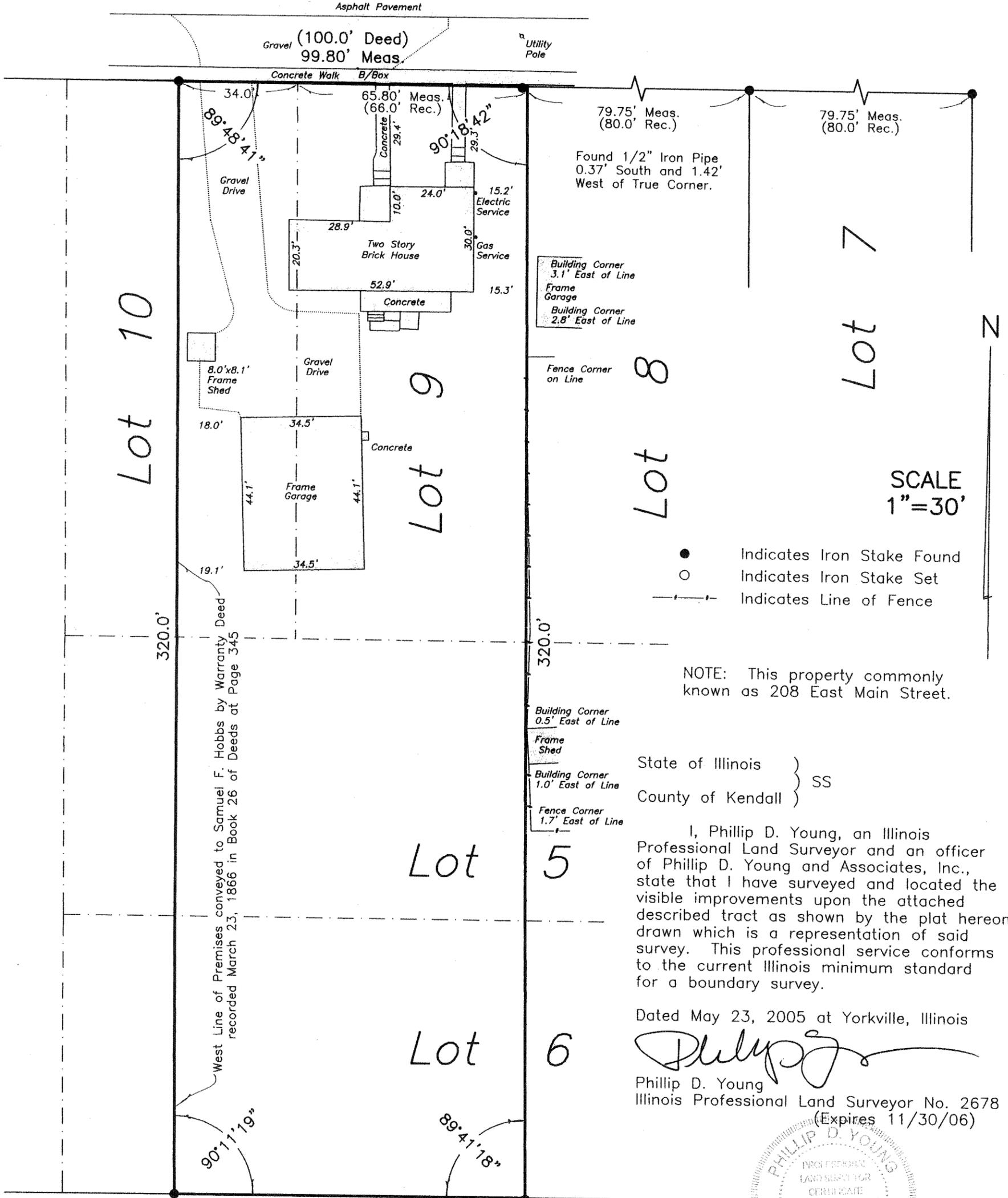






PLAT OF SURVEY OF
 LOT 9 AND PART OF LOTS 5, 6 AND 10 - BLOCK 22 - McCLELLAN'S ADDITION
 YORKVILLE KENDALL COUNTY ILLINOIS

MAIN STREET



SCALE
1"=30'

- Indicates Iron Stake Found
- Indicates Iron Stake Set
- Indicates Line of Fence

NOTE: This property commonly known as 208 East Main Street.

State of Illinois }
 County of Kendall } SS

I, Phillip D. Young, an Illinois Professional Land Surveyor and an officer of Phillip D. Young and Associates, Inc., state that I have surveyed and located the visible improvements upon the attached described tract as shown by the plat hereon drawn which is a representation of said survey. This professional service conforms to the current Illinois minimum standard for a boundary survey.

Dated May 23, 2005 at Yorkville, Illinois

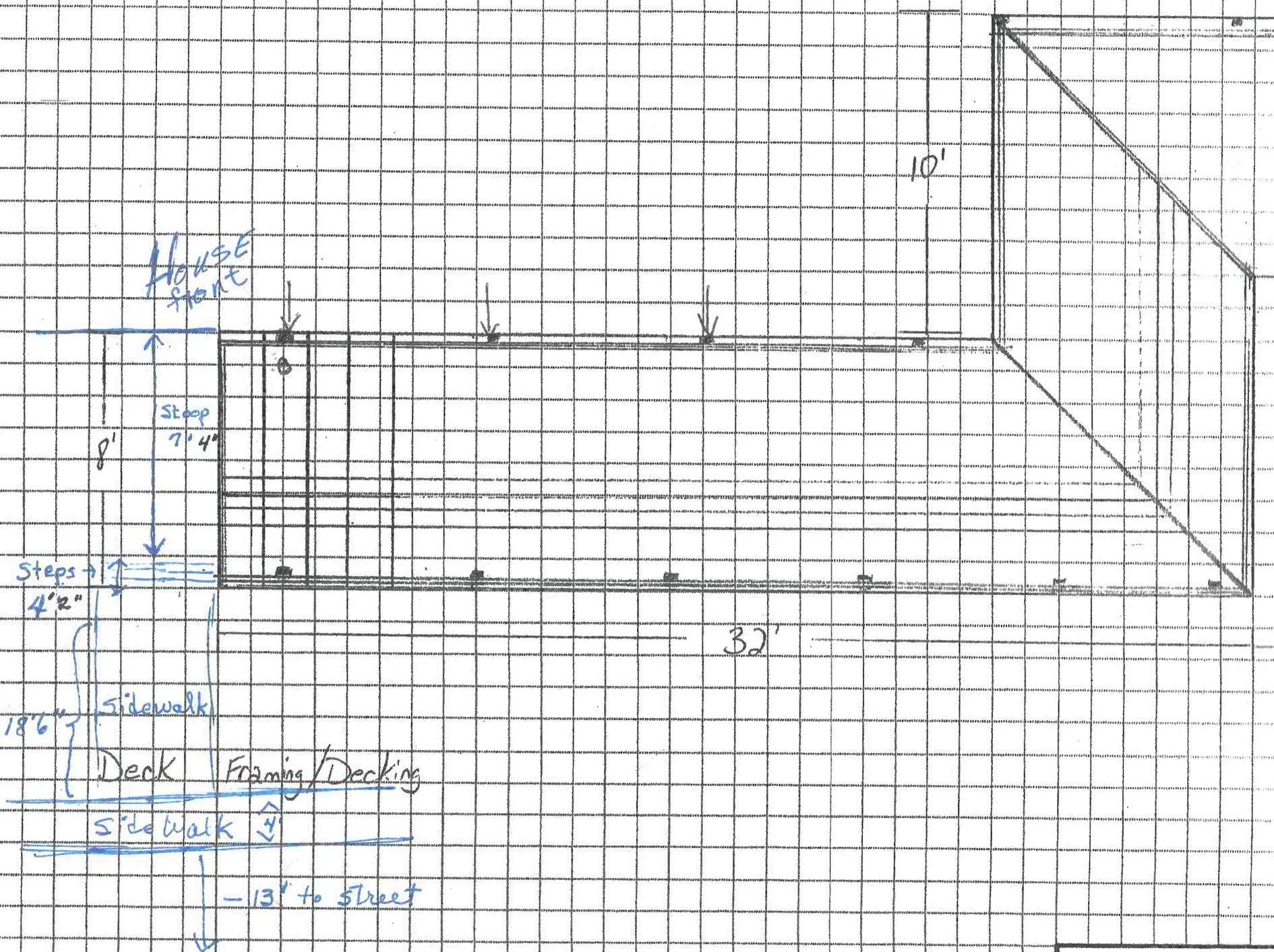
Phillip D. Young
 Phillip D. Young
 Illinois Professional Land Surveyor No. 2678
 (Expires 11/30/06)



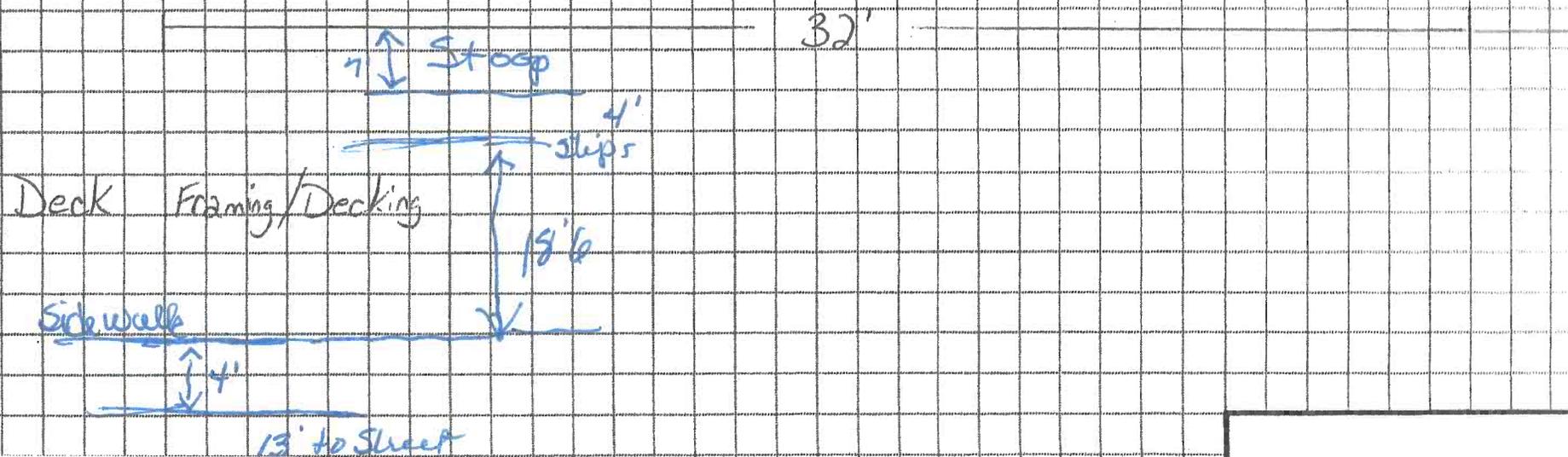
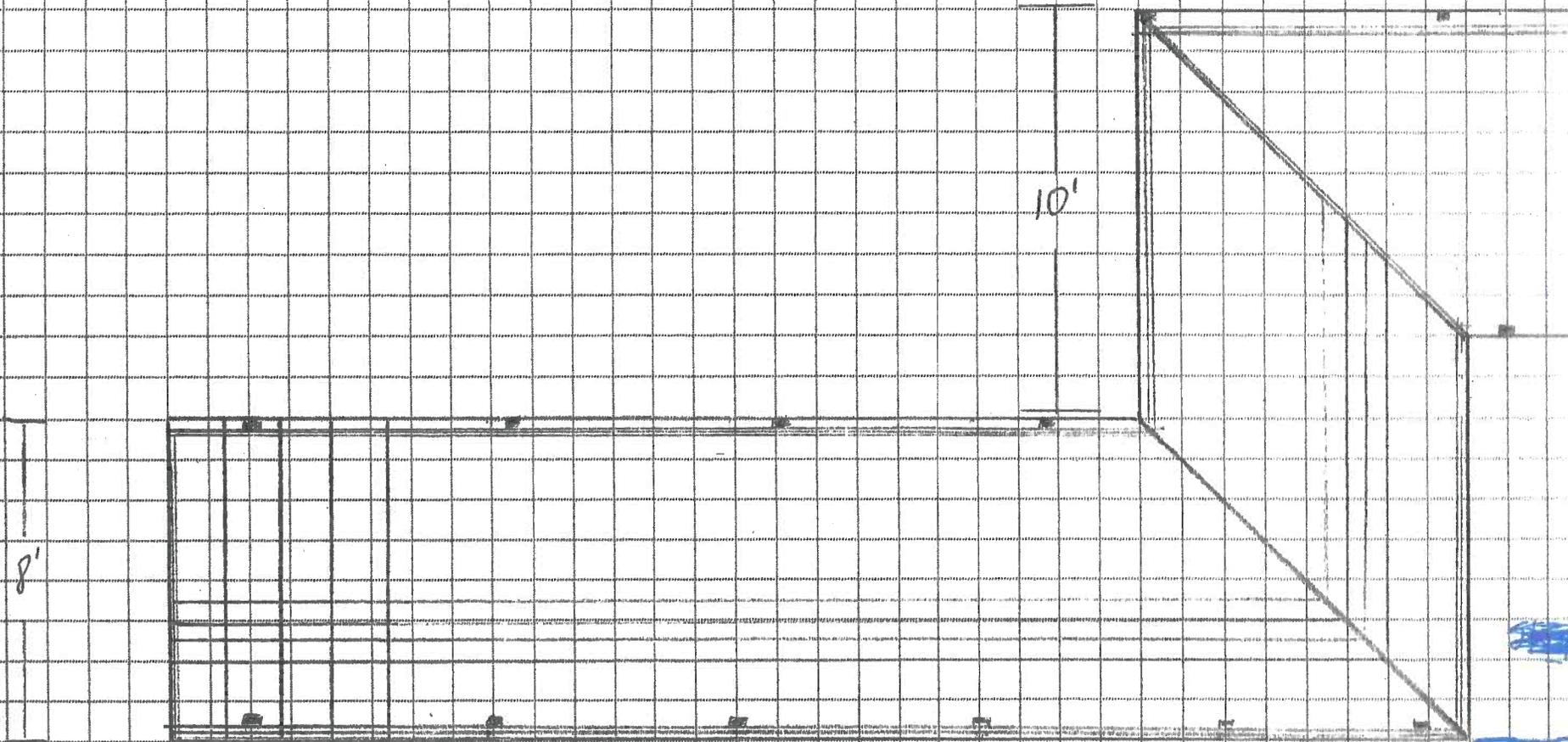
LEGAL DESCRIPTION:

Lot 9 and that part of Lots 5, 6 and 10 in Block 22 of McClellan's Addition to Bristol described as follows: Beginning at the Northeast Corner of said Lot 9; thence Southerly, along the Easterly Line of said Lot 9 and said Easterly Line extended, 320.0 feet to the Southerly Line of Lot 6; thence Westerly, along said Southerly Line, 100.0 feet; thence Northerly, parallel with said Easterly Line extended and said Easterly Line, 320.0 feet to the Northerly Line of said Lot 10; thence Easterly, along said Northerly Line, 100.0 feet to the point of beginning in the United City of the Village of Yorkville, Kendall County, Illinois.

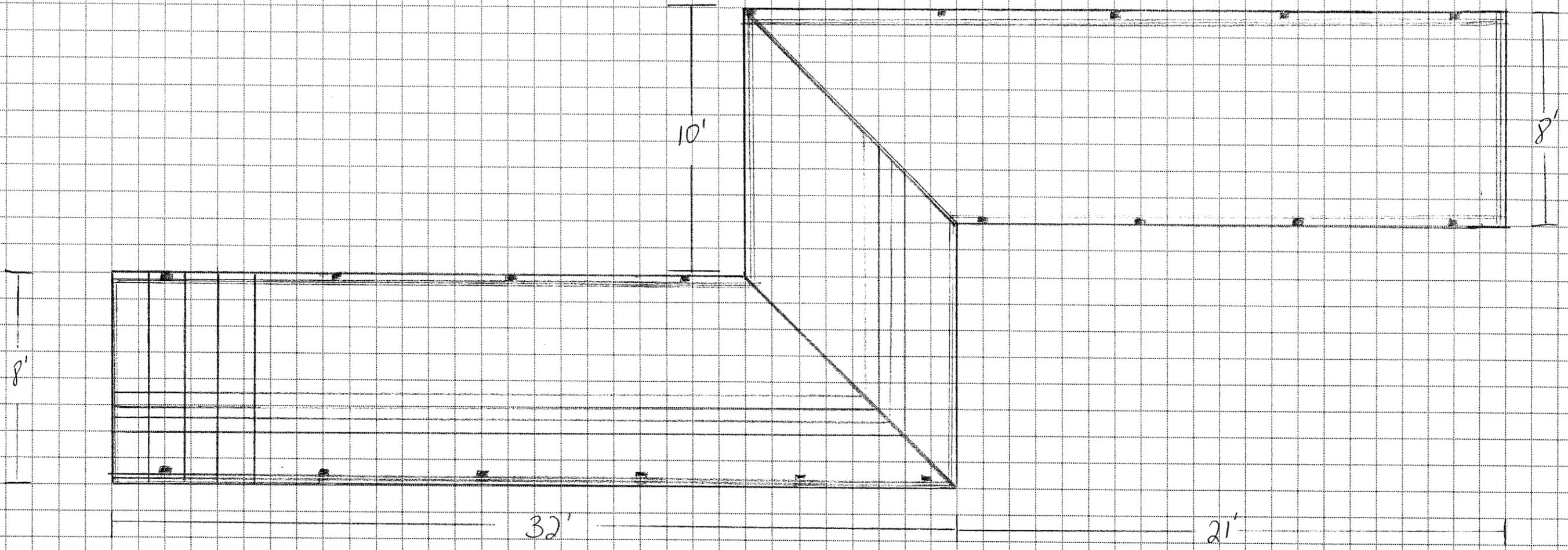
- 1" Thick composite decking



- 1" Thick composite decking



- 2" x 6" Floor Joists - Pressure Treated 16" O.C. Typical
- 2) 2" x 6" Beams with Joist Hangers
- 1" Thick composite decking



Deck Framing/Decking

Deck
Plan

JOB _____

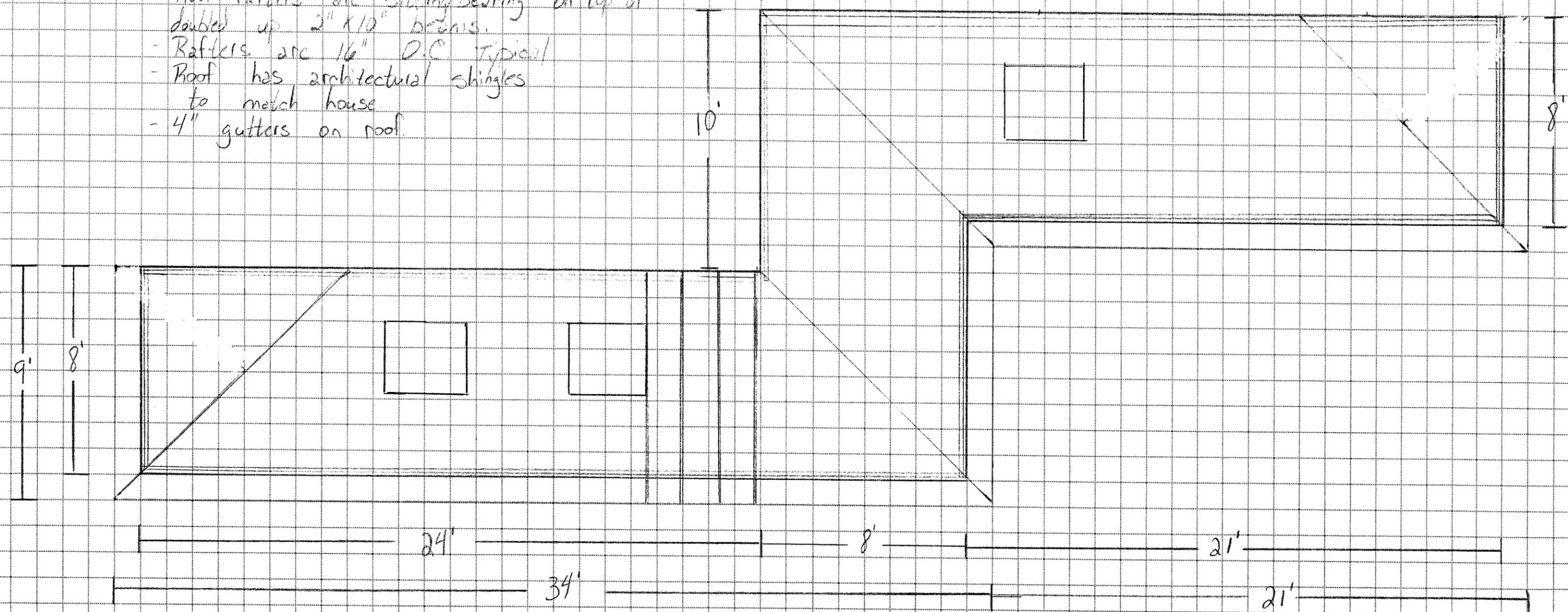
SHEET NO. _____ OF _____

CALCULATED BY _____ DATE _____

CHECKED BY _____ DATE _____

SCALE _____

- Red lines indicate roof outline
- There is a 12" overhang past the lower deck.
- There are 3 hips and 1 valley
- Roof rafters are sitting/bearing on top of doubled up 2" x 10" beams.
- Rafters are 16" O.C. Typical
- Roof has architectural shingles to match house
- 4" gutters on roof



- 8" Ground Level Deck
- 9" Roof covering Deck
- 16" O.C. Typical spacing for 2" x 6" Floor joists and 2" x 6" Roof rafters
- 3 - 33 1/2 x 33 1/2" Skylights lined up with windows below
- 2) 2" x 10" Beams Carrying roof rafters

Roof Plan

JOB _____

SHEET NO. _____ OF _____

CALCULATED BY _____ DATE _____

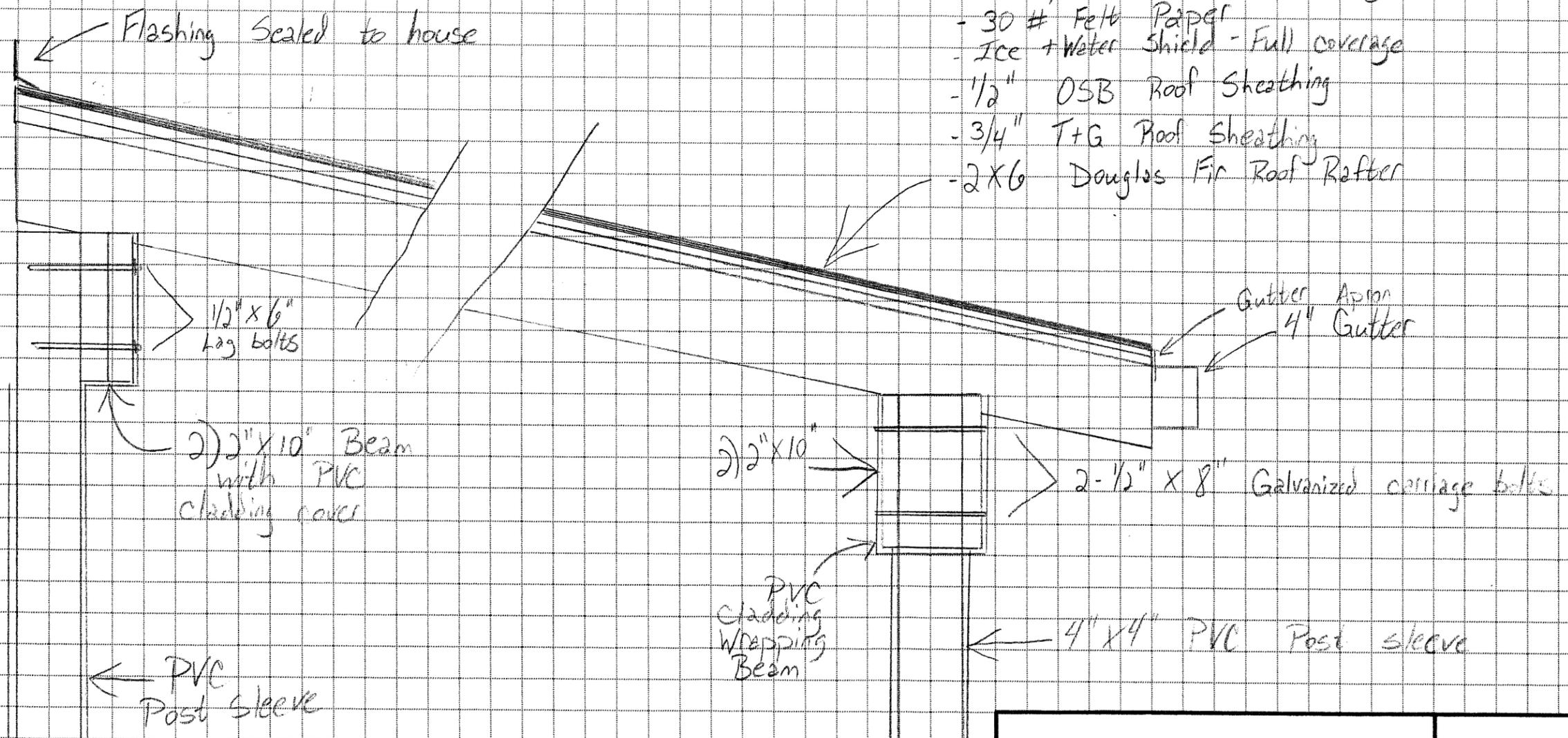
CHECKED BY _____ DATE _____

SCALE 1/4" = 1'

2/12 Roof Pitch

- 2x6 Douglas Fir Roof Rafters with pine 1"x6" tongue and groove cersiding on top.
- Exposed rafters and 1"x6" trim underneath

- 25 year Architectural Shingle
- 30 # Felt Paper
- Ice + Water Shield - Full coverage
- 1/2" OSB Roof Sheathing
- 3/4" T+G Roof Sheathing
- 2x6 Douglas Fir Roof Rafter



Roof Details

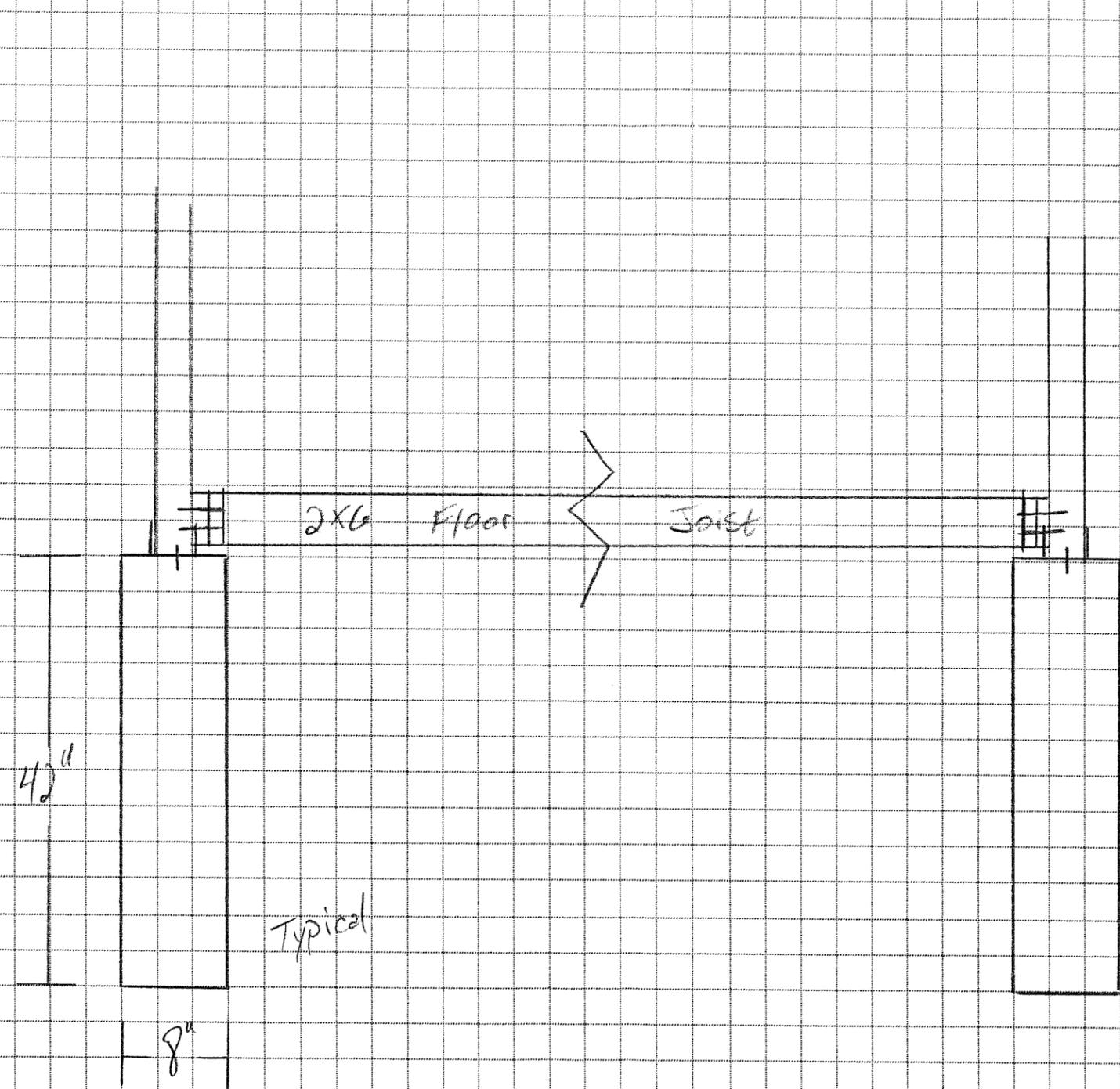
JOB _____

SHEET NO. _____ OF _____

CALCULATED BY _____ DATE _____

CHECKED BY _____ DATE _____

SCALE _____



4" x 4" post
fastened to
post bracket.
Post bracket
fastened to
concrete pile with
wedge anchor.

2-2" x 6" Beam
lag bolted to
4" x 4" post.

2-2" x 6" Beam
Joist hangers
with joists
1/2" O.C. Typical

Deck is ground
level deck

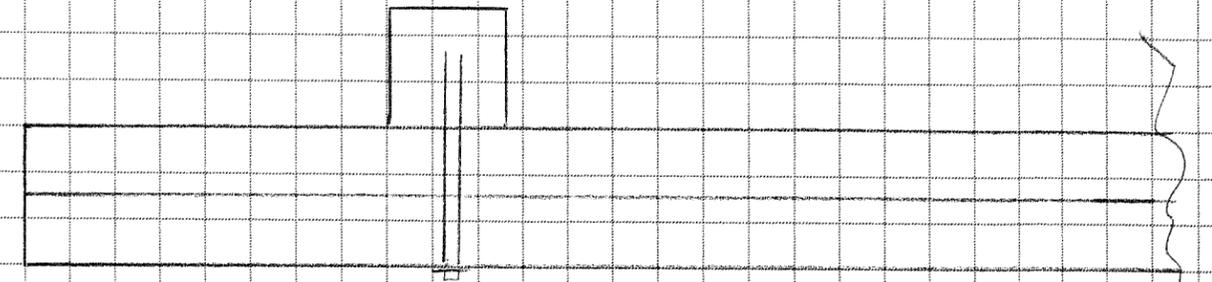
All 2" x 6" beams,
floor joists, and
4" x 4" posts
are pressure treated

Typical

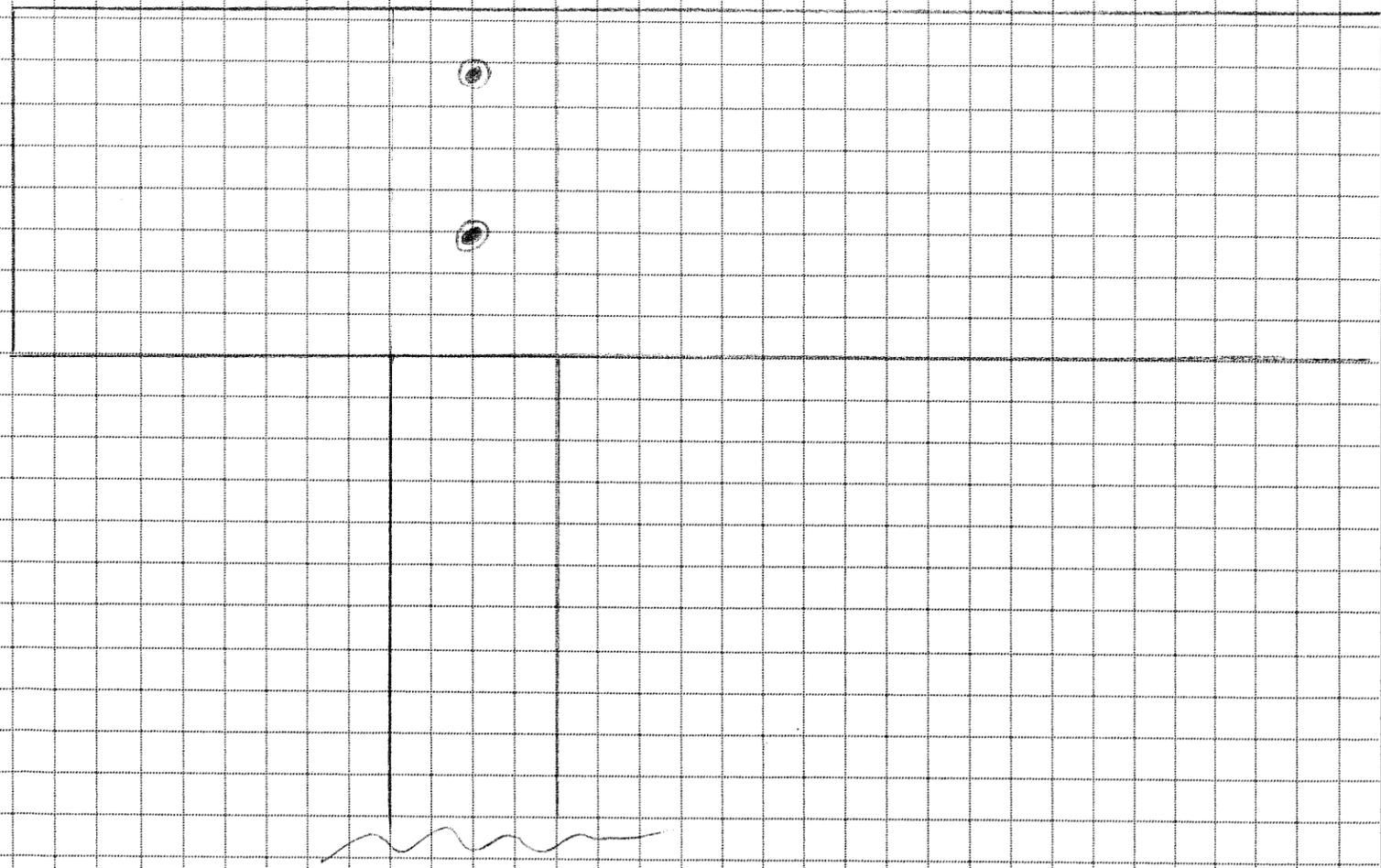
Post + Beam
Details

JOB _____
 SHEET NO. _____ OF _____
 CALCULATED BY _____ DATE _____
 CHECKED BY _____ DATE _____
 SCALE _____

- Doubled up 2"x10" Beam Fastened with 2 - 1/2" x 6" Lag bolts into 4" x 4".



- Doubled up 2"x10" beam supports roof rafters.



- 4" x 4" Post bracket anchored into concrete pier. 4" x 4" post fastened to post bracket.

- 4" x 4" s are pressure treated

42"

8"
Concrete
Pier

Roof Beams
Details

JOB _____
SHEET NO. _____ OF _____
CALCULATED BY _____ DATE _____
CHECKED BY _____ DATE _____
SCALE _____

PUBLIC NOTICE
NOTICE OF PUBLIC HEARING
BEFORE
THE UNITED CITY OF YORKVILLE
PLANNING & ZONING COMMISSION
PZC 2019-20

NOTICE IS HEREWITH GIVEN THAT Charles and Gayle Ashley, petitioners, have filed an application with the United City of Yorkville, Kendall County, Illinois, requesting a reduction in front yard setback from 30 feet to 20 feet for a residential parcel located at 208 E. Main Street. The purpose of this request is to allow for the reconstruction of an attached covered porch, as was part of the original home, to be located within the required front yard. The real property, zoned R-2 Traditional Family Residence District, is located east of South Bridge Street (IL Route 47) and immediately south of East Main Street in Yorkville, Illinois.

The legal description is as follows:

LOT 9 AND THAT PART OF LOTS 5, 6 AND 10 IN BLOCK 22 OF MCCLELLAN'S ADDITION TO BRISTOL DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHEAST CORNER OF SAID LOT 9; THENCE SOUTHERLY, ALONG THE EASTERLY LINE OF SAID LOT 9 AND SAID EASTERLY LINE EXTENDED, 320.0 FEET TO THE SOUTHERLY LINE OF LOT 6; THENCE WESTERLY, ALONG SAID SOUTHERLY LINE, 100.0 FEET; THENCE NORTHERLY, PARALLEL WITH SAID EASTERLY LINE EXTENDED AND SAID EASTERLY LINE, 320.0 FEET TO THE NORTHERLY LINE OF SAID LOT 10; THENCE EASTERLY, ALONG SAID NORTHERLY LINE, 100.0 FEET TO THE POINT OF BEGINNING IN THE UNITED CITY OF THE VILLAGE OF YORKVILLE, KENDALL COUNTY, ILLINOIS.

PIN: 02-33-109-006

NOTICE IS HEREWITH GIVEN THAT the Planning and Zoning Commission for the United City of Yorkville will conduct a public hearing on said application on **Wednesday, September 11, 2019 at 7:00 p.m.** at the United City of Yorkville, City Council Chambers, located at 800 Game Farm Road, Yorkville, Illinois 60560.

The public hearing may be continued from time to time to dates certain without further notice being published.

Application and information materials regarding this notice are available for public review and any questions or written comments should be addressed to the United City of Yorkville Community Development Department, City Hall, 800 Game Farm Road, Yorkville, Illinois. All interested parties are invited to attend the public hearing and will be given an opportunity to be heard.

By order of the Corporate Authorities of the United City of Yorkville, Kendall County, Illinois.

Lisa Pickering
City Clerk

Ordinance No. 2019-_____

**AN ORDINANCE OF THE UNITED CITY OF YORKVILLE, KENDALL COUNTY,
ILLINOIS, GRANTING A FRONT YARD SETBACK VARIANCE FOR THE
PROPERTY LOCATED AT 208 E. MAIN STREET
(Front Porch)**

WHEREAS, the United City of Yorkville, Kendall County, Illinois (the “City”) is a duly organized and validly existing non-home-rule municipality created in accordance with the Constitution of the State of Illinois of 1970 and the laws of the State; and,

WHEREAS, pursuant to the Illinois Municipal Code (65 ILCS 5/11-13-5) the Mayor and City Council of the City (the “Corporate Authorities”) may provide for and allow variances to provide relief when strict compliance with the requirements of the Yorkville Zoning Ordinance (the “Zoning Ordinance”) presents a particular hardship; and,

WHEREAS, Charles and Gayle Ashley (the “Applicants”), requested to reduce the required minimum front yard setback in the R-2 Single-Family Traditional Residence District from thirty feet (30’) to twenty feet (20’) for the purposes of constructing an attached porch on their residential property in the front yard that is not permitted pursuant to Section 10-07-01 of the Zoning Ordinance; and,

WHEREAS, A notice of a public hearing on said application was published and pursuant to said notice the Planning and Zoning Commission of the City conducted a public hearing on September 11, 2019, on said application in accordance with the State statutes and the ordinances of the City; and,

WHEREAS, the Planning and Zoning Commission made the required written Findings of Fact finding that the variation met the standards in Section 10-4-7C of the Zoning Ordinance and recommended that the variance be granted; and,

WHEREAS, the Corporate Authorities of the City of Yorkville have received and considered the recommendation of the Planning and Zoning Commission.

NOW, THEREFORE, BE IT ORDAINED, by the Mayor and City Council of the City of Yorkville, Kendall County, Illinois, as follows:

Section 1: That this Ordinance shall apply to the Subject Property legally described as:

LOT 9 AND THAT PART OF LOTS 5, 6 AND 10 IN BLOCK 22 OF MCCLELLAN'S ADDITION TO BRISTOL DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHEAST CORNER OF SAID LOT 9; THENCE SOUTHERLY, ALONG THE EASTERLY LINE OF SAID LOT 9 AND SAID EASTERLY LINE EXTENDED, 320.0 FEET TO THE SOUTHERLY LINE OF LOT 6; THENCE WESTERLY, ALONG SAID SOUTHERLY LINE, 100.0 FEET; THENCE NORTHERLY, PARALLEL WITH SAID EASTERLY LINE EXTENDED AND SAID EASTERLY LINE, 320.0 FEET TO THE NORTHERLY LINE OF SAID LOT 10; THENCE EASTERLY, ALONG SAID NORTHERLY LINE, 100.0 FEET TO THE POINT OF BEGINNING IN THE UNITED CITY OF THE VILLAGE OF YORKVILLE, KENDALL COUNTY, ILLINOIS.

Commonly known as 208 E. Main Street, Yorkville, Illinois,
Property Index Number: 02-33-109-006

Section 2: That a variation pursuant to Section 10-07-01 of the Zoning Ordinance to reduce the required minimum front yard setback in the R-2 Single-Family Traditional Residence District from thirty feet (30') to twenty feet (20') of the Subject Property is hereby granted.

Section 3: That the front porch shall be constructed, operated and maintained in accordance with the requirements of the Yorkville City Code and generally located as shown on the attached plat of survey prepared by Phillip D. Young and Associates, Inc. dated May 23, 2005 and made a part hereof as Exhibit A.

Section 4: That this ordinance shall be in full force and effect from and after its passage, approval and publication in pamphlet form as provided by law.

Passed by the City Council of the United City of Yorkville, Kendall County, Illinois, this
_____ day of _____, 2019.

CITY CLERK

KEN KOCH _____

DAN TRANSIER _____

JACKIE MILSCHEWSKI _____

ARDEN JOE PLOCHER _____

CHRIS FUNKHOUSER _____

JOEL FRIEDERS _____

SEAVER TARULIS _____

JASON PETERSON _____

Approved by me, as Mayor of the United City of Yorkville, Kendall County, Illinois, this
_____ day of _____, 2019.

MAYOR



Reviewed By:	
Legal	<input checked="" type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input checked="" type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #7

Tracking Number

EDC 2019-65

Agenda Item Summary Memo

Title: Tactical Urbanism/Planning Program

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: Details the proposed Tactical Planning projects

Council Action Previously Taken:

Date of Action: _____ Action Taken: _____

Item Number: _____

Type of Vote Required: _____

Council Action Requested: _____

Submitted by: Jason Engberg Community Development
Name Department

Agenda Item Notes:

See attached memo.



Memorandum

To: Economic Development Committee
From: Jason Engberg, Senior Planner
CC: Bart Olson, City Administrator
Krysti Barksdale-Noble, Community Development Director
Date: July 11, 2019
Subject: **Tactical Urbanism/Planning Program**

SUMMARY

Yorkville has taken steps to improve its long-term planning efforts in recent years by completing the 2016 Comprehensive Plan and currently finishing the Downtown Overlay District. These plans provide suggestions on how the City may accomplish its vision in the long-term by providing implementable objectives throughout each of the plans' horizons. It is the responsibility as Community Development staff to implement the given objectives to ensure that the community's vision is accomplished.

One of the objectives given in the Comprehensive Plan which was further detailed in the Downtown Overlay District, is to pursue "tactical urbanism/planning" efforts in the downtown. Tactical planning provides low-cost, temporary changes to the built environment intended to improve local neighborhoods and gathering places. This memorandum will outline tactical planning concepts, potential projects in Yorkville, and potential future opportunities in the tactical planning field.

WHAT IS TACTICAL PLANNING?

Typical long term urban planning practices include gathering existing conditions, evaluating goals and visions, creating a roadmap to achieve these goals, and finally a guide to implement the plan. This process is excellent for creating long term success for a community and gives direction for future development. While an excellent process for planning, the amount of time and resources dedicated to these efforts can be intense. Also, the length of time it takes to implement the plan is typically long. Tactical planning offers quicker and affordable solutions which provide a base for long term improvements.

The purpose of tactical planning is to create a sense of place and provide activity by utilizing underused areas within a public space immediately. It may be executed by City departments, local advocates, or businesses. Some examples of tactical planning include pop-up cafes/parklets, food trucks, chair bombing, or street painting (for additional information please see the attached "Tactical Urbanism 2" document).

Overall, tactical planning efforts provide short-term commitments and realistic expectations. It is a great way to start incremental change and provide something tangible that residents and guests can see immediately. Providing better public spaces improves the quality of life for residents while incentivizing private investment in the area.

TACTICAL PLANNING EXAMPLES



From top left to bottom right: turf parklet; painted shared roadway; street yoga; overhead lanterns.

PROJECT BACKGROUND

Yorkville's decision to explore the possibility of providing tactical planning projects throughout the downtown began with suggestions from the draft Streetscape Master Plan and a contest being held by the College of New Urbanism (CNU) in 2018. During its drafting, the Streetscape Master Plan provided several different ideas for public improvements including murals, public art, painted intersections, and parklets. During this process, staff was already prioritizing which of the ideas could be implemented by the City and how much capital and time it would take to complete.

As their charter states, the CNU is an organization of people which dedicates itself to the restoration of existing urban centers and towns within coherent metropolitan regions, the reconfiguration of sprawling suburbs into communities of real neighborhoods and diverse districts, the conservation of natural environments, and the preservation of our built legacy. In 2018, the CNU held a contest to provide funding for tactical planning projects. Applicants were given a chance to present their ideas and the winner would be given \$5,000 for implementation.

Staff utilized the ideas in the Streetscape Master Plan and proposed three separate tactical planning projects. Unfortunately, Yorkville did not win the competition. While the City did not win the implementation money, there is still value in these projects and \$2,500 has been budgeted for this fiscal year for implementation.



Original location map for tactical planning projects

POTENTIAL PROJECTS

For the original project, three places in the downtown were chosen for three separate projects: a beach box behind nearby businesses; life size chess near the dam; and kickball billiards at the Van Emmon Activity Center. Over the past year, some of these projects have changed in design and scope as potential issues were discussed. The final projects and locations that the City is proposing include:

- Beach Box – along the river behind nearby businesses
- Life Size Chess – near the dam and pedestrian bridge to the island
- Concrete Cornhole – on the hill at the Van Emmon Activity Center

The following pages will provide insight and basic information about each of these projects.

Beach Box

Location: Fox River



Existing Location



Planned Improvements

SUMMARY OF IMPROVEMENTS:

- Shallow sandbox into the ground
- Concrete wall painted with mural
- Chairs for sitting and enjoying the view
- Large umbrella for shade

POTENTIAL ISSUES AND MITIGATION:

Potential Issues

People will steal the umbrella.

People will steal the chairs.

What about ADA compliance?

Mitigation

The umbrella will be weighted and steaked in the ground beneath the beach box.

Similarly, the chairs will be chained to an underground weight or affixed to the ground in a semi-permanent way

The beach box will be recessed into the ground and be level with the adjacent walkway. The sand will be shallow to allow for easy movement.

Life Size Chess

Location: Fox River Dam



Existing Location



Planned Improvements

SUMMARY OF IMPROVEMENTS:

- Different paver patio blocks arrange to look like a chess board
- Plastic chess pieces purchased online

POTENTIAL ISSUES AND MITTIGATION:

Potential Issues

The pieces will blow away in the wind.

People will steal the pieces.

Mitigation

Each piece will be weighed down by pouring sand into the base of each piece.

Possibly. If this is a problem, the board could always be switched to checkers which would be less desirable to steal and easier to replace.

Concrete Cornhole

Location: Van Emmon Activity Center



Existing Location



Planned Improvements

SUMMARY OF IMPROVEMENTS:

- Permanent cornhole set on the grass
- Potential for bench seating if budget allows

POTENTIAL ISSUES AND MITTIGATION:

Potential Issues

Are we going to provide bags?

The ground isn't level.

Mitigation

No. Users will have to bring their own bags which will deter any nuisances with nearby traffic.

While the ground isn't perfectly level, the boards will be able to be shimmed and poured level to each other.

POTENTIAL FUTURE PROJECTS:

These three projects will be the initial step into a larger improvement downtown. There are many long-term plans for the area, but sometimes these small projects can provide interest and excitement for bigger and better things. For example, if we install one set of cornhole boards and the popularity is high, residents might request more sets in the downtown. Or if the beach box is well received and residents want more places to sit, the City could install hammocks all along the river. The hope is that these projects will entice residents, businesses, and visitors to come up with their own ideas for improvements and let the City know what they want to see. This type of participation in their own community is what gives residents a sense of ownership and pride in their community.

INSTALLATION AND FUNDING:

As stated previously, the City has allotted \$2,500 for these tactical planning projects. Installation of these projects is expected to be completed by City staff members. There is potential for volunteers to help with the construction of these projects, such as the boy scouts or local tradesmen. For example, the City could partner with the High School Art program to paint and design the mural on the beach box wall, or the boy scouts could help install the paver blocks for the chessboard. Currently, the plan is for members of staff to install these projects.

STAFF COMMENTS:

Staff is seeking input from the Economic Development Committee on these potential tactical planning projects. After review by the committee, staff will have a resolution drafted and it will be reviewed by the City Council.

ATTACHMENTS:

1. Tactical Urbanism 2 Short Term, Long Change

TACTICAL² URBANISM

Short-Term Action || Long-Term Change

tac·ti·cal

adj: \tak-ti-kəl\

1. of or relating to small-scale actions serving a larger purpose
2. adroit in planning or maneuvering to accomplish a purpose

STREET
PLANS
MIAMI NEW YORK



TACTICIANS

MIKE LYDON, Project Editor/Author

DAN BARTMAN, Layout/Copy Editor

TONY GARCIA, Contributor

RUSS PRESTON, Contributor

RONALD WOULDSTRA, Contributor



“The lack of resources is no longer an excuse not to act. The idea that action should only be taken after all the answers and the resources have been found is a sure recipe for paralysis. The planning of a city is a process that allows for corrections; it is supremely arrogant to believe that planning can be done only after every possible variable has been controlled.”

- Jaime Lerner

Architect, urbanist, former mayor of Curitiba, Brazil

A NOTE ON VOLUME 2

During the fall of 2010 I traveled to New Orleans to take part in a retreat with my friends and colleagues who sometimes self-identify as the Next Generation of New Urbanists. While staying there in the Marigny, I shared my notes on a number of seemingly unrelated, but similar small-scale initiatives happening in the streets of New York, San Francisco, Portland, and Dallas. Per usual, they challenged me to better articulate my ideas and offered to help.

Tactical Urbanism: Short-term Action, Long-term Change, Volume 1 was then assembled during the spring of 2011 and uploaded from 38,000 feet while flying to Bangkok—as inspirational a city as any for the tactical urbanist. The intent of the publication was to place an umbrella over a growing number of short-term, often self-funded efforts that were demonstrably leading to long-term change.

The term “tactical urbanism” is inspired by a June 2010 *faslanyc* blog post that discusses the pedestrianization of Times Square. In the introduction the author describes the DOT’s efforts as “tactical interventions,” and “hacks.” It was my first exposure to such terms applied to the built environment and it seemed to perfectly describe not just the Greenlight for Broadway project, but a groundswell of other low-cost, un- semi- and fully-sanctioned interventions.

Volume 1 was uploaded to my company’s website and to the Pattern Cities blog. In less than two months the document was downloaded more than 10,000 times, the maximum number allowed with our free SCRIBD account. We knew that the publication struck a chord with a whole new generation of citymakers.

Sensing a need to further discuss and share tactical initiatives from across the country, we partnered with numerous organizations in the fall of 2011 to produce the first Tactical Urbanism Salon. Approximately 150 people from around the country (and Canada) discussed their projects, listened to others, and drank free beer. Prior to the Salon we promised attendees that we’d release Volume II. However, with several other projects in the works, that promise turned out to be a bit too ambitious. Nonetheless, from Weed Bombing and Depave, to Ad-Busting and Camps, Tactical Urbanism Volume 2 presents updated information, new insights, new photos, and 12 additional short-term actions inspiring long-term change. I hope you find the initiatives presented herein as inspirational as they are practical.

MIKE LYDON

Editor / Author

The Street Plans Collaborative

March 2, 2012



*PARK(ing) Day participants temporarily reclaim an on-street parking space for a more productive use.
Credit: Parking Day flickr pool*

THE CHALLENGE

Cities are in a constant state of flux, which may explain we planners are often preoccupied with control. Large-scale transformations, such as stadiums, museums, large waterfront parks, and convention centers are all big-ticket items with measurable curb appeal (for some). Yet, such projects require a substantial investment of time, as well as a deep reserve of political, social, and fiscal capital. Moreover, the long-term economic or social benefit cannot be guaranteed.

In the pursuit of equitable progress, citizens are typically invited to engage in a process that is fundamentally broken: rather than being asked to contribute to incremental change at the neighborhood or block level, residents are asked to react to proposals they often don't understand, and at a scale for which they have little control. For better or worse, this often results in NIMBYism of the worst kind. Surmounting the challenges inherent to these "public" processes continues to prove difficult. Fortunately, cities were not always made this way. We do have alternatives.

TACTICAL URBANISM

Improving the livability of our towns and cities commonly starts at the street, block, or building scale. While larger scale efforts do have their place, incremental, small-scale improvements are increasingly seen as a way to stage more substantial investments. This approach allows a host of local actors to test new concepts before making substantial political and financial commitments. Sometimes sanctioned, sometimes not, these actions are commonly referred to as "guerilla urbanism," "pop-up urbanism," "city repair," or "D.I.Y. urbanism."

While exhibiting several overlapping characteristics, "tactical urbanism," is a deliberate approach to city-making that features the following five characteristics:

- A deliberate, phased approach to instigating change;
- An offering of local ideas for local planning challenges;
- Short-term commitment and realistic expectations;
- Low-risks, with a possibly a high reward; and
- The development of social capital between citizens, and the building of organizational capacity between

public/private institutions, non-profit/NGOs, and their constituents.

EXPERIMENTATION INFORMS DESIGN

Case studies from across North America reveal the benefit of taking an incremental approach to the process of city building—long-term change often starts with trying something small. Upon implementation, results may be observed and measured in real time. In this way, tactical urbanism projects intentionally create a laboratory for experimentation. Or, to paraphrase professor Nabeel Hamdi, they allow for the making of plans without the preponderance of planning.

When such experiments are done inexpensively, and with flexibility, adjustments may be made before moving forward with large capital expenditures. Indeed, there is real merit in a municipality or developer spending \$30,000 on temporary plaza before investing \$3,000,000 in changes that are permanent. If the project doesn't work as planned, the entire budget is not exhausted and future designs may be calibrated to absorb the lessons learned from what is surely a particular and dynamic context. If done well, such small-scale changes may be conceived as the first step in realizing lasting change. Thus, tactical urbanism is most effective when used in conjunction with long-term planning efforts that marry the urgency of now with the wisdom of patient capital.

If included as part of a public planning process, tactical urbanism may more quickly build trust amongst disparate interest groups and community leaders. Indeed, if the public is able to physically participate in the improvement of the city, no matter how small the effort, there is an increased likelihood of gaining increased public support for more permanent change later. Involving the public in the physical testing of ideas may also yield unique insights into the expectations of future users and the types of design features for which they yearn; truly participatory planning must go beyond drawing on flip charts and maps.

Finally, using tactical urbanism to activate those plans already sitting on the shelf may recover the momentum gained during the actual planning process and move some of the most realistic or exciting ideas closer to fruition.

Tactical urbanism can be used to simply build public awareness, to identify a small but pressing issue. Recently in Baltimore, Maryland a “guerilla crosswalk” was painted across a busy street with inexpensive white paint and rollers. Although the city typically removes unauthorized signs and pavement markings in short or-



Temporary experiments can test physical improvements prior to implementation. Credit: Edward Erfurt IV

der, the temporary improvement directly communicated the need, and a real desire from the public for better pedestrian infrastructure. The crosswalk has since become permanent.

Similarly, the Toronto-based Urban Repair Squad maintains a website that gathers images of D.I.Y. urban repairs in the city's public spaces. They recently featured images of bicycle symbols painted on streets indicating the need for future extensions of the city's bicycle network.

In an increasing number of instances, municipalities follow the lead of their citizens by more permanently implementing the short-term, low-budget livability improvements initiated by citizen-activists. In other cases, those working within city hall are taking the lead.

WHY NOW?

While many of the tactics profiled herein are new, the city- and place-making process we now call tactical urbanism is not (see pgs. 4-5). Indeed, the development of human settlements has always included, if not required incremental and self-directed action aimed towards increasing social capital, commercial opportunity, and urban livability. In many developing cities and countries, this remains the only way forward.

It is only recently that the triumvirate of cheap oil, easy financing, and government regulations, like Euclidean zoning, has dulled the spirit of the North American tactical urbanist. We attribute the new wave of tactical projects, which run along a continuum of unsanctioned to sanctioned efforts (see pgs. 7), to three recent and overlapping trends:

1. The Great Recession
2. Shifting demographics

3. The Internet as a tool for building the civic economy

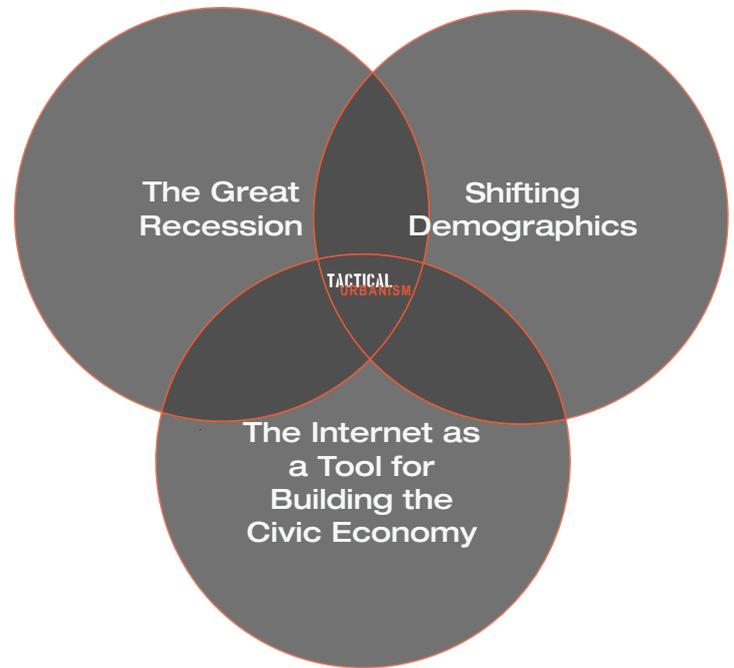
First, a benefit of the recession is that it slowed the North American growth machine. This effectively forced citizens, city departments, and developers to take matters into their own hands, get creative with project funding, and concentrate on smaller, more incremental efforts.

This has occurred while more and more people—especially the young and well educated—have continued to move into once forlorn walkable neighborhoods. This cohort includes retirees, who are also interested in re-making their chosen neighborhoods. Interestingly, some of these young people are also moving into government leadership positions as the baby boomers retire.

Finally, the culture of sharing tactics online has grown tremendously and is becoming more sophisticated. Thanks to web-based tools, a blogger can share something tactical in Dallas and have it re-blogged, tweeted, facebooked etc. in dozens of cities within minutes. The most industrious tactical urbanists, such as Team Better Block, Rebar, and Depave, are using the web as a platform for sharing free how-to manuals aimed at helping you bring their tactics to your town. Such a trend is an example of what Britain's National Endowment for Science, Technology and the Arts (NESTA) calls the 'civic economy' — the spirit of entrepreneurship combined with the aspiration of civic renewal.

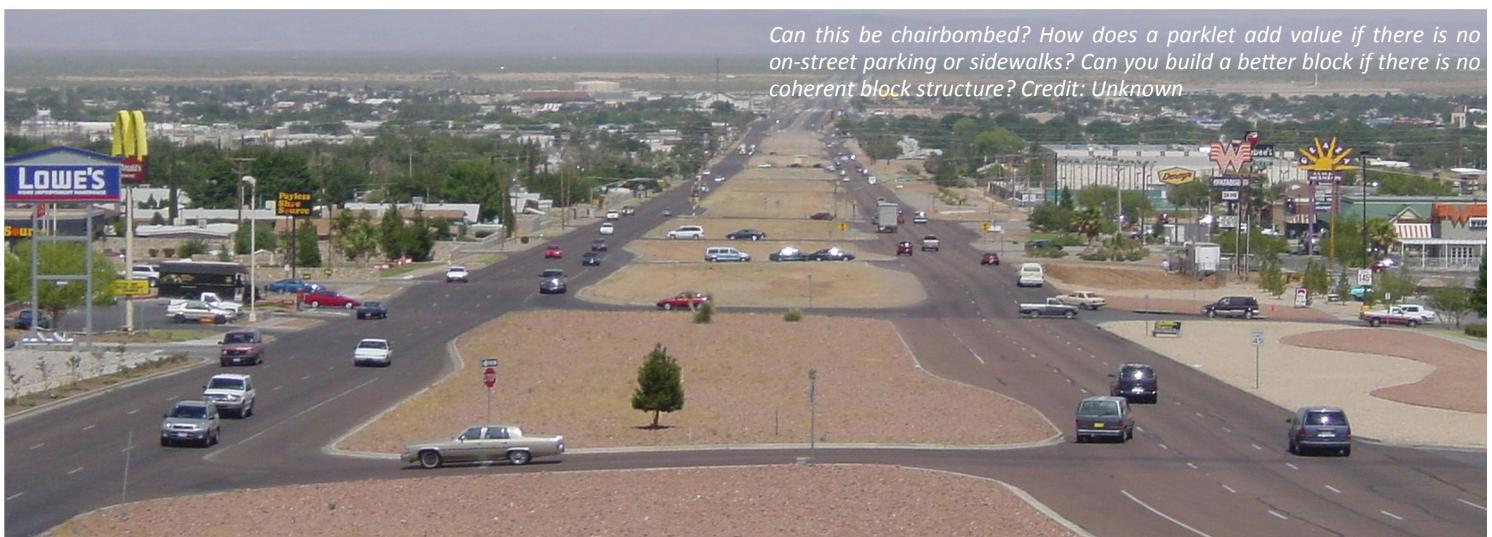
WHY HERE AND NOT THERE?

Theoretically, tactical urbanism can be applied to the arterials, parking lots, and cul-de-sacs of America. Yet, the best examples are consistently found in compact towns and cities featuring an undervalued/underutilized supply of walkable urban fabric. We believe this calls attention to the limited social, economic, and physical re-

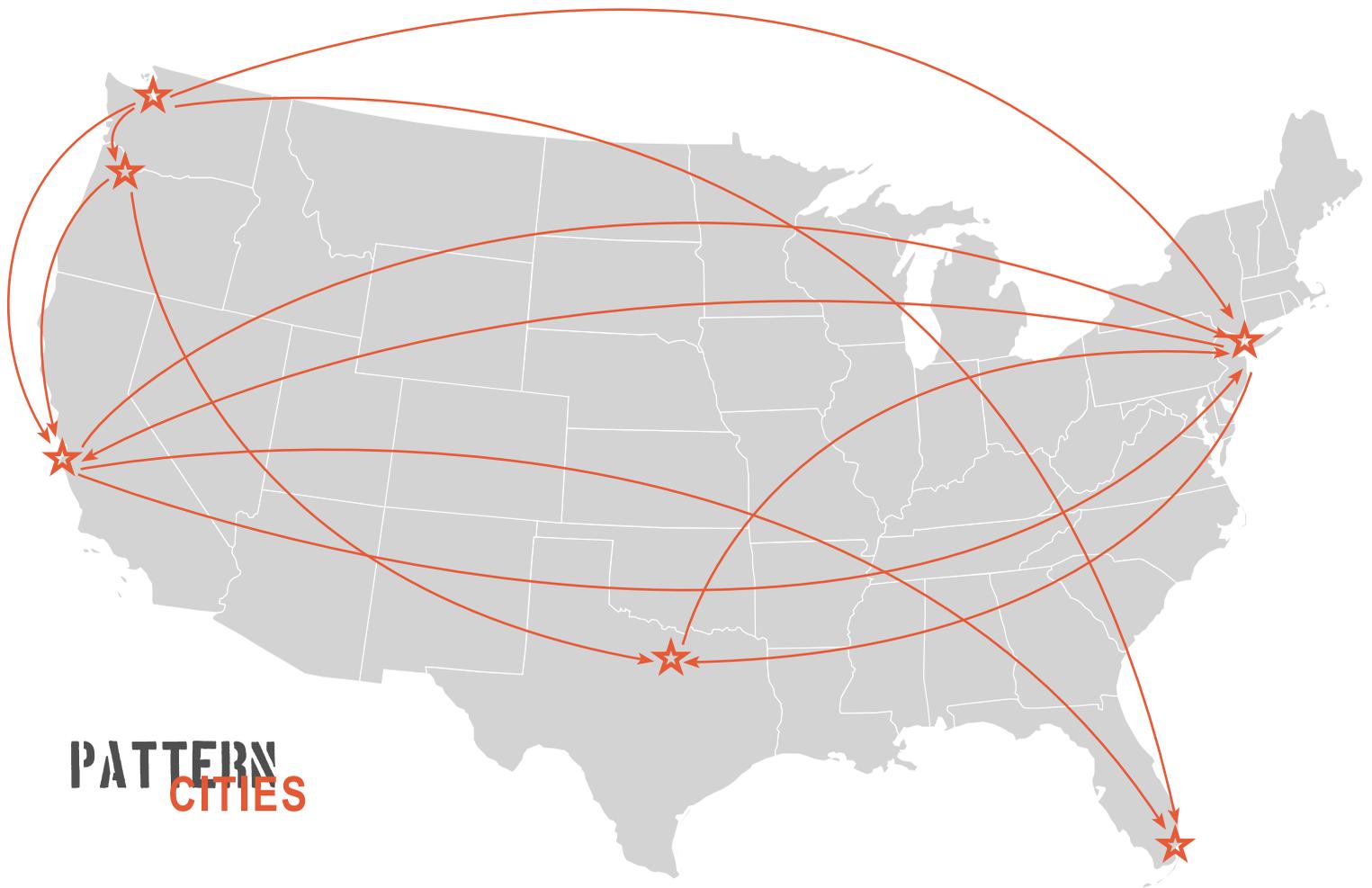


The rise in tactical urbanism may be attributed to three recent and overlapping trends.

siliency found in sprawling, auto-centric environments. It seems that human-scaled places, where social capital and creativity are most easily catalyzed, are a pre-requisite for tactical urbanism. The larger moves and design techniques, such as those highlighted in the *Sprawl Repair Manual* and *Retrofitting Suburbia* might provide more appropriate first moves in dealing with America's unwalkable suburbs.



Can this be chairbombed? How does a parklet add value if there is no on-street parking or sidewalks? Can you build a better block if there is no coherent block structure? Credit: Unknown



PATTERN CITIES

TACTICAL TIMELINE

Select Patterns of Influence

A handful of cities continually establish urban development paradigms that others readily follow. These cities, which we call Pattern Cities, aren't always the progenitors of innovation, nor are they necessarily Global Cities. Rather, they are the first to adapt ideas

at a meaningful scale and/or time in history. In the age of the Internet, new patterns spread faster than ever before. Tactical urbanism is but one pattern and it's being adopted in cities across the United States and beyond. Learn more at <http://patterncities.com>

- | | | | |
|-------------|---|-------------|---|
| 1914 | NYC launches the Play Streets program | 2007 | Sao Paulo, Brazil bans billboards, inspires Ad-Busters around the world |
| 1950 | Play Streets spread to London, England | 2009 | Pavement to Parks begins in San Francisco |
| 1965 | Seattle starts first Open Streets initiative | 2010 | Build a Better Block started in Dallas |
| 1970 | San Francisco artist Bonnie Ora Sherk introduces Portable Architecture project, a forerunner of Park(ing) Day | 2010 | DoTank Chair Bombs the Streets of Brooklyn |
| 1973 | Guerilla Gardening efforts start in NYC | 2010 | NYC rebrands San Fran's Pavement to Parks as Pop-Up Cafes |
| 1997 | City Repair adopt intersections in Portland | 2011 | City Point developers use Site Pre-Vitalization at Brooklyn's Dekalb Market |
| 2001 | First Pop-Up Retail event held in London | 2011 | Guggenheim Museum opens its traveling T Town Hall, the Guggenheim Lab |
| 2005 | Park(ing) Day develops in San Francisco | 2011 | San Francisco launches the Parkmobile |
| 2006 | Programmed, Bogotá-styled Open Streets initiatives spread to North America | 2011 | Activists begin Weed Bombing Miami streets |
| 2006 | Pavement to Plazas program starts in NYC | 2012 | Grand Central Park, Biscayne Parkway bring insta-parks to downtown Miami |
| 2007 | Depave program launched in Portland, OR | | |
| 2007 | Site Pre-Vitalization used for Hercules Market in Hercules, CA | | |



Les Bouquinistes. Credit: Charles Simond, La vie parisienne à travers le XIXe siècle, Paris, E. Plon, Nourrit et cie, 1900, p. 458.



What began as an unsanctioned use of public space in the 16th century has become a UNESCO World Heritage Site. Credit: EventSeekr.com

TACTICAL TIMELINE

Les Bouquinistes

Les Bouquinistes in Paris, France, demonstrate that tactical urbanism is nothing new. Beginning in the 16th century, unsanctioned booksellers began congregating along the banks of the Seine to hawk the latest best-sellers.

However, much like brick and mortar restaurants denounce today's food trucks, physical bookshop owners complained loudly enough to have the booksellers banned in 1649. Not to be deterred, les bouquinistes proved so popular that the city had to eventually allow their presence. However, regulations confined them to specific locations and stipulated that each "shop" must collapse into a box at day's end.

In 2007 the area occupied by Les Bouquinistes was designated as a UNESCO World Heritage site, which makes this tactic one of the slowest, if not the most lauded, examples of tactical urbanism.

- 1500s** Traveling Book Sellers begin setting up informal "pop-up" shops along the Seine
- 1649** Book sellers banned at Pont Neufe, later reinstated
- 1789** "Bouquiniste" appears in French dictionary
- 1859** City permits Bouquinistes at fixed points along the Seine, regulates amount of space allowed, charges registration fee
- 1930s** Box dimensions are fixed, regulated by City
- 1993** Jacque Chirac signs law standardizing new box size
- 2007** Declared UNESCO World Heritage site
- 2012** 300,000 books, 900 boxes, 240 sellers, along 3km of prime Seine real estate



*The boxes of Les Bouquinistes.
Credit: Acscosta via Wikipedia*

TACTICAL Urbanism

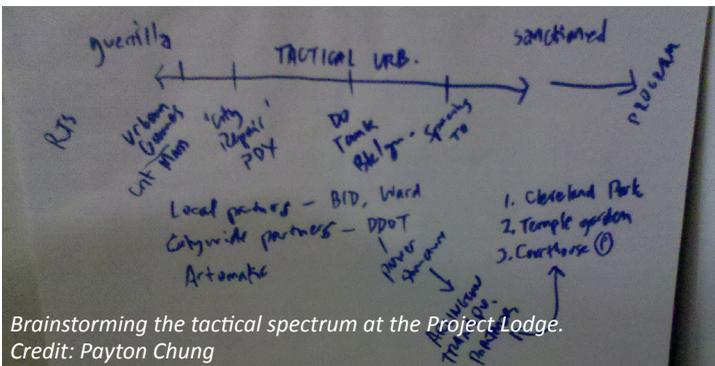


THE TACTICAL URBANISM SPECTRUM

Tactical urbanism projects can be placed along a spectrum of unsanctioned to sanctioned efforts. Many examples in this guide began as unsanctioned grassroots interventions that proved so successful that they soon became sanctioned or permanent. The Depave program, for example, began with neighborhood activists but has transformed into a non-profit funded by the City of Portland and the EPA. This is how short-term action creates long-term change. This is tactical urbanism.

VOLUME 2

Two dozen tactics are included in this, the second survey of tactical urbanism projects. While not comprehensive, and mostly limited to the North American context, the work described herein presents numerous opportunities to transform our towns and cities into better places to occupy together. If you have additional tactics to add, please send them to info@streetplans.org





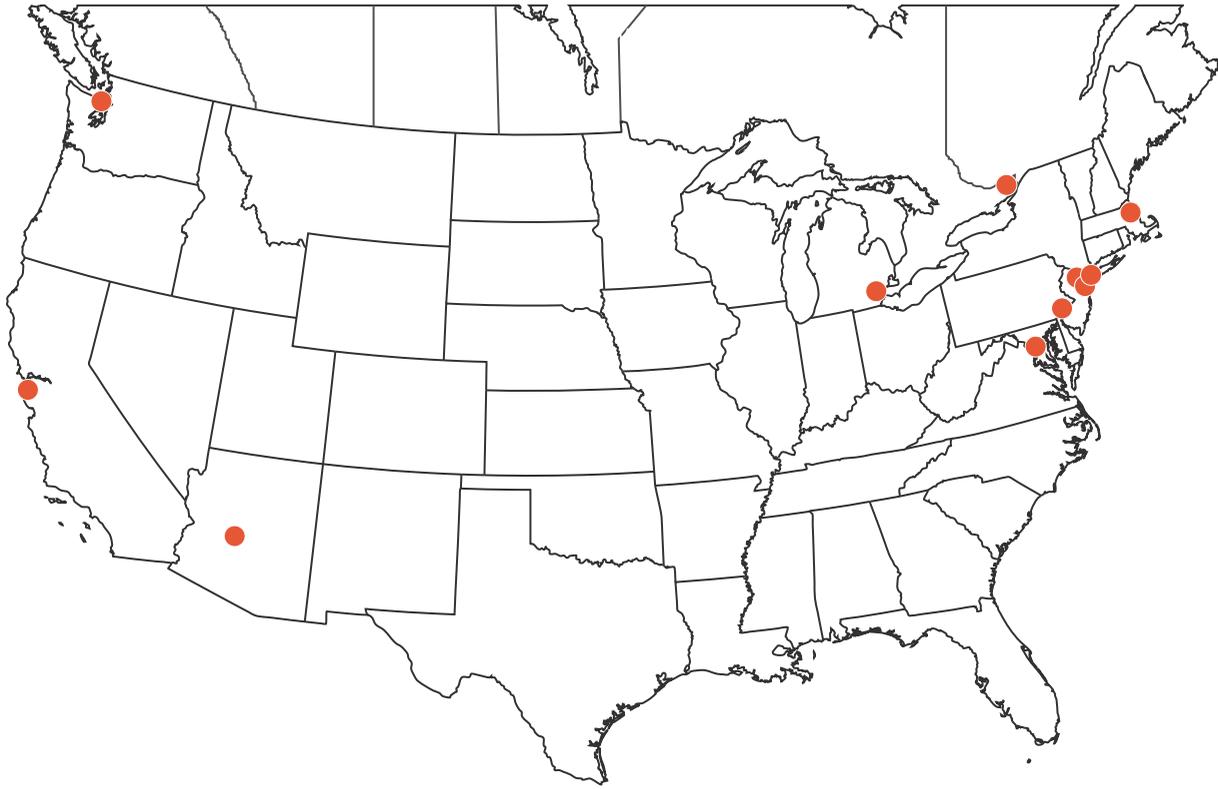
Because there should be one...
Credit: Team Better Block

TACTICS

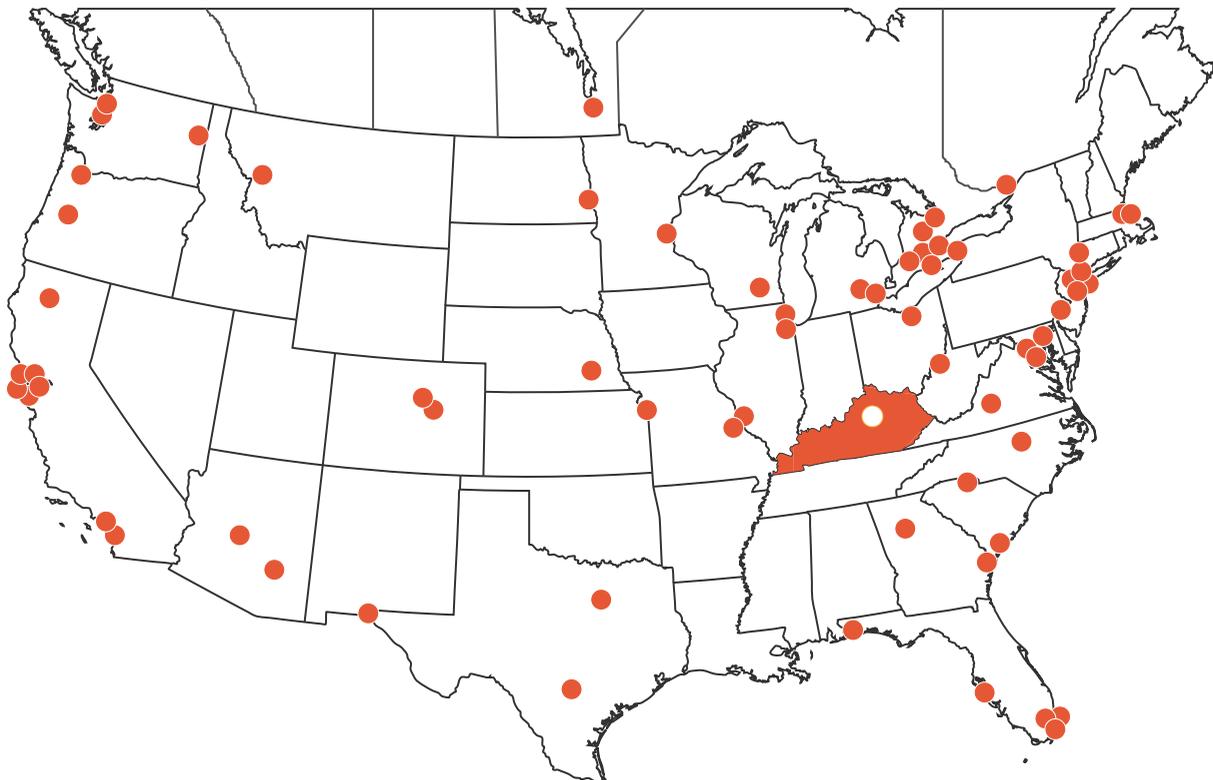
OPEN STREETS	Ten
PLAY STREETS	Twelve
BUILD A BETTER BLOCK	Thirteen
PARK(ING) DAY	Fourteen
GUERILLA GARDENING	Sixteen
POP-UP RETAIL	Seventeen
PAVEMENT TO PLAZAS	Eighteen
PAVEMENT TO PARKS	Twenty
POP-UP CAFES	Twenty One
DEPAVE	Twenty Two
CHAIR BOMBING	Twenty Four
FOOD CARTS/TRUCKS	Twenty Six
SITE PRE-VITALIZATION	Twenty Seven
POP-UP TOWN HALL	Twenty Eight
INFORMAL BIKE PARKING	Thirty
INTERSECTION REPAIR	Thirty One
AD-BUSTING	Thirty Two
RECLAIMED SETBACKS	Thirty Four
PARK MOBILE	Thirty Five
WEED BOMBING	Thirty Six
MOBILE VENDORS	Thirty Eight
MICRO-MIXING	Thirty Nine
PARK-MAKING	Forty
CAMPS	Forty Two



OPEN STREETS SUMMARY MAP - 2005



OPEN STREETS SUMMARY MAP - 2011



OPEN STREETS

PURPOSE: To temporarily provide safe spaces for walking, bicycling, skating and social activities; promote local economic development; and raise awareness about the detrimental effects of the automobile on urban living.

LEADERS: City departments
Politicians
Advocates
Non-Profits

SCALE: City || District || Corridor

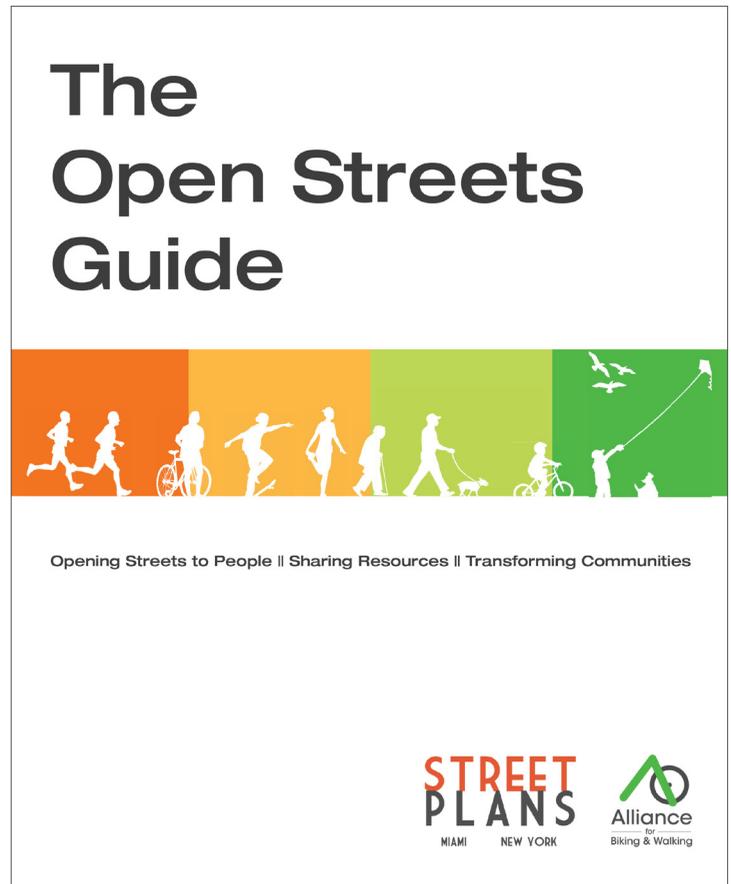
FACT: 50 of the 70 known North American open streets initiatives began within the last three years.

Open Streets initiatives are increasingly common in cities seeking innovative ways to meet environmental, social, economic, and public health goals. Open streets are often referred to as “ciclovía,” which in Spanish translates literally as “bike path.” The origin is largely thought to be Bogotá, Colombia, a city known worldwide for being a leader of the ciclovía/open streets movement. However, before there was Ciclovía in Bogotá, there was “Seattle Bicycle Sundays,” which first launched in 1965, predating Bogotá’s ciclovía by nearly a decade.

While the benefits of Open Streets initiatives are widely recognized, perhaps the most tangible benefit is the social interaction and activity that develops—thousands of people of all ages, incomes, occupations, religions, and races have the opportunity to meet in the public realm while sharing in physical or social activities. In doing so, participants develop a wider understanding of their city, each other, and the potential for making streets friendlier for people.

The resulting vibrancy therefore enables people to experience their city’s public realm in a different way, which helps build broader political support for undertaking more permanent pedestrian, bicycle, and other livability improvements. In this way, open streets are a tool for building social and political capital, while having very real economic impacts for businesses, vendors, and organizations along the chosen route.

Perhaps Waterloo, ON City Councilor, Melissa Durrell, said it best when describing her city’s Car Free Sunday initiative: “This is about bringing people into the core.”



*The Open Streets Project Guide.
Credit: Street Plans and Alliance for Biking & Walking*



Madison, Wisconsin's Ride The Drive initiative consistently draws 20,000 - 25,000 participants. Credit: Mike Lydon



*Atlanta's Streets Alive! initiative brings people together.
Credit: Mike Lydon*

PLAY STREETS

- PURPOSE:** To create safe spaces for people of all ages to be social and active.
- LEADERS:** Neighborhood / Block Associations
Advocates
City departments
- SCALE:** Street || Block
- FACT:** Many city neighborhoods lack adequate park and open space. Play Streets fill this need by providing a safe space for recreation and community interaction.

“Play streets,” popular in New York City and London, re-purpose the public right-of-way for recreational activities. In essence, play streets create a public playground within a space formerly used for the movement and storage of private automobiles. They often occur seasonally and are typically located adjacent to schools or in neighborhoods where open space is scarce. When implemented in low-income neighborhoods, they may be paired with farmers’ markets or underutilized school playgrounds, which can have a multiplier effect.

In New York City, a ‘play street’ is made possible when 51% of the residents living on a one-way residential block sign a petition and offer it to their local police and transportation officials, who then send it to the local Community Board for review. If approved, the City provides youth workers to oversee the program. Approximately 75% of these initiatives are organized by the New York City Police Athletic League, which began organizing play streets in 1914.

New York’s Jackson Heights neighborhood demonstrates how an incremental approach may bring forth permanent change. In 2008 and 2009 a single block of 78th street (located next to a school, between Northern Boulevard and 34th street) was closed to motorists on Sundays-only during the spring, summer, and fall months. Instantly loved by community members, the block was then closed in July and August to motor vehicle traffic every day during the following year. Succeeding once again, the play street expanded to include the month of September in 2011 so that public health officials could study how the street closure works when school is in session. If deemed successful by the City, the street will become permanently closed to motor vehicles, while remaining fully open to people.



A London play street.
Credit: streetplaylondon.blogspot.com



In Jackson Heights, car-free space creates carefree play space.
Credit: Clarence Eckerson



Play streets create playgrounds where they don't currently exist
Credit: New York Times

BUILD A BETTER BLOCK

PURPOSE: To promote livable streets and neighborhood vitality.

LEADERS: Local Advocates
Local Businesses

SCALE: Street || Block || Building

FACT: Fort Worth's Better Block Project was "built" using only \$500 worth of materials. Today, some of the changes proposed are being made permanent by the City of Fort Worth.

Local community activists in the Dallas neighborhood of Oak Cliff launched the Build a Better Block project. Spearheaded by Go Oak Cliff, the organization relied upon cheap or donated materials, and the work of many volunteers to transform a single underutilized urban block.

In short, Build a Better Block encourages local activists and property owners to temporarily activate vacant storefronts and public space. In Dallas, the first effort utilized food vendors and sidewalk cafe tables as places to congregate. "New York style" cycle tracks painted along the curb pushed cars outward to reduce the number of travel lanes, which effectively slowed traffic. Finally, native landscaping and street furniture helped improve the block's sense of place.

To date, the Build a Better Block effort has had a substantial spin-off effect: the momentum gained from the project led to the permanent use of formerly underutilized retail space. Additionally, the process helped advocates in Dallas and Fort Worth garner a commitment from their leaders to permanently implement complete street improvements. It has also spurred a new consultancy firm, called Team Better Block. They are now advising other organizations and cities to use the low-cost, low-risk process to to incite change.

As a touchstone of the tactical urbanism movement, the Better Block approach continues to capture the attention of urbanists and advocates. Similar efforts have now taken place in Fort Worth, Oklahoma City, Philadelphia, Memphis, and beyond.

In Oyster Bay, NY, a similar strategy deployed by DoTank and Street Plans utilized "pop-up shops" and a farmers' market, which have since become permanent. This includes Billy Joel's 20th Century Cycles, a motorcycle showroom that has become a regional draw. Joel participated in the initial event and was struck by the potential of the space he now rents.



Before and after: Dallas Build a Better Block
Credit: Go Oak Cliff

You are here: [Home](#) > [Architecture & Urban Design](#) > South Main Loses Highway Designation – Door Now Open for Complete Streets Makeover, Bike Lanes On the Way

South Main Loses Highway Designation – Door Now Open for Complete Streets Makeover, Bike Lanes On the Way

Written by Kevin Buchanan on January 11, 2011



The Fort Worth, TX Build a Better Block effort led to permanent change.
Credit: [fortworthology.com](#)

Search Fort Worthology

Search...

Categories

- Architecture & Urban Design
- Bicycles
- General
- Linklog
- Preservation
- Transit & Infrastructure
- Urban Development

Tags

7th Street
Architecture art
Bicycling
commercial
complete streets
Cultural District
Downtown Election
2011 events Fairmount



In Oyster Bay, Billy Joel rehabbed a vacant storefront for his motorcycle collection, which is now a regional draw. Credit: Mike Lydon



A street temporarily transformed.
Credit: Art Monaco Portland via my.parkingday.org

PARK(ING) DAY

- PURPOSE:** To reclaim space devoted to automobiles, and to increase the vitality of street life
- LEADERS:** Advocates
Non-Profits
Community Groups
- SCALE:** Street || Block
- FACT:** In 2011, 975 on-street parking spaces were temporarily reclaimed in 165 cities, 35 countries, and across six continents.

PARK(ing) Day is an annual event where on-street parking spaces are converted into park-like public spaces. The initiative is intended to draw attention to the sheer amount of space devoted to the storage of private automobiles.

While its provenance is sometimes debated among advocates in New York and San Francisco, research reveals that Bonnie Ora Sherk, a San Francisco-based artist, first began converting pavement to parks with her 1970 project entitled “Portable Architecture.”

Nonetheless, the outward marketing of the initiative first occurred in 2005 when the interdisciplinary design group Rebar converted a single San Francisco parking space into a mini-park. The group simply laid down sod, added a bench and tree, and fed the meter with quarters. Instantly garnering national attention, PARK(ing) Day has spread rapidly amongst livable city advocates and is thought to be the pre-cursor to New York and San Francisco’s parklet and pavement to parks programs.

At its core, PARK(ing) Day encourages collaboration amongst local citizens to create thoughtful, but temporary additions to the public realm. Once reclaimed, parking spaces are programmed in any number of ways; many focus on local, national, or international advocacy issues, while others adopt specific themes or activities. The possibilities and designs are as endless as they are fun.

While participating individuals and organizations operate independently, they do follow a set of established guidelines. Newcomers can pick up the PARK(ing) Day Manifesto, which covers the basic principles and includes a how-to implementation guide.



*Bonnie Ora Sherk's 1970 Portable Architecture project.
Credit: Bonnie Ora Sherk*



*A PARK(ing) Day installation.
Credit: flickr user iomarch*



*In 2010 a group of non-profit and neighborhood organizations hosted a pot-luck park(ing) day after party below the Brooklyn-Queens Expressway.
Credit: flickr user Brodowski*

GUERRILLA GARDENING

- PURPOSE:** To introduce more greenery and gardening into the urban environment
- LEADERS:** Neighborhood Advocates
- SCALE:** Block || Lot
- FACT:** Guerilla Gardening first began in 1973 when New York City activists threw condoms with local seeds, water, and fertilizer into vacant lots

First coined by Liz Christy and her Green Guerilla group in 1973, guerilla gardening is now an international movement. Although there are many permutations, guerrilla gardening is the act of gardening on public or private land without permission. Typically, the chosen sites are vacant or underutilized properties in urban areas. The direct re-purposing of the land is often intended to raise awareness for a myriad of social and environmental issues, including sustainable food systems, urban storm-water management, improving neighborhood aesthetics, and the power of short-term, collaborative local action.

When applying the tactic to contested land, guerilla gardeners often take action under the cover of night, where vegetables may be sowed, or flower gardens planted and cared for without running the risk of being caught.

Guerilla gardening is an excellent tactic for noticeably improving an urban neighborhood. Often times, gardens are cared for years after they are first created, illegally. Indeed, the first garden started in a vacant New York City lot by the Green Guerillas became so loved that volunteers and the New York City Parks Department now maintain it. This exemplifies how unsanctioned short-term action creates sanctioned, long-term change.



*Guerilla Gardening offers an outlet for creative energy.
Credit: Lorelee Edwards, Lethbridge Guerilla Gardening*



*Green Guerillas at work.
Credit: Guerilla Gardening Development Blog*

POP-UP RETAIL

- PURPOSE:** To promote the temporary use of vacant retail space or lots.
- LEADERS:** Developers
Local Entrepreneurs
Artists
Corporations
- SCALE:** Street || Building
- FACT:** 'Pop-up retail,' was coined in late 2003 by trendwatching.com. The concept of pop-up urban improvement initiatives has since been applied to dozens of other similar temporary interventions.

Apparel companies looking to generate brand awareness first utilized the pop-up retail tactic. At first, such efforts made temporary use of vacant retail spaces by creating an event-like atmosphere for a limited time—sometimes even just one day. For retailers, the focus is generally on marketing or proof-of-concept, rather than producing sales.

Companies large and small, property owners, artists and community organizations quickly adopted and calibrated the concept for a wide range of purposes. To be sure, pop-up retail is moving beyond the common seasonal holiday store and the trendy one-off designer brand sales event to becoming a sanctioned community revitalization technique. Main streets with vacant store fronts, oversized parking lots, and underutilized public spaces are increasingly being used as local incubators for a seemingly endless number of temporary to permanent commercial and community uses.

For example, in Oakland, CA an urban planner and restaurant owner are collaborating in producing Pop-uphood, a three-block stretch of short-term lease pop-up stores intended to revitalize the downtown through new business incubation. In this way, the pop-up trend reflects the new economy where aligning the interests of property owners and local entrepreneurs is happening in a more nimble and creative way.

Finally, the interest and public exposure pop-ups generate by way of their temporary nature provide a powerful tool for sparking long-term change. Successful pop-up stores often earn the right to stay, creating a win-win for the property owner, occupant, and neighborhood.



*Manifesto Bicycles is one of six stores in Oakland's Popuphood.
Credit: Oakland North*



*Melbourne's Rebecca Walk features pop-up retail under a rail viaduct.
Credit: Mike Lydon*



*The solar powered Kioskiosk is a pop-up store that can be setup almost anywhere.
Credit: Kioskiosk*



The iconic, but temporary changes in Times Square will be soon be upgraded and made permanent.
Credit: New York City Department of Transportation

PAVEMENT TO PLAZAS

- PURPOSE:** To reclaim underutilized asphalt as public space without large capital expenditure
- LEADERS:** City Departments
Business Improvement Districts
- SCALE:** Street || Block
- FACT:** Following the implementation of the New Times Square pedestrian plaza, injuries to motorists and their passengers declined by 63%. Similarly, pedestrian injuries decreased 35%, even while pedestrian volumes increased

Pavement to Plaza programs, popularized recently in New York City, but echoed in cities like San Francisco, seemingly define sanctioned tactical urbanism. These interventions typically start by using temporary, inexpensive materials to re-assign excessive motor vehicle space for the use of pedestrians and/or bicyclists. Because these efforts do not require a large outlay of capital, public spaces are able to appear almost overnight. While the city funds the design and the construction, partners from the local business or advocacy community are usually asked to operate, maintain, and manage the new plazas.

Following the immediate closure of Times Square, the centerpiece of New York's highly successful "Greenlight for Midtown" street improvement project, Tim Tompkins of the Times Square Alliance realized that people might want to sit somewhere. So, he bought 376 folding chairs for \$10.74 each and "instantly — millions of people have a new way of enjoying the city."

By taking this experimental "pilot project" approach using temporary materials, the City and public-at-large are able to test the performance of each new plaza without wasting scarce public resources. When successful, the plazas transition into a more permanent design and construction phase, which is happening currently in many of New York City's first generation of pilot plazas and sustainable street projects.



*Broadway at Columbus Circle: Before and after.
Credit: New York City Department of Transportation*



*Putnam Triangle Plaza: Before and after.
Credit: New York City Department of Transportation*

PAVEMENT TO PARKS (PARKLETS)

- PURPOSE:** To reclaim underutilized asphalt as public space without large capital expenditure
- LEADERS:** Local Restaurants
Municipalities
Transportation Departments
Business Improvement Districts
- SCALE:** Street || Block
- FACT:** Startup costs for a “parklet” in San Francisco include a \$1,000 application fee, and a \$650 fee for the removal of parking meters. A \$221 fee is then charged annually. Parallel Park, a parklet in Vancouver, BC cost \$18,000 to design and construct.



One of San Francisco’s many parklets.
Credit: flickr user Jeremy Shaw

The similar Pavement to Plaza projects carried out in New York City inspired San Francisco’s Pavement to Parks program. Using many of the same techniques—moveable tables and chairs, painting asphalt, and the installation of inexpensive planters and re-purposed stone blocks, municipal officials were able to quickly establish a formal program that increased the balance of public space.

The Pavement to Parks program is a collaborative effort between the San Francisco Mayor’s Office, the Department of Public Works, the Planning Department, and the Municipal Transportation Agency. The low-cost “parklets” are now found in more than 20 locations throughout the city.

As in New York, the City views each parklet as a laboratory for testing the potential of a more permanently designed public space. The materials are meant to be temporary and the design malleable should changes be desired during the trial period.

A typical parklet consists of a platform that sits flush with the sidewalk. Each one is built in the place of two or three parking spaces. In most cases, they include seating and various amounts of greenery and occasionally include bicycle parking and/or tables to serve as outdoor dining areas for nearby restaurants or cafes. The cost of constructing a parklet is typically covered by an individual business or several businesses that recognize the ability to attract customers. Privately sponsored or not, parklets are part of the public realm and completely open to the public at large.



Some parklets feature bicycle parking.
Credit: flickr user Jeremy Shaw



Vancouver, BC’s “Parallel Park.”
Credit: Facebook user Parallel Park

POP-UP CAFES

- PURPOSE:** To promote outdoor public seating in the parking lane (during the warm months) and to promote local businesses
- LEADERS:** City Departments
Local Restaurants
Business Improvement Districts
Local Designers
- SCALE:** Block || Street
- FACT::** Parking spaces used for Pop-Up Cafes in NYC are leased from the Department of Transportation

Inspired by New York City's Pavement to Plaza program, San Francisco began its own Pavement to Parks effort, which includes parklets (page 19). That riff on the concept was the imported back to New York an re-named pop-up cafés, which have become specialized tactic used where public outdoor seating is sorely needed.

Similar to how things are set up in San Francisco, New York City's program requires that the business sponsoring the café must agree to cover the design, construction and maintenance costs. If such agreement is reached, the City's Department of Transportation provides technical assistance and may even make street improvements, such as applying traffic markings or placing safety bollards around the cafe.

In cities with a short supply of public space and a need for more publicly accessible seating, pop-up cafés are fast becoming a valued addition to the public realm. When successful, they act as placeholders for a time when city sidewalks can be permanently expanded. Outside of New York City and San Francisco, parklets and pop-up cafés may be found in Long Beach, Los Angeles, and Vancouver.



*A narrow sidewalk limits the possibility of outdoor seating.
Credit: DNAinfo.com*



*Trading parking space for outdoor seating improves the public realm.
Credit: DNAinfo.com*



*Pop-up cafes are sponsored by private businesses, but are public spaces open to anyone
Credit: DNAinfo.com*



Depavers assess their work.
Credit: Brent Wojahn/The Oregonian

DEPAVE

- PURPOSE:** To reduce storm water pollution and increase the amount of land available for habitat restoration, urban farming, tree planting, native vegetation, and social gathering
- LEADERS:** Neighborhood Activists
Non-Profits
- SCALE:** Lot || Block
- FACT:** Since 2007, over 700 volunteer have replaced more than 94,100 square feet of unnecessary asphalt with permeable gardens and community green space. As a result, 2,221,115 gallons of stormwater is diverted annually.

While impervious surfaces are a fact of urban life, the paving of millions of acres contributes to numerous environmental problems, namely the polluting of our waterways through stormwater runoff.

Portland's all-volunteer Depave organization seeks to incrementally reduce stormwater pollution by surgically removing unnecessary pavement. To do so, Depave transforms impervious driveways and parking into community green spaces and gardens that naturally mitigate stormwater runoff pollution.

Depave began as an unsanctioned, self-organized neighborhood effort in 2007, but has blossomed into an influential non-profit organization that has received grants from the U.S. Environmental Protection Agency, the Oregon Department of Environmental Quality, Patagonia, and the Multnomah Soil and Water Conservation Districts. It is also supported by many other businesses, organizations, government departments and schools. Depave therefore provides a great example of how short-term unsanctioned initiatives can become sanctioned, long-term efforts within a very short amount of time.

Over the past four years Depave has turned nearly 100,000 thousand square feet of parking lots into expanded school yards, community gardens, food forests, and pocket parks. While this work has reduced millions of gallons of stormwater runoff, it has also built strong ties between neighbors and the city in which they live.

If you want learn more, Depave written a helpful how-to guide describing their process.



Clear instructions.
Credit: Picasa user DepavePDX



Depaving in action.
Credit: Picasa user DepavePDX



The Fargo Forest Garden replaced 3,000 square feet of asphalt.
Credit: Picasa user DepavePDX



Chairbombing.
Credit: Aurash Khawarзад

CHAIR BOMBING

PURPOSE: To improve the social well-being of neighborhoods by salvaging waste materials and activating the public realm.

LEADERS: Community Activists
Local Property Owners
Small Businesses

SCALE: Street || Building

FACT: By taking discarded shipping pallets and converting them into quality public seating, the urban waste stream can be reduced, and streets made more welcoming.



*Collecting discarded shipping pallets.
Credit: Ted Ulrich Design*

Whether to rest, socialize, or to simply watch the world go by, increasing the supply of public seating almost always makes a street, and by extension, a neighborhood, more livable.

Chairbombing is the act of removing salvageable material from the local waste stream, and using it to build public seating. The entire process of building and placing the chairs requires attention to the design and construction, but also a thoughtful approach as to where they are needed most, and where they would be able to support existing social activity, or serve as a catalyst for community gathering.

In general, chair bombing calls attention to the general lack of public seating in the urban environment. It also indicates locations where further physical improvements may be made by a municipality, property owner, retail tenant, and/or other change agents.

Chair placement begins by retrieving discarded materials such as shipping pallets from dumpsters, construction sites, or other locations where solid waste is found. The pallets can be disassembled and then reassembled for seating.

DoTank, a Brooklyn-based interdisciplinary collective, has placed chairs in six locations throughout northern Brooklyn. While the bombings are often done in conjunction with outdoor events, the first effort has resulted in permanent seating at Blue Bottle Coffee in Brooklyn's Williamsburg neighborhood.



*Chairs placed adjacent to Brooklyn's Blue Bottle Coffee.
Credit: Aurash Khawarзад*



*Permanent seating added by Blue Bottle Coffee.
Credit: Aurash Khawarзад*

FOOD CARTS / TRUCKS

- PURPOSE:** To provide low cost food, incubate small businesses, and activate undertutilized sites.
- LEADERS:** Entrepreneurs
Municipalities
- SCALE:** Block || Lot
- FACT:** Portland's food carts are regulated as normal restaurants, including semi-annual inspections. They also pay rent for the off-street parking spots they occupy.

From construction sites to industrial parks, hospitals to local neighborhood centers, cities large and small continue to witness a surge in street food vending activity.

Long before the Twitter-induced food truck craze, noted urbanist William H. Whyte acknowledged that street food is a magnet for human activity. Food carts and trucks not only stimulate entrepreneurial activity, they serve a critical role in the activation of the public realm. Indeed, few people know where people tend to congregate as well as a good food vendor, as his or her success relies upon high pedestrian volumes. In turn, a good food vendor, or groups of vendors quickly become an additional destination within already successful places of congregation.

In Portland, Oregon, food carts take on a unique character. Typically housed in stationary trailers, RVs, or self-made shacks, the City encourages vendors to cluster their fare. With each new arrival, each food vendor's prosperity often increases. And because they mask surface parking lots, they activate otherwise harsh street edges that repel human activity. In many cases, porches, bar stools and garden seating are added, resulting in an even more inviting streetscape. Interestingly, Portland's food carts are regulated as normal restaurants, including semi-annual inspections. They also pay rent for the off-street parking spots they occupy. Today, the city's most successful food cart entrepreneurs are able to move into more permanent locations, which allows opportunity for the next operator to take their place in a succession of small business development.

From Los Angeles to Miami, smart cities not only lower the barrier to entry, but also nurture such businesses because they reflect and contribute to the strength of the local economy, and enhance the city's sense of place.



Food carts mask surface parking lots in downtown Portland.
Credit: Better Cities & Towns (New Urban Network)



Food carts line the central square in Seaside, FL.
Credit: Mike Lydon



Vendors activate a park edge in Brunswick, ME
Credit: Mike Lydon

SITE PRE-VITALIZATION

- PURPOSE:** To temporarily activate a (re)development site.
- LEADERS:** Municipalities
Developers
Citizen Activists
Business Improvement Districts
- SCALE:** Block || Lot || Building
- FACT:** Red Barn, the developer behind the temporary Hercules Market Hall, sees the project as market research on “what people look for, what they want to eat, the goods they want, and how long they will stay” for a more permanent project they call Market Town.



*The Dekalb Market entrance in downtown Brooklyn
Credit: Mike Lydon*

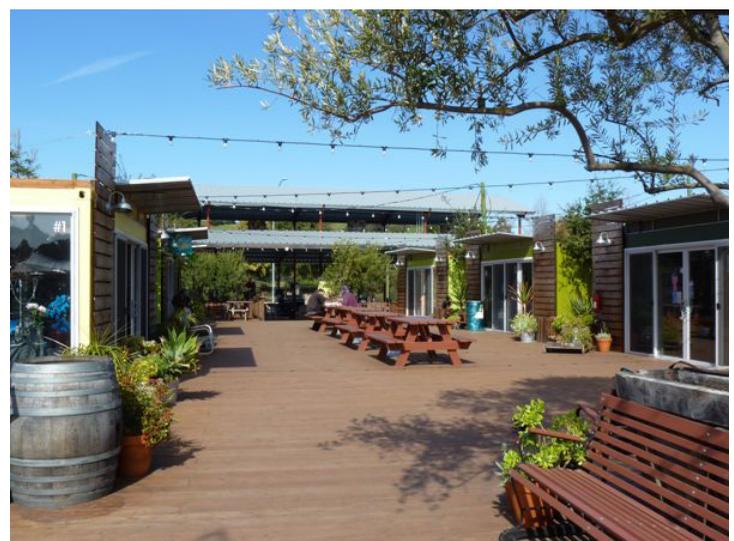
Site Pre-vitalization is the temporary re-activation of a previously inactive, underutilized parcel of land. This tactic brings a variety of art, food, and retail uses to a single location. Typically, this is done to generate needed revenue for the land owner/developer, raise the community’s awareness about the site’s long-term potential, and to build community while supporting local entrepreneurs.

Site pre-vitalization uses often include public markets, art exhibitions and studios, community festivals, beer gardens, micro-retail opportunities, flea markets, and other temporary programs capable of “pre-vitalizing” a site before more permanent building is possible. By activating a site during the planning, approvals, and financing stages, a vacant site can therefore provide low-cost community building and economic opportunities while a more formal transition occurs, from inactive parcel of land to a fully redeveloped and programmed addition to the town or city.

Site Pre-vitalization is a relatively new tactic and one that is largely a private sector response to the restrictive commercial lending standards now imposed by banks. Thus, the tradeoff between the temporary uses—markets, beer gardens, ping-pong tables, art shows, vegetable growing etc.—and the realization of the final long-term private development has not yet been addressed. We’ll be watching this tactic closely as Proxy, Dekalb Market and others like it move from temporary installations to permanent development.



*Proxy is a temporary placeholder for a long-term development plan.
Credit: Inside Scoop SF*



*Hercules Market.
Credit: Dan Gregory*



The BMW Guggenheim Lab, New York City.
Credit: Bob Arihood

POP-UP TOWN HALL

- PURPOSE:** To provide a temporary forum for discussions of civic importance
- LEADERS:** Philanthropic Organizations
Community Coalitions
Activists
- SCALE:** Block || Building
- FACT:** The Guggenheim Lab will travel to nine major world cities in six years.

A pop-up town hall provides an informal, non-government sponsored venue for serious civic discourse and the exchange of ideas. While pop-up town halls often make use of underutilized city spaces, such as vacant lots or storefronts, they can also be held in a myriad of other venues. Pop-up town halls should capitalize on locations where a healthy dose of civic discussion is already occurring; for instance, some pop-up town halls are organized in tandem with conferences, exhibitions, festivals and other events.

The BMW Guggenheim Lab captures the spirit of this tactic. The Lab, which is designed to temporarily re-imagine an overlooked city space as a civic forum, is a migratory town hall that is traveling between nine different cities. The carbon fiber structure is to be disassembled and reassembled in each of the nine cities—it is sometimes referred to as a “traveling toolbox.”

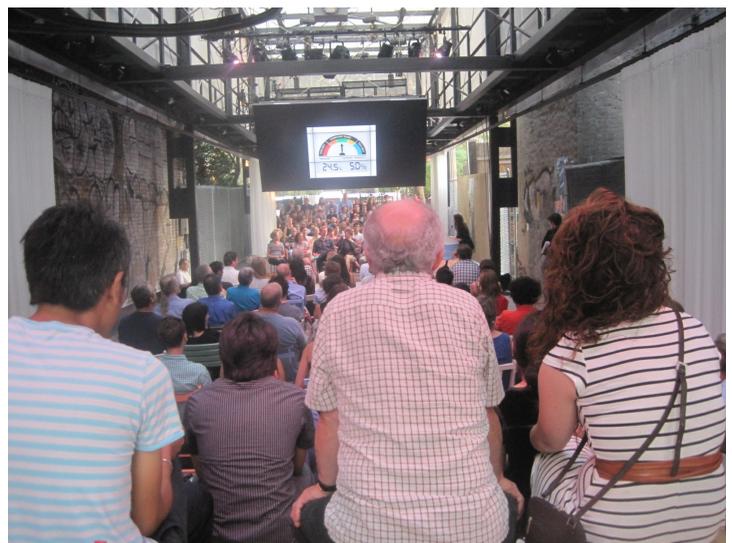
The objective is to provide a space that brings people of many disciplines together brings in a discussion on the future of their city. The organizers of the BMW Guggenheim Lab call their invention “part urban think tank, part community center, and part public gathering space” which are the basic characteristics of any pop-up town hall.



The BMW Guggenheim Lab site before construction began,
Credit: wallpaper.com



A temporary cafe was built adjacent to the Lab.
Credit: © flickr user archidose



Over 100 free events were hosted at the Lab.
Credit: Mike Lydon

INFORMAL BIKE PARKING

- PURPOSE:** To increase the supply of bicycle parking where needed.
- LEADERS:** Community Activists
Local Businesses
Property Owners
- SCALE:** Street || Block || Building
- FACT:** Every parked bicycle belongs to a potential customer.

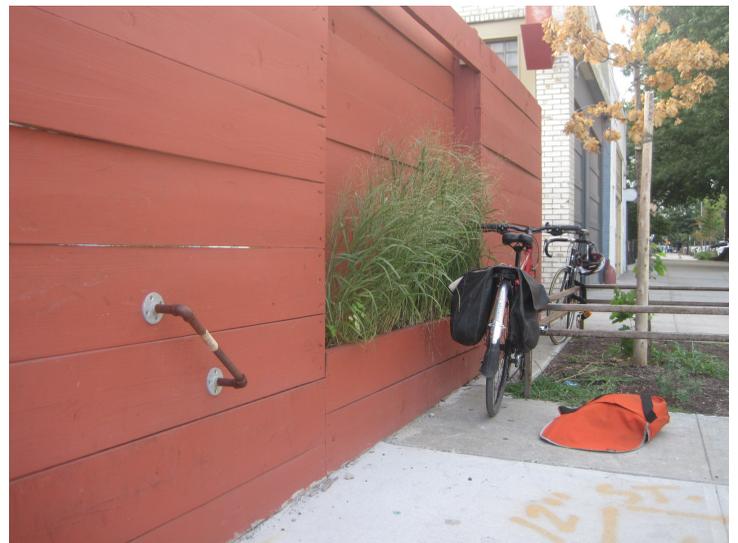
At the intersection of tactical urbanism and transportation infrastructure is the creation and installation of informal bike racks. The lack of parking options in many cities is inspiring activists, including progressive businesses and “enlightened” property owners, to take it upon themselves to install various types of temporary and semi-permanent solutions.

Groups like DoTank have built bike racks and tested them on lamp posts in Williamsburg, with the intent of providing bike parking while also making a statement about the city’s lack of bicycle parking. Businesses such as ¿Por Que No? taqueria in Portland, have also taken it upon themselves to install bike parking in front of their store as a tactic for attracting and servicing customers from the city’s growing bicycle community. Finally, property owners, such as Green Desks in Brooklyn’s DUMBO neighborhood, have affixed “bike rails” directly adjacent to the entrance of the building to meet growing demand.

While not all informal bike racks designs are practical or secure for long-term use, they do indicate demand. Smart cities will leave the well-designed racks alone, while replacing the more poorly conceived temporary solutions with higher performing city racks.



*Valet event parking by The Street Plans Collaborative.
Credit: Mike Lydon*



*Small bike rails provide bicycle parking at a popular beer garden.
Credit: Mike Lydon*



*To meet demand, this property owner installed a long bike rail.
Credit: Ronald Woudstra*

INTERSECTION REPAIR

- PURPOSE:** To repurpose neighborhood street intersections as community space.
- LEADERS:** Neighbors
Activists
Community Organizations
Home Owners Associations
- SCALE:** Street || Block
- FACT:** City Repair and local residents refer to intersection repair as an exercise in “Village Building.”

Intersection repair reclaims neighborhood streets as public gathering places. The initiative first began in Portland, OR and continues to be stewarded by City Repair, a local placemaking group.

While the brightly painted intersections communicate to drivers that they are entering a place of neighborhood importance, it’s the process of making and maintaining the repaired intersections that matters most. Indeed, once the initial reclamation occurs, neighbors often take it upon themselves to further enhance their new public gathering place with benches, community bulletin boards, gardens and art positioned prominently at the corners. In some cases, less temporary paint has given way to bricks and cobblestones.

While the design elements are important, “repaired intersections” encourage neighbors to interact more frequently and give them a nearby place to care about outside their homes.

Intersection repair provides another great example of how tactical urbanism initiatives move along the continuum, from unsanctioned activity to a fully sanctioned program. Unsurprisingly, Portland’s Bureau of Transportation did not initially allow intersection repair. Regardless, communities continued to transform implement them anyway. As is the case with many unsanctioned tactics, the City no longer stands in the way. In fact, the Bureau of Transportation has an official City Repair liaison. This liaison helps coordinate the permitting, street closures, and oversees some of the designs to aid in the success of each project.

Intersection repair projects may now be found in communities across the United States.



*Intersection repair is a placemaking and community building tactic.
Credit: Flickr user Sara Dent*



*An intersection repair project in Los Angeles, CA.
Credit: Los Angeles Eco-Village Blog*



*This is what an intersection repair master plan looks like.
Credit: Sara Dent*



*A billboard turned swingset.
Credit: Broken City Lab*

AD · BUSTING

PURPOSE: To reduce visual pollution within the public realm.

LEADERS: Activists
Municipal Leaders

SCALE: Corridor || Block || Building

FACT: Citizens of São Paulo were amazed at the architecture of their city once billboards blocking building facades were removed.

In 2007, one of Brazil's largest cities adopted a radical ban on outdoor advertising. São Paulo's "Lei Cidade Limpa" or Clean City Law was a project led by mayor Gilberto Kassab and gave the world an example of what a major city could look like without being overrun by advertising.

In the United States, marketing and advertising has pervaded the public realm to such a degree that activists and civil servants have begun to develop and implement creative tactics combating the daily onslaught of consumerism. While the goal of ad-busting is to improve the aesthetic quality of public spaces, it also sends the message that the public realm should not be used unabashedly for the selling and consumption of commercial products.

Groups such as the Public Ad Campaign and DoTank have developed physical methods of altering existing ads to reflect a different message—one that highlights local art and/or community events and assets. DoTank's digital community billboard project, called WeSee.U.S., allowed passersby to anonymously submit live photos from their smart phones. The submitted photos were displayed in real-time on an empty billboard, which allowed individuals to share their own visuals with the community at large.

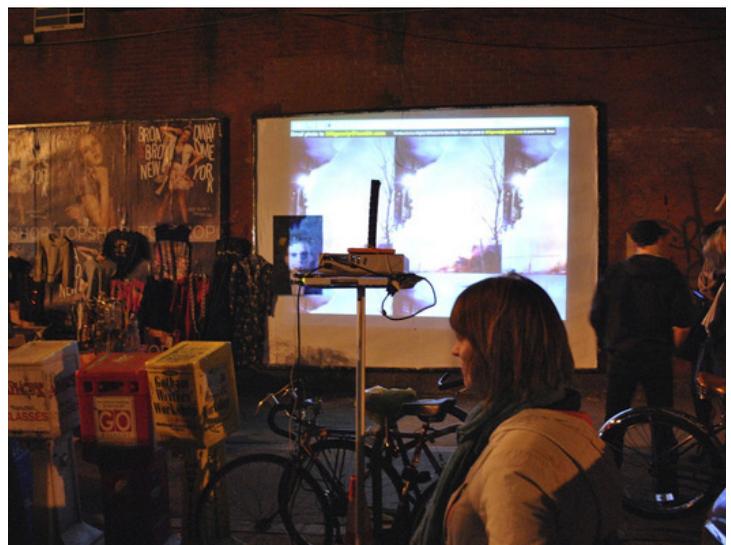
The advertising content generated during the WeSee.U.S. installation quickly became community oriented, focusing on the people and events taking place in the neighborhood rather than on the products corporations urge us to buy.



*What if...
Credit: This is Not an Ad*



*Ad-busting.
Credit: Credit: This is Not an Ad*



*The WeSee.U.S. installation.
Credit: Aurash Khawarзад*

RECLAIMED SETBACKS

- PURPOSE:** To create a more engaging streetscape by activating the space between the structure and the sidewalk.
- LEADERS:** Property Owners
Activists
- SCALE:** Lot
- FACT:** Most municipal zoning codes require setbacks between the street and the primary structure, which often creates awkward and underutilized semi-public spaces.

Setback reclamations intentionally activate the underutilized, semi-public space found between the public right-of-way (typically the sidewalk) and a property owner's principal structure. The distances are uniformly mandated in municipal zoning codes and homeowner associations often limit their use beyond ornament. Awkward setback distances of 20 feet or more are especially common in single-family neighborhoods built after the 1920s. Increased setback distances broke the traditional relationship between street, building, and building owner. As a result, most social activity is found in the backyard, not the front.

Setback reclamations range from illegal structure extensions to temporary programming to community gardening. Such tactics help diminish the real and perceived distances found between the structure and the sidewalk. If done well, reclamations can effectively create a more engaging and social neighborhood street environment.



*Free poems make for an engaging experience in this Streamboat, CO neighborhood.
Credit: Mike Lydon*



*Front setbacks provide a perfect opportunity to grow food.
Credit: flickr user Shira Golding*

PARK MOBILE

- PURPOSE:** To add more neighborhood green space and to further activate streets with public seating.
- LEADERS:** City Departments
Business Owners
Business Improvement Districts
Neighborhood Organizations
- SCALE:** Street || Block
- FACT:** Each parkmobile costs approximately \$6,000.

One of the most appropriate uses of tactical urbanism is to jumpstart the implementation of long-term vision plans. Parkmobiles, designed by San Francisco-based CMG Landscape Architecture, were installed in the summer of 2011. They are a direct response to the neighborhood's desire for more green space, as voiced in San Francisco's Yerba Buena Street Life strategic plan. Fashioned from custom dumpsters, each parkmobile fits within a single vehicular parking space and contributes to "a vision and road map for a next generation of public space in the Yerba Buena District." Other initiatives featured in the plan include widened sidewalks, mid-block crossings, and the tactical conversion of alleys into plazas or shared streets.

At present there are six parkmobiles being moved periodically around the neighborhood. Each unit contains a different type of vegetation, including Tasmanian Tree Ferns, Strawberry Trees, Yuccas, and shrubs that attract birds and butterflies. In doing so, they highlight the importance of an agreeable pedestrian experience and recognize the importance that vegetation and seating play in creating an attractive environment for pedestrians.

Overall, the initiative pays homage to San Francisco's longstanding tactical tradition of improving the larger urban landscape in small and fluid ways.



*Parkmobiles are, well, mobile.
Credit: Dwell via Miyoko Ohtake*



*A parkmobile located outside of SPUR's urban center.
Credit: Dwell via Miyoko Ohtake*



*A parkmobile working as intended.
Credit: Mark Boster/Los Angeles Times.*



Weed bombing on an overgrown corner in downtown Miami (Not photoshopped).
Credit: Grant Stern

WEED BOMBING

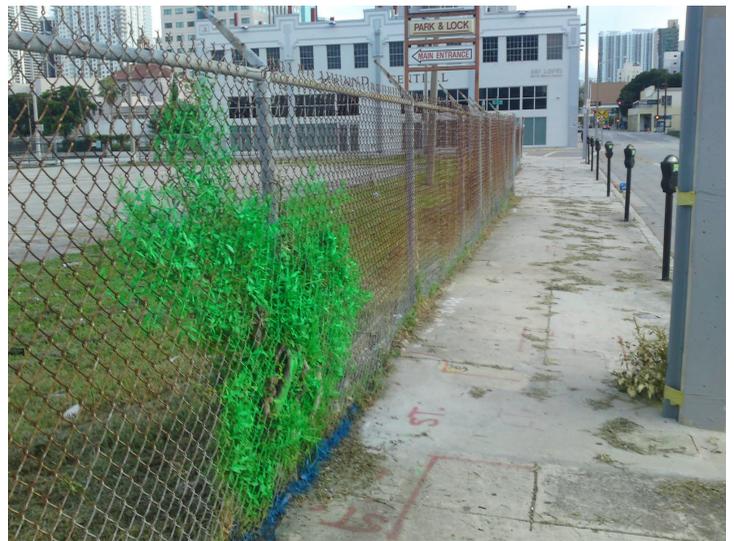
- PURPOSE:** To draw attention to blighted neighborhoods and to incite action in cleaning them up.
- LEADERS:** Neighborhood Associations
Artists
Activists
- SCALE:** Street || Block
- FACT:** Weed bombing began in Miami, FL under the cover of darkness, but sheds light on public and private property negligence.

Weed bombing is the act of converting overgrown weeds into works of street art. Inspired by other forms of 'tactical bombings,' downtown Miami resident and business owner, Brad Knoefler enlisted other neighborhood activists and artists to spray paint weeds in bright colors. Knoefler, who is also the founder of the Omni Parkwest Redevelopment Association (OPRA), is a vocal critic of the various large-scale redevelopment plans slated for his Omni Parkwest, and the lack of maintenance efforts. He is also known to take action into his own hands. "We used to cut the weeds ourselves," say Knoefler, "but it's much more beneficial to beautify them and convert them into street art. Unlike traditional graffiti, weed bombing doesn't damage private or public property and has immediate benefits to our quality of life."

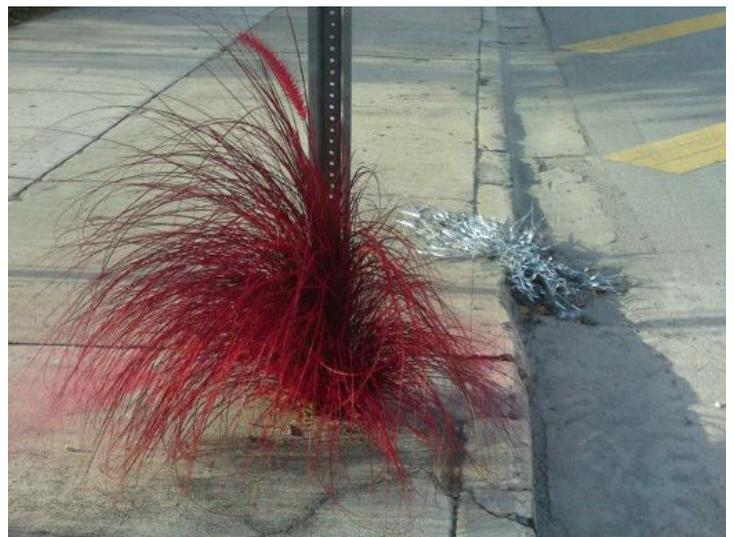
While Knoefler undertook the effort in creative protest, he's found little resistance from the city and downtown development authority. Still, according to one newspaper article, Knoefler plans to keep bombing the weeds until a more concerted, sanctioned city effort is made.



Weed bombing can make overgrowth look more like flowers.
Credit: Kerry McLaney



A weed bomb just north of downtown Miami.
Credit: Kerry McLaney



Weed bombs highlight disparities in where public and private maintenance dollars are spent.
Credit: Kerry McLaney

MICRO-MIXING

- PURPOSE:** Too incubate new businesses and sustain existing ones through the co-location of mutually supportive uses.
- LEADERS:** Entrepreneurs
Property Owners
Business Improvement Districts
- SCALE:** Block || Building
- FACT:** The simplest form of retail micro-mixing is the coffee shop inside the book store, something corporations figured out long ago.

By mixing multiple businesses in a single retail space, micro-mixing provides entrepreneurial shop owners an opportunity to mitigate the high overhead costs associated with opening a new venture. This tactical retailing technique combines complimentary uses and activities that can be used by existing retailers as well as cooperative and startup incubator spaces. The best use of the micro-mixing technique can create some of the most exciting and memorable shopping experiences.

The success of large format bookstores in the 1990's and 2000's illustrated the mutually beneficial relationship that exists between a coffee shop and a bookstore. These two uses together, a latte and a new book, seem to fit together naturally.

Today, more unconventional and perhaps more exciting mixes are emerging. In Denver, Salvagetti Bicycle Workshop has included an espresso bar on its sales floor. In New York, Barbershops are selling clothing, and jewelry stores are selling Vietnamese sandwiches. In Providence, a craft co-op store is running a gallery out of the back of house space and a coffee shop is selling potted plants and gardening supplies. All of these operators are creatively maximizing the return on their space's floor area by working together with complimentary retailers.

At its core, micro-mixing is a "buddy system" economic development strategy that helps small businesses grow incrementally. By using clever merchandising and efficient space planning, existing retail spaces can quickly be adapted to handle multiple users or businesses. Thus, micro-mixing provides an easy opportunity to test new ideas. If successful, micro-mixing tenants can one day strike out on their own.



Salvagetti Bicycle Workshop
Credit: Panoramio user chachafish



Happy Coffee Espresso Window
Credit: Jonathan Shikes



*The construction of Grand Central Park took one month.
Credit: Grand Central Park*

PARK- MAKING

PURPOSE: To increase the supply of park space by quickly reclaiming undertutilized parcels of vacant land and parking lots.

LEADERS: Activists
Artists
City Departments
Business Improvement Districts
Neighborhood Organizations

SCALE: Lot || Block

FACT: Miami's Grand Central Park is a five-acre park that was built in 30 days.

While it's exciting that the tactic of PARK(ing) Day has led to the development of parklets and pavement to plazas programs, larger public spaces are still needed in many urban neighborhoods. A new response to this challenge is the tactic of park-making, which utilizes the techniques of PARK(ing) Day and deploys them at a larger scale. In Miami, FL, local activists are partnering with private sector companies and government entities to transform some of the city's most overlooked opportunity sites into sizeable parks.

The 5-acre Grand Central Park, pictured at left, is the brainchild of the Miami's most notorious Weed Bomber, Brad Knoefler (see pgs 36-37). Knoefler, and his colleagues at the Omni Parkwest Redevelopment Association (OPRA) took it upon themselves to transform the former Miami Arena site into a much needed park space. After securing a local grant, OPRA procured pro bono landscape design services and signed a multi-year lease with the property owner. Fees generated from programmed events, concerts, and food vendors will sustain the maintenance of the park and pay the lease obligation. Built in early 2012, and in just 30 days, the park has already been a huge success.

Building on this model is Bayfront Parkway, a one-week installation led by Street Plans and the engineering firm C3TS. While several a downtown plans have called for transforming Biscayne Boulevard's median parking lots into a green doorstep for downtown Miami, action has never been taken...until now. At the time of publication, the one-block park intervention is currently underway and generating support from stakeholders to follow through with the long-term park plan.



*In Miami, Biscayne Boulevard's medians are dedicated parking lots.
Credit: Ana Bikic*



*Biscayne Parkway transformed one parking lot into a park.
Credit: Ana Bikic*



*Biscayne Parkway's exercise equipment is a popular attraction.
Credit: Ana Bikic*

CAMPS

PURPOSE: To create or take part in the temporary occupation of space, often with an eye toward creating permanent change.

LEADERS: Activists
Protesters
Refugees
Government
NGOs

SCALE: Lot || Building || Block || District

FAST FACT: In his book *Camps: A Guide to 21st Century Space*, author Charlie Hailey organizes 103 types of camps into three broad categories: Autonomy, Control, and Necessity.

Whether for protest, military action, recreation, or necessary shelter, camps provide a ubiquitous cross-cultural reference, and not just one of outdoor occupation. While pervasive the cultural understanding of camps shifts on an almost daily basis. The much debated meaning of the Occupy movement is one of our most recent examples.

According to Hailey, the process of urbanization often begins with informal camps that eventually move along a continuum, from temporary to permanent, from being unsanctioned to sanctioned places of habitation. By their nature, camps capably adapt to various conditions and therefore serve well in the transitional phases of development. In this way, the connections between camp and city is nothing new. Indeed, many of the world's great cities began as Roman military camp outposts, including Vienna, Barcelona, and Prague. A less sexy example is that of the trailer park where the possibility of mobility is often a misnomer—mobile homes don't usually move, they evolve over time and very much stay rooted in place.

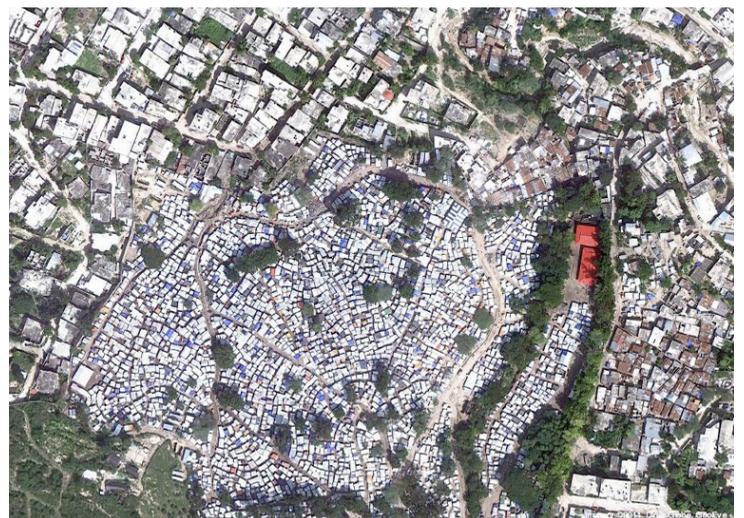
Camps of all types, demonstrate how the short-term action of temporarily occupying space, often leads to incremental, permanent change.



Occupy Camp.
Credit: Chuck Wolfe



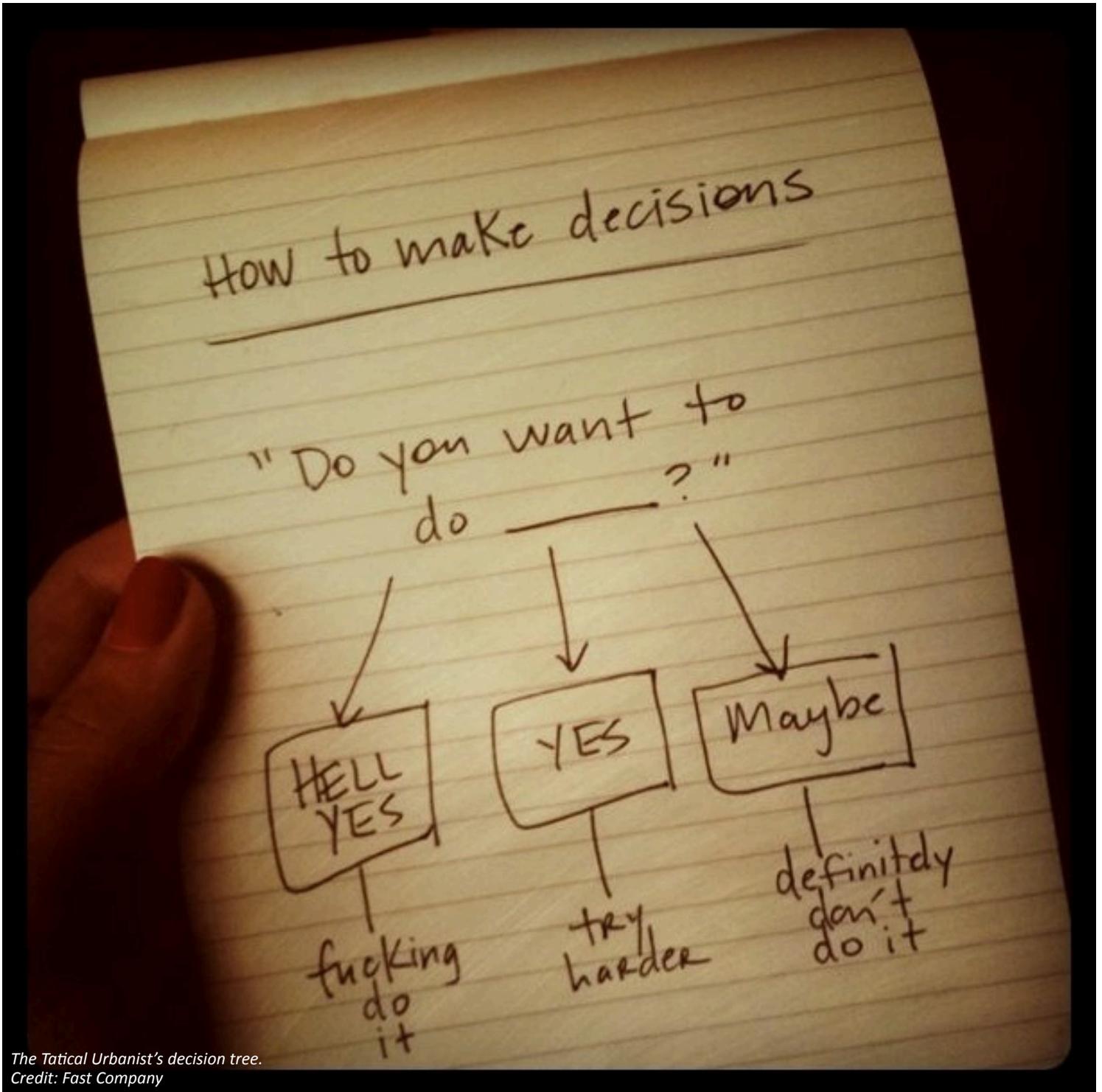
A park in Port-Au-Prince, Haiti before the earthquake.
Credit: New York Times.



Same park in Port-Au-Prince, Haiti as post-earthquake relief camp.
Credit: New York Times

SHARE YOUR TACTICS

This tactical urbanism survey includes several strategies employed by individuals, local community groups, and municipalities. However, it's by no means exhaustive. The Tactical Urbanism Project is only going to get better through reader contributions. If you or someone you know has an addition to make or would like to organize a Tactical Urbanism Salon, please [EMAIL INFO@STREETPLANS.ORG](mailto:INFO@STREETPLANS.ORG) or share it on the Tactical Urbanism Facebook page.



“...in order to do something big, to think globally and act globally, one starts with something small and one starts where it counts. Practice, then, is about making the ordinary special and the special more widely accessible — expanding the boundaries of understanding and possibility with vision and common sense. It is about building densely interconnected networks, crafting linkages between unlikely partners and organizations, and making plans without the usual preponderance of planning. It is about getting it right for now and at the same time being tactical and strategic about later. This is not about forecasting, nor about making decisions about the future. But it is about the long range, about making sure that one plus one equals two or three, about being politically connected and grounded, and about disturbing the order of things in the interests of change.”

- Nabeel Hamdi

Author, Small Change: About the Art of Practice and the Limits of Planning in Cities

SOURCES

TACTICAL URBANISM

The Street Plans Collaborative
Pattern Cities
Tactical Urbanism Salon
The Interventionists Toolkit
Tactical Urbanism Facebook page
Grist
The New York Times
The Atlantic Cities
More Grist

OPEN STREETS

The Open Streets Project

PLAY STREETS

NYC Play Streets
Play Street Becomes a Sanctuary
Reclaiming The Residential Street
As Play Space

PARK(ING) DAY

Parkingday.org

BUILD A BETTER BLOCK

Build a Better Block
Fort Worth Better Block Project
A New Face for an Old Broad

GUERRILLA GARDENING

Guerilla Gardening.org
Wikipedia

PAVEMENT TO PLAZAS

NYC Plaza Program

PAVEMENT TO PARKS

Pavement to Parks Program

POP-UP CAFES

NYC DOT Pop-UP Cafes

CHAIR BOMBING

DoTank

POP-UP RETAIL

Trendwatching

DEPAVE

Depave
Streetfilms: Depaving Day!

FOOD CARTS

Portland Food Carts
The Architectural Maturation of
Portland Food Carts

SITE PRE-VITALIZATION

Dekalb Market
Proxy
Hercules Market: What Comes
Next?
Box Park Shoreditch

POP-UP TOWN HALL

BMW Guggenheim Lab

INTERSECTION REPAIR

City Repair
Streetfilm

AD-BUSTING

Public Ad Campaign

PARK MOBILE

New Forms of Public Space:
Parkmobile

WEED BOMBING

Weed Bombing

MOBILE VENDORS

The Street Vendor Project
Custom Bike Urbanism

MICRO-MIXING

PARK-MAKING

Bayfront Parkway

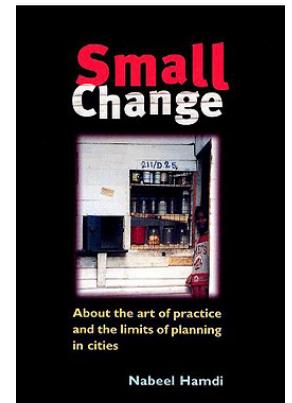
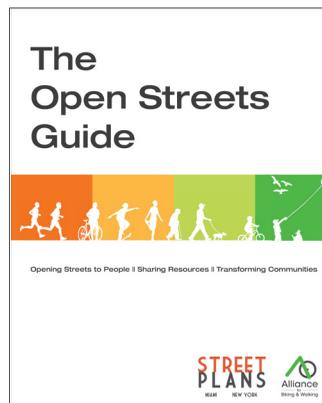
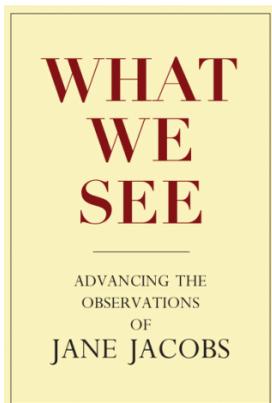
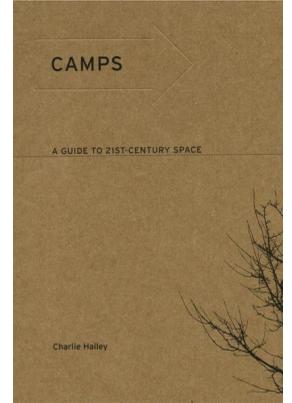
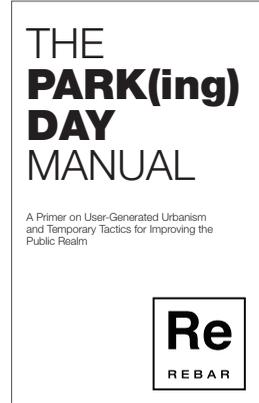
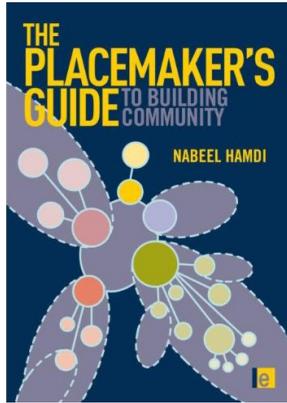
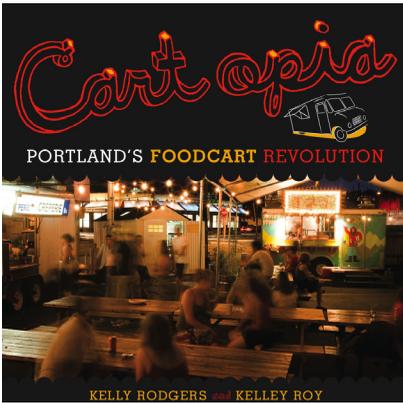
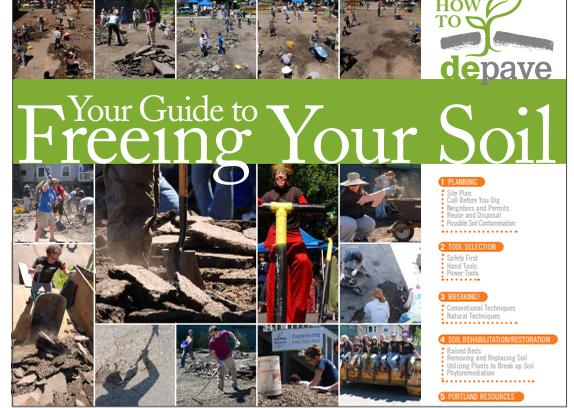
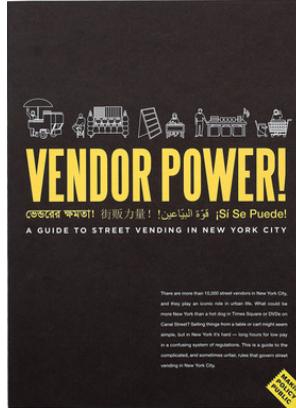
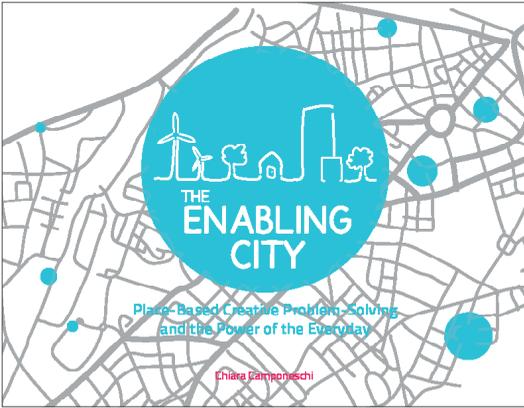
CAMPS

Camps: A Guide to 21st Century
Space



A project of Locke's Department of Urban Betterment.
Credit: Design Taxi

TACTICAL LIBRARY





STREET PLANS

MIAMI NEW YORK





Reviewed By:	
Legal	<input checked="" type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input checked="" type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #8

Tracking Number

EDC 2019-66

Agenda Item Summary Memo

Title: Downtown Landscape Hill Project

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: Bid Request for Downtown Landscape Hill project consisting of a retaining wall sign and base landscaping budgeted for \$50,000.

Council Action Previously Taken:

Date of Action: _____ Action Taken: _____

Item Number: _____

Type of Vote Required: Majority

Council Action Requested: Approval

Submitted by: Krysti J. Barksdale-Noble Community Development
Name Department

Agenda Item Notes:

See attached memo.



Memorandum

To: Economic Development Committee
From: Krysti J. Barksdale-Noble, Community Development Director
CC: Bart Olson, City Administrator
Date: July 31, 2019
Subject: **Downtown Landscape Hill – Request for BID**

Summary

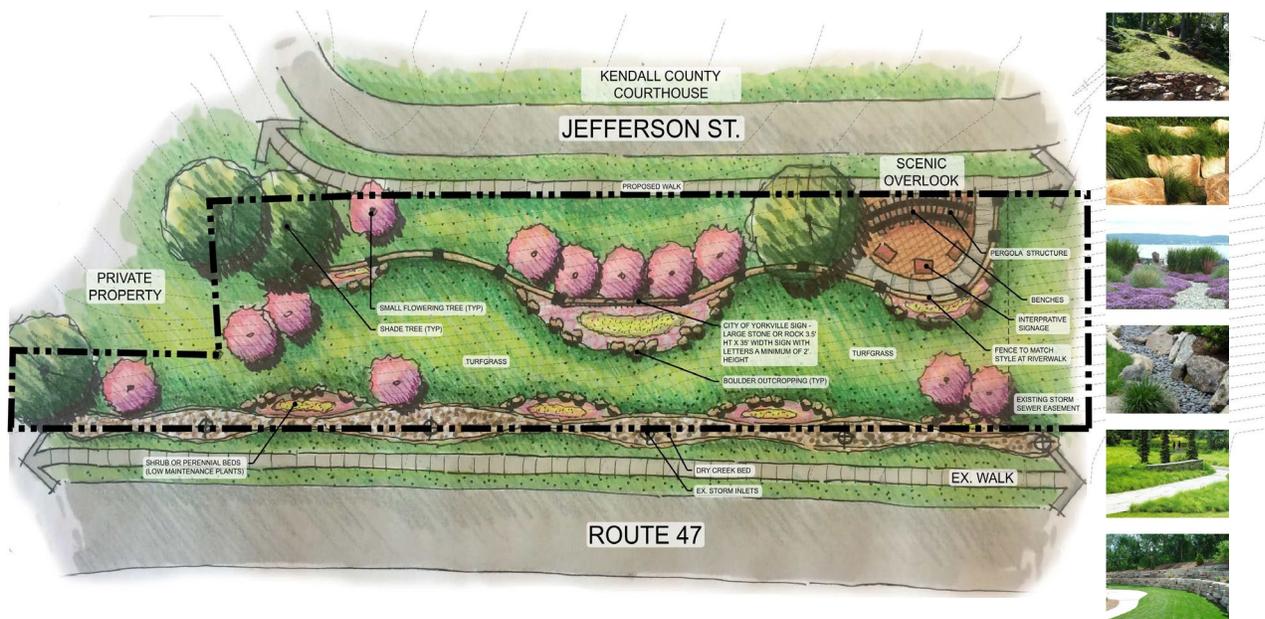
Recommendation to go out for bid on the budgeted approved Downtown Landscape Hill project. The approved budget of \$50,000 will cover a limited scope of the overall concept plan, consisting of the retaining wall sign and surrounding base landscaping.

Background

In April/May 2016, City staff engaged HR Green to prepare a downtown landscape plan that beautifies and improves the visual character of the corridor along IL Route 47 in the downtown to soften the expanse of roadway, create a pedestrian scale experience of greenery and ultimately contributes to an overall sense of place when entering Downtown Yorkville. The area proposed for this landscape feature is the approximately 75' wide, 560' long and 20' tall embankment slope on the west side of IL Route 47/Bridge Street located between Van Emmon and Fox Streets.



The final proposed concept plan prepared by HR Green, as seen on the following page, illustrates a meandering dry creek bed parallel to IL Route 47 flanked by pockets of perennial and/or annual plants for year-round color. The most prominent detail of the plan is the great retaining wall/sign feature facing IL Route 47 constructed of a limestone veneer with individually mounted letters reading “YORKVILLE”. Clusters of plantings near the base of the wall sign will again enhance year-round interest and small stature flowering trees will provide a sophisticated backdrop. Other amenities offered as part of the proposed landscape plan is a scenic overlook in the northwest corner with a pergola structure, benches and fence details which match other decorative fencing in the City along the river walk for continuity and to create a sense of place.



While the estimated total cost of the full proposed landscape hill plan is approximately \$566,000.00, staff is proposing a scaled-down scope of the plan to fit the \$50,000 budgeted in FY 20, as follows:

- | | |
|---|----------------------------------|
| 1. Installation of Landscaping | approx. \$2,975.00 |
| a. Five (5) Ornamental Trees | |
| b. Fifteen (15) shrubs | |
| 2. Installation of Hardscape | approx. \$19,725.00 |
| a. Limestone out-croppings | |
| b. Lighting | |
| 3. Installation of 35' of Retaining Wall/Sign Feature | approx. \$27,300.00 |
| | Total approx. \$50,000.00 |

These estimates were based upon engineer's opinion of probable construction costs provided by HR Green with the 2016 concept plan and should be consistent with current costs for materials and labor.

BID Request

As proposed in the attached draft bid packet, the City is requesting design-build bid proposals for the landscape hill project. The design component of the bid proposal will consist of the responder providing two (2) to three (3) limestone veneer sign wall concepts; while the build component will be cost estimates for the complete installation of the sign wall and landscape materials, in accordance with the approved plan. Staff aims to have the bid proposal posted in August 2019 with responses received by Monday, September 23, 2019.

Once the bids have been submitted, staff proposes to provide all sign wall concepts to the Economic Development Committee for final recommendation to City Council for final bid award. Based upon this schedule, it is anticipated the bid will be awarded in fall 2019 and all work completed by May 1, 2020.

Recommendation

Staff recommends approving the attached Downtown Landscape Hill project bid packet for publication. Should you have any questions regarding this request, staff will be available at Tuesday night's meeting for further discussion.



INVITATION TO BID

THE DESIGN AND INSTALLATION OF THE DOWNTOWN HILL LANDSCAPE PROJECT

Deadline:

**Monday, September 23, 2019
11:30 a.m. Local Time**

UNITED CITY OF YORKVILLE, ILLINOIS
LEGAL NOTICE
INVITATION FOR BID

The United City of Yorkville, Illinois will accept sealed bids for the Design and Installation of the Downtown Hill Landscape Project.

Sealed bids will be received at the address listed below until **Monday, September 23, 2019, at 11:30 a.m. local time**. Bids will be publicly opened and read aloud at this time.

All proposals should be addressed to:

United City of Yorkville
Re: (Contractor name)
Sealed Bid Downtown Hill Landscape Project
Attention: Carri Parker, Purchasing Manager
800 Game Farm Road
Yorkville, IL 60560

Proposal packets are available online at <http://www.yorkville.il.us>. The link can be found under the Business Tab-Bids & RFPs. Additional packets may be picked up at the United City of Yorkville City Hall, 800 Game Farm Road, Yorkville, IL 60560. Please contact the Purchasing Manager to schedule a time to pick up the packet.

Any questions or interpretation request regarding this Legal Notice or actual bid specifications shall be directed to Carri Parker, Purchasing Manager in writing at cparker@yorkville.il.us, not less than ten (10) business days prior to the scheduled bid opening date.

The contractor shall at all times observe and conform to all laws, ordinances, and regulations of the Federal, State, and City which may in any manner affect the preparation of bids or the performance of the contract.

Carri Parker
Purchasing Manager

INTRODUCTION

Purpose of Request:

The United City of Yorkville’s Comprehensive Plan Update, approved in 2016, identified key planning goals and strategies for creating a “vibrant city” within the downtown district. Revitalizing Yorkville’s downtown is a high priority to its residents and business owners within the area. Therefore, the City is seeking an experienced landscape vendor, offering design-build services, to propose and install a retaining wall identification sign and surrounding plantings on a berm within the center of the downtown. This placemaking and streetscape design treatment will enhance the visual appearance, pedestrian environment and functionality of downtown Yorkville.

Project Description:

The City is requesting design-build bid proposals for the landscape hill project in the Downtown area, according to the Project Specifications and Plans contained herein (the “Project”).

The subject project area is located on a grass embankment, approximately 75 feet wide, 560 feet long and 20 feet tall, on the west side of Illinois Route 47/Bridge Street, beginning at the southwest corner of IL Rte. 47 and East Van Emmon Street and continuing south. The property is owned by the Illinois Department of Transportation (“IDOT”) and is maintained by the United City of Yorkville.



The design component of the bid proposal will consist of no less than two (2), but up to three (3) limestone veneer sign wall design concepts (3.5’ height x 35’ linear feet width) with costs for construction and complete installation of the landscape materials in accordance with the approved plan. All work will be completed by May 1, 2020.

PROJECT SPECIFICATIONS AND PLANS

Scope of Work:

The City has a completed final concept “Yorkville Downtown Landscape Plan” designed and approved by the City Council. The approved plan is attached. The scope of work will focus only on the indicated area of the final concept plan which includes the following specifications provided on the bid sheet.

Installation:

The contractor shall install all items that are designated per the plan, depending on scope of the project.

Requirements shall be as follows:

- A. The Contractor shall have the locations surveyed for underground utilities (JULIE) prior to starting work.
- B. All plant material will be locally grown and will be inspected by the City Representative prior to planting.
- C. Plant beds shall be prepared prior to planting according to specifications.
- D. Plant material will be planted per design and in accordance to “National Standards” with all containers, the top 1/3 burlap, twine and/or baskets being removed. A “water holding material” will be added to each hole prior to planting, as approved by the City.
- E. A minimum of 3” hardwood mulch will be added around each plant and/or plant bed.
- F. After planting, each plant will be watered accordingly.
- G. Boulder Outcropping, Retaining Wall & Signage will be installed according to plan.
- H. Any topsoil used shall be pulverized and free of any foreign material.

Site Restoration:

The Contractor will be responsible for the repairs or other damages that might be caused during the execution of this Contract.

The Contractor shall insure that all disturbed areas be re-seeded with the same grass seed and blanket. Site cleanup shall take place at the completion of the project with all materials and debris generated during the job, be removed from the work areas. This includes any parking lots, sidewalks, rights-of-way and any other areas affected by the work. If site cleanup is to be delayed for any particular reason, barricades, cones and/or caution tape must be used until the site is clean as designated by the City’s Representative. Upon completion of the entire project, the sites outside the scope of the project should be returned to the same condition that existed prior to work being done.

Maintenance:

The Contractor will be responsible for plant material maintenance for a period of 60 days after final acceptance.

Guarantees:

All plant material and workmanship shall be guaranteed for one (1) year, at time of City's acceptance of the completed project.

Bid Schedule:

Selection Process Steps	Estimated Date(s)
Committee Approval to Release	
City Council Approval to Release	
Release of Invitation to Bid (ITB)	
Optional Pre-Bid Meeting	
Final Date for Contractors to Submit Questions	
Addendum Posted	
Proposals Due	
Committee Approval	
City Council Approval	
Mandatory Pre-Season Meeting	

INSTRUCTIONS TO CONTRACTORS - GENERAL CONDITIONS

These Instructions to Contractors and General Conditions apply to all bids requested and accepted by the City and become a part of the contract unless otherwise specified. Contractors or their authorized representatives are expected to fully inform themselves as to the conditions, requirements, and specifications before submitting bids. The City assumes that submission of a bid means that the Contractor has familiarized himself with all conditions and intends to comply with them unless noted otherwise.

1. **Forms** – All bids must be submitted on the forms provided, complete with all blank spaces filled in and properly signed in ink in the proper spaces and submitted in a sealed envelope. All bid forms may be obtained from the United City of Yorkville located at 800 Game Farm Road, Yorkville, IL 60560. Please contact the Purchasing Manager to schedule a time to pick up the packet. When completed, deliver the packet to the United City of Yorkville, Attn. Carri Parker, Purchasing Manager, 800 Game Farm Road, Yorkville, IL 60560, prior to the bid opening date and time. Bids must be identified as such on the outside of the sealed envelope by marking the envelope “SEALED BID” and with the following information: Company’s name, address, item bid, date and time of opening. Contractors may attach separate sheets for the purpose of explanation, exception, or alternative proposal and to cover required unit prices.
2. **Submittal of Bid** – Bids must be submitted to the attention of the Carri Parker, Purchasing Manager at the above address no later than **Monday, September 23, 2019, at 11:30 a.m. local time**. Bids arriving after the specified time will not be accepted. Mailed bids which are delivered after the specified hour will not be accepted regardless of postmarked time on the envelope. Contractors should carefully consider all bid delivery options and select a method that will successfully deliver their bid by the required time and date.
3. **Examination of Bid Forms, Specifications, and Site** – The contractor shall carefully examine the bid forms which may include the invitation to bid, instruction to contractors, general conditions, special conditions, plans, specifications, bid form, bond, contract, and any addenda to them before submitting the bid. The contractor shall verify all measurements relative to the work, shall be responsible for the correctness of same. Failure of the contractor to notify the City, in writing, of any condition(s) or measurement(s) making it impossible to carry out the work as shown and specified, will be construed as meaning no such conditions exist and no additional money will be added to the contract.

The submission of the bid shall be considered conclusive evidence that the contractor has investigated and is satisfied as to all conditions to be encountered in performing the work, and is fully informed as to character, quality, quantities, and costs of work to be performed and materials to be furnished, and as to the requirements of the bid forms. If the bid is accepted, the contractor will be responsible for all errors in his proposal resulting from his failure or neglect to comply with these instructions, and the City shall not be responsible for any charge for extra work or change in anticipated profits resulting from such failure or neglect.

4. **Scope of Work** – The contractor shall supply all required supervision, skilled labor, transpiration, new materials, apparatus, and tools necessary for the entire and proper

completion of the work. The contractor shall supply, maintain and remove all equipment for the performance of the work and be responsible for the safe, proper and lawful construction, maintenance and use of the same. This work shall be completed to the satisfaction of the City. The City is not responsible for site safety. The contractor is solely and exclusively responsible for construction means, methods, and technologies.

5. **Interpretation of Bid Documents** – Questions regarding bid documents, discrepancies, omissions, or intent of the specifications or plans shall be submitted in writing to the Purchasing Manager at cparker@yorkville.il.us, at least ten (10) working days prior to the opening of bids to provide time for issuing and forwarding an addendum. Any interpretations of the Contract Documents will be made only by addendum duly issued or delivered by the City to each person receiving a set of bid documents. The City will not be responsible for any other explanations for interpretations of the Contract Documents.

Letters, requested interpretations, clarifications, and/or explanations shall be so noted on the outside of the envelope and on the first page of the letter with the words, **INTERPRETATION REQUEST**. Letters not properly marked will not be considered as a formal request. Any letter received within ten working days of the bid date will be returned unopened.

6. **Bid Guarantee** – Unless specifically waived, each bid shall be accompanied by a bid security in an amount of ten percent (10%) or such other percentage as stated in the supplementary conditions of the full amount of the bid in the form of a certified or bank cashier's check or bid bond. In a reasonable time after the bid opening, bid deposits of all except the three lowest responsible bids will be released. The remaining deposits will be released after the successful contractor has entered into the contract and furnished the required insurance and bonds. The bid deposit shall become the property of the City if the successful contractor within fourteen (14) days from awarding the contract refuses or is unable to comply with the contract requirements, not as a penalty, but as liquidated damages.
7. **Receiving Bids** – Bids received prior to the time of opening will be securely kept, unopened. The Purchasing manager, whose duty it is to open them will decide when the specified time has arrived, and no bid received thereafter will be considered. No responsibility will be attached to the Purchasing Manager or the City for the premature or nonopening of a bid not properly addressed and identified, except as otherwise provided by law.
8. **Late and Fax Bids** – Bids arriving after the specified time, whether sent by mail, courier, or in person, will not be accepted and will be refused and returned unopened. It is the contractor's responsibility for timely delivery regardless of the methods used. Mailed bids which are delivered after the specified hour will not be accepted regardless of postmarked time on the envelope. Facsimile machine transmitted bids will not be accepted, nor will the City transmit bid documents to prospective contractors by way of a facsimile machine.
9. **Completeness** – All information required by the Invitation to Bid must be supplied to constitute a responsive bid. The Contractor's submittal shall include the completed Bid Proposal found in the contract documents. The City will strictly hold the contractor to the terms of the bid. The bid must be executed by a person having the legal right and authority to bind the contractor.

10. **Error in Bids** – When an error is made in extending total prices, the unit bid price and/or written words shall govern. Otherwise, the contractor is not relieved from errors in bid preparation. Erasures in bids must be explained over signature of contractor.
11. **Withdrawal of Bids** – A written request for the withdrawal of a bid or any part thereof may be granted if the request is received by the Purchasing Manager prior to the specified time of opening. After the opening, the contractor cannot withdraw or cancel his bid for a period of forty-five (45) calendar days, or such longer time as stated in the bid documents.
12. **Contractor Interested in More than One Bid** – Unless otherwise specified if more than one bid is offered by any one party, by or in the name of his or their agent, partner, or other persons, all such bids may be rejected. A party who has quoted prices on work, materials, or supplies to other contractors is not thereby disqualified from quoting prices to other contractors or from submitting a bid directly for the work, materials, or supplies.
13. **Contractor's qualifications** – No award will be made to any contractor who cannot satisfy to the City that they have sufficient ability and experience in this class of work, as well as sufficient capital and equipment to do the job and complete the work successfully within the time named (i.e., responsible). The City's decision or judgment on these matters shall be final and binding. The City may make such investigations as it deems necessary. The contractor shall furnish to the City all information and data the City may request for the purpose of investigation.
14. **Bid Award for All or Part** – Unless otherwise specified, bids shall be submitted for all of the work or items for which bids are requested. The City reserves the right to make award on all items, or any of the items, according to the best interests of the City.
15. **Samples** – Samples or drawings requested shall be delivered and removed at no cost to the City. The City shall not be responsible for damage to samples. Samples shall be removed by the contractor within thirty (30) days after notification. Samples must be submitted prior to the time set for the opening of bids.
16. **Equipment or Materials** – Each contractor shall submit catalogs, descriptive literature, and detailed drawings necessary to fully describe those features or the material or work not covered in the specifications. The parts and materials bids must be of current date (latest model) and meet specifications. This provision excludes surplus, remanufactured, and used products except as an alternate bid. The brand name and/or manufacturer of each item proposed must be clearly stated in the bid. Guarantee and/or warranty information must be included with this bid.
17. **Toxic Substance** – Prior to delivery of any material which is caustic, corrosive, flammable or dangerous to handle, the supplier shall provide written directions as to methods of handling such products, as well as the antidote or neutralizing material required for its first aid (Material Safety Data Sheet).
18. **Delivery** – Where applicable all materials shipped to the City must be shipped F.O.B. delivered, designated location, Yorkville, Illinois. If delivery is made by truck, arrangements must be made in advance by the bidder, with concurrence by the City, for receipt of the materials. The materials must be delivered where directed. Truck deliveries

will be accepted at the Public Works Facility between 7:00 AM and 3:30 PM and at all other City locations 8:00 AM and 4:00 PM, weekdays only.

19. **Estimated Bid Quantities** – On “Estimated Quantities,” the City may purchase more or less than the estimates. The Contractor shall not be required to deliver more than ten (10) percent in excess of the estimated quantity of each item unless otherwise agreed upon.
20. **Trade Names – Alternative Bid** – When an item is identified in the specifications by a manufacturer’s or trade name or catalog number, the contractor shall bid upon the item so identified.

If the specifications state “or equal” bids on other items will be considered, provided the contractor clearly identifies in his proposal the item to be furnished, together with any descriptive matter which will indicate the character of the item.

Contractors desiring to bid on items which deviate from these specifications, but which they believe are equivalent, are requested to submit alternate bids. However, ALTERNATE BIDS MUST BE CLEARLY INDICATED AS SUCH AND DEVIATIONS FROM THE APPLICABLE SPECIFICATIONS PLAINLY NOTED. The bid must be accompanied by complete specifications for the items offered. Contractors wishing to submit a secondary bid must submit it as an alternate bid.

The City shall be the sole and final judge unequivocally as to whether any substitute from the specifications is of equivalent or better quality.

21. **Price** – Unit prices shall be shown for each unit on which there is a bid as well as the aggregate price and shall include all packing, crating freight and shipping charges, and cost of unloading at the destination unless otherwise stated in the bid.

Unit prices shall not include any local, state, or federal taxes. The City is exempt, by law, from paying State and City Retailer’s Occupation Tax, State Service Occupation and Use Tax and Federal Excise Tax. The City will supply the successful contractor with its tax exemption number.

Cash discounts will not be considered in determining overall price but may be used in an overall evaluation.

22. **Consideration of Bid** – No bid will be accepted from or contract awarded to any person, firm or corporation that is in arrears or is in default to the City upon any debt or contract, or that is a defaulter, as surety or otherwise, upon any obligation to the City or had failed to perform faithfully any previous contract with the City.

The contractor, if requested, shall present within 48 hours evidence satisfactory to the City of performance ability and possession of necessary facilities, pecuniary resources, and adequate insurance to comply with the terms of these specifications and contract documents.

23. **Award or Rejection** – The City reserves the right to reject and/or award any and all bids or parts thereof and to waive formalities and technicalities according to the best interests of the City. Any bid submitted will be binding for forty-five (45) days subsequent to the

date of the bid opening. A contract will be awarded to the lowest responsible bid complying with the conditions of the contract documents only when it is in the best interest of the City to accept the bid. The City shall be the sole judge of compliance with the specifications and reserves the right to accept or reject any and/or all bids or parts thereof.

24. **Execution of Contract** – The successful contractor shall, within fourteen (14) days after notification of the award: (a) enter into a contract in writing with the City covering all matters and things as are set forth in the specifications and his bid and (b) carry insurance acceptable to the City, covering public liability, property damage, and workmen’s compensation.

After the acceptance and award of the bid and upon receipt of a written purchase order executed by the proper officials of the City, this Instruction to Contractors, including the specifications, will constitute part of the legal contract between the United City of Yorkville and the successful contractor.

25. **Performance and Labor and Material Payment Bonds** – Unless specifically waived or amended in the Special Conditions, the successful bidder shall furnish at the time of execution of the contract a performance bond for the full amount of the contract acceptable in form and surety to the City to guarantee the completion of any work to be performed by the contractor under the contract

Unless specifically waived or amended in the Special Conditions, the successful bidder shall furnish at the time of execution of the contract a bond for the full amount of the contract acceptable in form and surety to the City conditioned upon the faithful performance and completion of the contract, payment of material used in such work, and for all labor performed in such work including by subcontractors.

In the event that the bidder fails to furnish the bonds within 14 days after notification of the award, then the bid guarantee shall be retained by the City as liquidated damages and not as a penalty. It is agreed that the sum is a fair estimate of the amount of damages that the City will sustain due to the bidder's failure to furnish the bonds.

26. **Compliance with All Laws** – All work under the contract must be executed in accordance with all applicable federal, state, and local laws, ordinances, rules, and regulations which may in any manner affect the preparation of the bid or performance of the contract.

27. **RESERVED**

28. **Compliance with the Substance Abuse Prevention on Public Works Projects Act** – The Contractor and its Subcontractors shall comply with the Substance Abuse Prevention on Public Works projects Act (820 ILCS 265/1 et seq.) and prior to commencing work on a “public works” project (as defined in the Prevailing Wage Act) file with the City its program to comply with the Act or file that portion of its collective bargaining agreement that deals with the matters covered by the Act.

29. **Equal Employment Opportunity** – During the performance of the contract and/or supplying of materials, equipment, and suppliers, bidder must be in full compliance with

all provisions of the Acts of the General Assembly of the State of Illinois relating to employment, including equal opportunity requirements.

30. **Contract Alterations** – No amendment of a contract shall be valid unless made in writing and signed by the City Administrator or his/her authorized agent.
31. **Notices** – All notices required by the contract shall be given in writing.
32. **Nonassignability** – The Contractor shall not assign the contract, or any part thereof, to any other person, firm, or corporation without the previous written consent of the City Manager. Such assignment shall not relieve the Contractor from his obligations or change the terms of the contract.
33. **Indemnity** – To the fullest extent permitted by law, the Contractor hereby agrees to defend, indemnify, and hold harmless the City, its officials, agents, and employees, against all injuries, deaths, loss, damages, claims, patent claims, suits, liabilities, judgments, cost, and expenses, which may in anywise accrue against the City, its officials, agents, and employees, arising in whole or in part or in consequence of the performance of this work by the Contractor, its employees, or subcontractors, or which may anywise result therefore, except that arising out of the sole legal cause of the City, its agents, or employees, the Contractor shall, at its own expense, appear, defend, and pay all charges of attorneys and all costs and other expenses arising therefore or incurred in connections therewith, and, if any judgment shall be rendered against the City, its officials, agents, and employees, in any such action, the Contractor shall, at its own expense, satisfy and discharge the same.

Contractor expressly understands and agrees that any performance bond or insurance policies required by this contract, or otherwise provided by the Contractor, shall in no way limit the responsibility to indemnify, keep, and save harmless and defend the City, its officials, agents, and employees as herein provided.

34. **Insurance** – In submission of a bid, the contractor is certifying that he has all insurance coverages required by law or would normally be expected for contractor's type of business. In addition, the contractor is certifying that he has or will obtain at least the insurance coverages on the attached Insurance requirements.
35. **Default** – The City may terminate a contract by written notice of default to the Contractor if:
 1. The Contractor fails to make delivery of the materials or perform the services within the time specified in the proposal, or
 2. fails to make progress so as to endanger performance of the contract, or
 3. fails to provide or maintain in full force and effect, the liability and indemnification coverages or performance bond as required.

If the City terminates the contract, the City may procure supplies or services similar to those so terminated, and the Contractor shall be liable to the City for any excess costs for similar supplies and services, unless the Contractor provides acceptable evidence that failure to perform the contract was due to causes beyond the control and without the fault or negligence of the Contractor.

36. **Inspection** – The City shall have a right to inspect, by its authorized representative, any material, components, or workmanship as herein specified. Materials, components, or workmanship that has been rejected by the authorized representative as not in accordance with the terms of the specifications shall be replaced by the Contractor at no cost to the City.
37. **Supplementary Conditions** – Wherever special conditions are written into the specifications or supplementary conditions which are in conflict with conditions stated in these Instructions to Contractor, the conditions stated in the specifications or supplementary conditions shall take precedence.
38. **Permits and Licenses** – The successful contractor and their subcontractor(s) shall obtain, at their own expense, all permits and licenses which may be required to complete the contract. Fees for all City permits and licenses shall be waived.
39. **Contractor's Certification** – - In compliance with the Illinois State Law that requires each contractor to file a certification regarding bid rigging and bid rotating and that it is not delinquent in its taxes, the contractor shall file with its bid the attached Bid Proposal Certification.
40. **Change Orders** – After the contract award, changes in or additions to the work and/or a change in the amount of money to be paid to the contractor must be the result of an approved change order first ordered by the Director of the lead department and approved by the City Administrator and/ or City Council.
41. **Time of Completion** – The successful contractor shall completely perform its bid in strict accordance with its terms and conditions within the number of consecutive calendar days after notification of award of the contract as stated in the bid proposal.
42. **Payment** – Payment will be made within thirty (30) days after acceptance of the job by the City after the completion of the work as covered within the contract documents.
43. **Guarantees and Warranties** – All guarantees and warranties required shall be furnished by the successful contractor and shall be delivered to the City before final payment on the contract is issued.
44. **Waiver of Lien** – where applicable a waiver of lien and contractor's affidavit must be submitted by the successful contractor, verifying that all subcontractors and material invoices have been paid prior to the City approving final payment.

SPECIAL CONDITIONS

1. **Contractors Qualifications** - If requested, the interested Contractor must provide a detailed statement regarding the business and technical organization of the Contractor that is available for the work that is contemplated. Information pertaining to financial resources, experiences of personnel, previously completed projects, plant facilities, and other data may also be required to satisfy the City that the Contractor is equipped and prepared to fulfill the Contract should the Contract be awarded to him. The competency and responsibility of Contractors and of their proposed subcontractors will be considered in making awards.

If requested by the City, the Contractor shall include a complete list of all equipment and manpower available to perform the work intended on the Plans and Specifications. The list of equipment and manpower must prove to the City that the Contractor is well qualified and able to perform the work, and it shall be taken into consideration in awarding the Contract. The list shall be included with the sealed bid package.

The City may make such investigations as it deems necessary, and the Contractor shall furnish to the City under oath, if so required, all such information and data for this purpose as the City may request. A responsible Contractor is one who meets all of the following requirements:

- Have adequate financial resources or the ability to secure such resources.
 - Have the necessary experience, organization, and technical qualifications, and has or can acquire, the necessary equipment to perform the proposed Contract.
 - Is able to comply with the required performance schedule or completion date, taking into account all existing commitments.
 - Has a satisfactory record of performance, integrity, judgment, and skills.
 - Is qualified and eligible to receive an award under all applicable laws and regulations.
2. **Basis of Payment** - The Contractor shall submit with each payment request the Contractor's Partial Waiver of Lien for the full amount of the requested payment. Beginning with the second payment request, and with each succeeding payment request, the Contractor shall submit partial waivers of lien for each subcontractor and supplier showing that the amount paid to date to each is at least equivalent to the total value of that subcontractor's work, less retainage, included on the previous payment request. The Contractor's request for final payment shall include the Contractor's Final Waiver of Lien which shall be for the full amount of his Contract, including any change orders thereto, and Final Waivers of Lien from all subcontractors and suppliers for which Final Waivers of Lien have not previously been submitted.
 3. **Project Acceptance Procedures** – All final pay item quantities shall be agreed upon between the City and the Contractor, and a final invoice shall be submitted by the Contractor, complete with all required waivers of lien and surety. Approval of the final pay estimate by the Director of Public Works shall constitute acceptance of the project by the United City of Yorkville, and written notice of such action shall be given to the Contractor. The date of approval of the final pay estimate shall be the Date of Acceptance and shall also be the date of the Start of Guarantee.

Prior to the required Date(s) of Substantial Completion, a partial project acceptance may be granted in accordance with the foregoing procedure for entire project acceptance, with the exception that the pay estimate for the quantities of items included in the partial project acceptance shall not be the final pay estimate for the entire project. In the case of the City's acceptance of any portion of the work as may be required because of the inclusion of a Date of Substantial Completion requirement, or for other reasons as may be agreed to by the United City of Yorkville, such partial acceptance action shall not constitute acceptance of any other portion of the project not noted in the written notice of partial acceptance which shall be provided to the Contractor by the Director of Public Works.

4. **General Guarantee** - Neither the final certificate of payment nor any provision in the Contractor Documents, shall constitute an acceptance of work not done in accordance with the Contract Documents or relieve the Contractor of liability in respect to any express warranties or responsibility for faulty materials or workmanship. The Contractor shall remedy any defects in the work and pay for any damage to other work resulting therefrom, which shall appear within a period of one (1) year from the date of final acceptance of the work unless a longer period is specified. The United City of Yorkville will give notice of observed defects with reasonable promptness. The Contractor shall guarantee all materials and workmanship as defined by the Performance Bond, and Maintenance Bond requirements.

Unless otherwise amended in writing by the Purchasing Manager, the date of the Start of any Guarantees, Warranties and Maintenance Bonds shall be coincident with the Date of Acceptance of the entire project.

5. **Maintenance Bond** – The Maintenance Bond shall be in a penal sum of twenty percent (20%) of the final Contract amount for a period of one (1) year after the date of acceptance of the entire project. The Contractor may elect to reduce the Performance Bond, and the Labor and Material Bond to twenty percent (20%) of the final Contract amount in lieu of a separate Maintenance Bond. Such bond shall guarantee the work against defective workmanship and materials for a period of not less than one (1) year following acceptance of work.
6. **Termination of Contract** - The United City of Yorkville reserves the right to terminate the whole or any part of this Contract, upon written notice to the Contractor, in the event that sufficient funds to complete the Contract are not appropriated by the Board of Trustees of the United City of Yorkville.

The United City of Yorkville further reserves the right to terminate the whole or any part of this Contract, upon written notice to the Contractor, in the event of default by the Contractor. Default is defined as failure of the Contractor to perform any of the provisions of this Contract, or failure to make sufficient progress so as to endanger performance of this Contract in accordance with its terms. In the event of default and termination, the United City of Yorkville may procure, upon such terms and in such manner as the United City of Yorkville may deem appropriate, supplies or services similar to those so terminated.

The Contractor shall be liable for any excess costs for such similar supplies or service unless acceptable evidence is submitted to the United City of Yorkville that failure to perform the Contract was due to causes beyond the control and without the fault or negligence of the Contractor.

7. **Notification of Work** - The Contractor shall notify the City's Director of Public Works 48 hours prior to commencement of work and 24 hours prior to each inspection.
8. **Cleaning** - During construction, the Contractor and his subcontractors shall remove from the premises, rubbish, waste material, and accumulations, and shall keep the premises clean. The Contractor shall keep the premises clean during construction to the satisfaction of the Director of Public Works.
9. **Restoration** - the calculation of quantities and limits of Restoration which will be allowed for payment will be in accordance with the "Standard Specifications." Restoration, fill sand, stone, etc., required outside of the limits as indicated in the Contract Documents will be considered incidental to the Contract. The Contractor shall at all times maintain all equipment and materials within the limits of restoration as specified.
10. **Removal and Replacement** - all removal and replacement items will be marked and measured for payment prior to removal by the Contractor. Any removal beyond these lines will be replaced to the satisfaction of the Director Public Works with no additional compensation.
11. **Business/Resident Notification** - The Contractor shall not close any street or private driveway without the consent of the Director of Public Works, and the proper notification of the affected business/resident.
12. **Use of Fire Hydrants** - no fire hydrants shall be used to obtain water for non-emergency use. Water can be purchased and obtained at the City's Director of Public Works Building.

INSURANCE REQUIREMENTS

The Contractor shall provide satisfactory proof of commercial general liability, property damage, bodily injury, automobile liability, and workers compensation insurance, naming the United City of Yorkville, together with its officers, agents, employees and engineers as additional primary, non-contributory named insureds prior to commencing work. All policies shall be written on a "per occurrence" basis.

The Contractor shall procure and maintain insurance for protection from claims under worker's compensation acts, claims for damages because of bodily injury including personal injury, sickness or disease or death of any and all employees or of any person other than such employees, and from claims or damages because of injury to or destruction of property including loss of use resulting therefrom, alleged to arise from the Contractor's or its officers', agent's, employees' negligence in the performance of services under this Agreement.

Contractors certificate of insurance shall contain a provision that the coverage afforded under the policy(s) will not be canceled or reduced without thirty (30) days prior written notice (hand delivered or registered mail) to the City. Contractor shall promptly forward new certificate(s) of insurance evidencing the coverage(s) required herein upon annual renewal of the subject policies.

Failure of the Contractor to supply a valid certificate of insurance, or if a previously valid certificate of insurance has expired and is not replaced, is grounds for issuance of a stop work order until such time as a valid certificate of insurance is provided. Failure of the City to collect or demand a certificate of insurance shall not be deemed a waiver of the requirement to provide one.

The limits of liability for the insurance required by this Subsection shall not be less than the following:

Workmen's Compensation Insurance:

All Liability imposed by Workmen's Compensation statute

Employer's Liability Insurance	\$1,000,000
Contractual Liability Insurance	\$1,000,000
Completed Operations Insurance	\$ 500,000

Owned, Hired, and Non-Ownership Vehicle, Bodily Injury and Property Damage to the following Limits:

Commercial General Liability	\$2,000,000 (each occurrence)
Bodily Injury	\$2,000,000 (each person)
	\$2,000,000 (each accident)
Property Damage	\$2,000,000 (each accident)
Automobile Liability	\$1,000,000 combined single limit (each accident)
Umbrella Liability	\$3,000,000 (each occurrence)
	\$3,000,000 (aggregate)

BID PROPOSAL

Note: The Contractor must complete all portions of this Bid Sheet

The undersigned, having examined the specifications, and all conditions affecting the specified project, offer to furnish all services, labor, and incidentals specified for the price below.

The undersigned Bidder certifies that they are not barred from bidding on this contract as a result of a conviction for the violation of state laws prohibiting bid rigging or bid rotating (720 ILCS 5/33E-1, *et seq.*) and is not delinquent in any taxes to the Illinois Department of Revenue (65 ILCS 5/11-42.2-1).

It is understood that the City reserves the right to reject any and all bids and to waive any irregularities and that the prices contained herein will remain valid for a period of not less than forty-five (45) days.

I (We) propose to complete the following project as more fully described in the specifications for the following:

Bidding Company Name: _____

_____ Our firm has not altered any of the written texts within this document. Only those areas requiring input by the respondent have been changed or completed.

If it is the Contractor's intention to utilize a subcontractor(s) to fulfill the requirements of the Contract, the City must be advised of the subcontractor's company name, address, telephone and fax numbers, and a contact person's name at the time of bid submittal.

YES

NO

Will you be utilizing a subcontractor?

If yes, have you included all required Information with your bid submittal?

OR

NO BID – Keep our company on your Contractors List

Signature

Date

NO BID – Remove our company from Your Contractors List

Signature

Date

BID PROPOSAL CONTINUED

Plant List

Description	Size (height)	Quantity	Unit Cost	Total Cost
Acer Platanoides "Royal Red" Norway Maple	6 foot	5	\$	\$
Juniperus Chinensis "Sea Green" Sea Green Juniper	24-48 inches	15	\$	\$
Labor Cost				\$
Total Plant List Cost				\$

Hardscape

Description	Size	Quantity	Unit Cost	Total Cost
Limestone Veneer Retaining Wall 3.5' exposed	35 foot	1	\$	\$
Individual Channel Steel Letters "YORKVILLE"	24 inches	9	\$	\$
Limestone Boulder Outcroppings	12-18 inches	15	\$	\$
Solar Lighting (up lighting or back channel letter lighting)	TBD	TBD	\$	\$
Labor Cost				\$
Total Hardscape Cost				\$

Total Project Cost	\$
---------------------------	-----------

Contractor's Firm Name Date

Signed Name

Print Name Title

BID PROPOSAL CONTINUED

The undersigned contractor;

A. Certifies that it is not barred from bidding or contracting with the City as a result of a violation of either Paragraph 33E-3 (Bid rigging) or 33E-4 (Bid rotating) of Act 5, Chapter 720 of the Illinois Compiled Statutes regarding criminal interference with public contracting, and

B. Swears under oath that it is not delinquent in the payment of any tax administered by the Illinois Department of Revenue as required by Chapter 65, Act 5, paragraph 11-42.1 of the Illinois Compiled Statutes, and

C. States that it has a written sexual harassment policy as required by the Illinois Human Rights Act (775 ILCS 5/2-105(A) (4) a copy of which shall be provided to the City upon request, and

D. Agrees to comply with the requirements of the Illinois Human Rights Act regarding Equal Employment Opportunities as required by Section 2-105 of the Illinois Human Rights Act (775 ILCS 5/2-105) and agrees to comply with the EQUAL EMPLOYMENT OPPORTUNITY CLAUSE in Appendix A, Section 750, Part 750, Chapter X, Subtitle B of Title 44 of the Illinois Administrative Code incorporated herein by reference, and

D. Agrees to comply with the civil rights standards set forth in Title VII of the Civil Rights Act as mandated in Executive Order No. 11246, U.S.C.A. Section 2000e n.114 (September 24, 1965), and

E. Agrees to comply with the Substance Abuse Prevention on Public Works Projects Act (820 ILCS 265/1 et seq.) if this Project is a “public work” within the meaning of the Illinois Prevailing Wage Act (820 ILCS 130/.01 et seq.) and prohibit substance abuse while performing such work and has a substance abuse prevention program, and

F. Agrees to provide a drug free workplace pursuant to the Drug Free Workplace Act (30 ILCS 580/1 et seq.) (25 or more employees under a contract of more than \$5,000 or for individuals only when greater than \$5,000), and

G. Agrees to comply with the Employment of Illinois Workers on Public Works Act (30 ILCS 570/0.01 et seq.) and employ Illinois laborers if at the time of this contract is executed or if during the term of this contract there is excessive unemployment in Illinois as defined in the Act.

Contractor’s Firm Name Date

Signed Name

Print Name Title

Street Address City State Zip

Phone Number Fax Number Email Address

DETAIL EXCEPTION SHEET

Any exception must be clearly noted on this sheet. Failure to do so may be reason for rejection of the bid. It is not our intention to prohibit any potential contractor from bidding by virtue of the specifications, but to describe the material(s) and service(s) actually required. The City reserves the right to accept or reject any or all exceptions.

Contractor's exceptions are:

SUBCONTRACTOR LISTING

Provide the name, contact information, and value of work for each and every subcontractor which will be employed on this project.

Subcontractor No. 1

Business Name

Address

City, State, Zip Code

Telephone Number

Value of Work Subcontracted

Nature of Work Subcontracted

Subcontractor No. 2

Business Name

Address

City, State, Zip Code

Telephone Number

Value of Work Subcontracted

Nature of Work Subcontracted

Subcontractor No. 3

Business Name

Address City, State, Zip Code

Telephone Number Value of Work Subcontracted

Nature of Work Subcontracted

If additional sheets are needed, please make copies.

REFERENCES

General Information, the list below current business references for whom you have performed work similar to that required by this proposal.

Reference No. 1

Business Name

Address

City, State, Zip Code

Contact Person

Telephone Number

Dates of Service

Reference No. 2

Business Name

Address

City, State, Zip Code

Contact Person

Telephone Number

Dates of Service

Reference No. 3

Business Name

Address City, State, Zip Code

Contact Person Telephone Number

Dates of Service

If additional sheets are needed, please make copies.

**UNITED CITY OF YORKVILLE
800 GAME FARM ROAD
YORKVILLE, ILLINOIS 60560**

CONTRACT

THIS CONTRACT made this ____ day of _____, 2019, by and between the United City of Yorkville, an Illinois municipal corporation hereinafter called the “*Owner*” and _____ located at _____ hereinafter called the “*Contractor*”.

WITNESSETH:

WHEREAS, the Owner has heretofore solicited Bid Proposals for all labor and materials necessary to complete the work specified in the Invitation To Bid – Design and Installation of the 2019 Downtown Hill Landscape Project;

WHEREAS, the Owner has found that the Contractor is the lowest responsible bidder for said work and has awarded the Contractor this contract for said work.

NOW, THEREFORE, for and in consideration of their mutual promises and agreements, the parties hereto do hereby agree as follows:

1. The Contractor agrees to furnish all materials, supplies, tools, equipment, labor and other services necessary to commence and complete the work proposed in accordance with the conditions and prices stated in the Invitation to Bid, Instructions to Contractors – General Conditions, Special Conditions, Insurance Requirements, Specifications and Plans, Bid Proposal, and Detail Exception Sheet all of which are made a part hereof and herein called the “*Contract Documents*”.
2. The Owner will pay the Contractor in the amounts, manner and at times as set forth in the Contract Documents.

IN WITNESS WHEREOF, the parties hereto have executed, or caused to be executed by their duly authorized officials, this Contract as of the day and year first above written:

OWNER: UNITED CITY OF YORKVILLE

By: _____
Mayor

Attest:

CONTRACTOR:

City Clerk

By: _____
Signature

Attest:

Print Name and Title

Witness



Reviewed By:	
Legal	<input type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #9

Tracking Number

EDC 2019-67

Agenda Item Summary Memo

Title: Kane/Kendall Council of Mayors Bike Sharing Services – Memorandum of Understanding

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: Please see attached.

Council Action Previously Taken:

Date of Action: _____ Action Taken: _____

Item Number: _____

Type of Vote Required: Majority

Council Action Requested: Approval

Submitted by: Erin Willrett Administration
Name Department

Agenda Item Notes:



Memorandum

To: Economic Development Committee
From: Erin Willrett, Assistant City Administrator
CC: Bart Olson, City Administrator
Date: August 6, 2019
Subject: Bike Sharing Memorandum of Understanding

Summary

Partner with Kane County DOT as part of Kane/Kendall Council of Mayors to research the viability and cost of a bike-share system for the City of Yorkville.

Background

The Kane County Division of Transportation (KDOT) has begun to work with its municipalities, non-profits, park districts, forest preserves, and private companies to develop a countywide bike sharing system. The anticipated launch of the system is August 2020. The next step in the process is to collect signed Memorandum of Understanding's (MOU) from interested parties that wish to participate in the program. These MOU's are not legally or financially binding whatsoever, but simply state that the City is potentially interested in participating. The main purpose in signing these MOU's is that only participants that sign the MOU before the deadline will be able to view and provide comments on the Request for Proposal (RFP) for a bike sharing company that KDOT will be releasing in September 2019. Signing of the MOU does not require the City to take part in the program.

A bicycle-sharing system, public bicycle system, or bike-share scheme, is a service in which bicycles are made available for shared use to individuals on a short term basis for a price or free. Many bike share systems allow people to borrow a bike from a "dock" and return it at another dock belonging to the same system. Docks are special bike racks that lock the bike, and only release it by computer control. The user enters payment information, and the computer unlocks a bike. The user returns the bike by placing it in the dock, which locks it in place. Other systems are dockless. For many systems, smartphone mapping apps show nearby available bikes and open docks.

The benefits of bike sharing schemes include transport flexibility, reductions to vehicle emissions, health benefits, reduced congestion and fuel consumption, and financial savings for individuals. Research has shown that bike sharing has a positive impact on commercial activity, as well. Users reported that they engaged in new spending at these neighborhood businesses because of the access provided by bike sharing. In short, for both businesses and consumers, bike share systems have a positive effect on business activity due to increased accessibility within local neighborhoods.

With help and investment from local municipalities, non-profits, organizations, and recreational districts, Kane County is hoping to create a system with at least 12 stations for the initial planned launch in August 2020. These stations will be placed at recreational, residential, commercial destinations, as well as strategic public transportation stops. However, each participant has the flexibility to place the station wherever they see fit within their community and/or property. The County will provide support to all participants to ensure that stations are not being placed in close proximity to another station, which would decrease ridership for both stations.

Each station owner is responsible for the capital and annual financial obligations of owning and operating a bike share station. Each station will cost an estimated \$10,000 per year to own and operate, with an estimated \$3,000 installation fee. However, these costs will fluctuate depending on the number of stations included within the system. A higher number of stations will decrease the price for all participants.

One of the main sources of revenue for a bike share station owner is selling advertising/sponsorship space on the bikes and station. Station owners have full autonomy on the

sponsors that they procure for their stations. Owners also have the right to set sponsorship prices independent of other stations.

Recommendation

Staff is seeking approval for the Memorandum of Understanding Regarding Bike Sharing Services. This would allow staff to provide comments on the future Request for Proposal for a bike sharing company. The RFP would be released in September 2019. This does not require the City to take part in the bike sharing program.

Kane County Bike Share System Development

Program Overview

The Kane County Division of Transportation (KDOT) is very excited to announce its intention to work with its municipalities, park districts, forest preserve(s), non-profits, and private institutions to create a countywide bike sharing system!

A bicycle-sharing system, public bicycle system, or bike-share scheme, is a service in which bicycles are made available for shared use to individuals on a short term basis for a price or free. Many bike share systems allow people to borrow a bike from a "dock" and return it at another dock belonging to the same system. Docks are special bike racks that lock the bike, and only release it by computer control. The user enters payment information, and the computer unlocks a bike. The user returns the bike by placing it in the dock, which locks it in place. Other systems are dockless. For many systems, smartphone mapping apps show nearby available bikes and open docks.

The benefits of bike sharing schemes include transport flexibility, reductions to vehicle emissions, health benefits, reduced congestion and fuel consumption, and financial savings for individuals. Research has shown that bike sharing has a positive impact on commercial activity, as well. Users reported that they engaged in new spending at these neighborhood businesses because of the access provided by bike sharing. In short, for both businesses and consumers, bike share systems have a positive effect on business activity due to increased accessibility within local neighborhoods.

Bike sharing will be an affordable, healthy, safe, environmentally sustainable, and popular addition to Kane County's transportation options. Implementing a top-quality system will boost the County's transportation network and further advance Kane County's position as a high quality destination and place to live. Bike share will help connect residents, commuters and visitors to more of Kane County's businesses, institutions and attractions. Future requests from neighboring counties or municipalities for participation in the Kane County System offer the possibility for expansion beyond the initial service area.

If you have any questions regarding the program please contact Ryan Peterson at 630-444-3143 or peteronryan@co.kane.il.us.

System Implementation

With help and investment from local municipalities, non-profits, organizations, and recreational districts, Kane County is hoping to create a system with at least 12 stations for the initial planned launch in August 2020. We hope that these stations will be placed at recreational, residential, commercial destinations, as well as strategic public transportation stops throughout the County. However, each participant has the flexibility to place the station wherever they see fit within their community and/or property. The County will provide support to all participants to ensure that stations are not being placed in close proximity to another station, which would decrease ridership for both stations.

Each station owner is responsible for the capital and annual financial obligations of owning and operating a bike share station. Each station will cost an *estimated* \$10,000 per year to own and operate, with an estimated \$3,000 installation fee. However, these costs will fluctuate depending on the number

of stations included within the system. A higher number of stations will decrease the price for all participants. The County will also help to subsidize the cost of these stations.

There are three main types of bike share systems: docked, dockless, and hybrid. Each system type has their own benefits and drawbacks, making them ideal for different settings and locales. The Kane County system will focus on only docked stations at the initial launch in 2020. This type was chosen because docked systems have the least administrative strain; lowest potential for vandalism and theft; highest potential for advertising; and are easily implemented into suburban locations. Other systems can be incorporated into the County system at a later date, however. Pictured below is a typical configuration of a docked bike share station and the features that come standard on most bike share bicycles.



Timeline

In order to create the system by August 2020, there needs to be lots of coordination and communication between the participating station owners, sponsors, and the bike share organizers. The draft timeline listed below outlines the dates for a successful, timely launch. *These dates are subject to change.*

2019	
May	KKCOM Bike & Pedestrian Committee meeting; presentations by bike sharing companies
May 31 & June 6	Conference call(s) involving interested stakeholders (municipalities, forest preserves, park districts, etc.)
August 23	Deadline to sign non-binding MOU to participate in opening round of funding
September	Release RFP to select bike share provider
October	Select preferred provider; start drafting network design
November	Start holding public input meetings to educate public on bike sharing and determine ideal station locations
2020	
January	Finalize financial obligations for participants
February	Finalize network locations
March	Secure advertising, sponsors for system
June	Start marketing and outreach efforts
July	Financial obligations due to bike share provider
August	Construction and implementation of stations

Sponsorship Opportunities

One of the main sources of revenue for a bike share station owner is selling advertising/sponsorship space on the bikes and station. Station owners have full autonomy on the sponsors that they procure for their stations. Owners also have the right to set sponsorship prices independent of other stations. Pictured below are diagrams that display some of the space available to station owners for sponsorships.



Throughout the county, there have been a variety of different sponsorship formats that bike share systems and the station owners have created in order to generate revenue. The five main types of sponsorship opportunities that are recommended are listed below:

1. Title Sponsor
 - System naming rights
 - Brand inclusion on bikes/stations
 - Brand inclusion on system website, social media, and location-based promotions
 - In-app advertising
2. Presenting Sponsor
 - If no title sponsor is found, 3-4 presenting sponsors will take place
 - Many of the same advertising components but without system naming rights
3. Station Sponsor
 - Brand inclusion on station kiosk
4. Station Host
 - Donate property to host station at residential, recreational or commercial location
5. Digital Advertising
 - In-app advertising
 - Social media

Applicable Case Studies

- St. Lucie County, Florida
 - Population: 313,506
 - Density: 486 residents per sq. mile
 - Created a nine station, 50 bike system aimed at increasing connectivity throughout the county
 - Spaced stations one to four miles apart to cover a larger area but not restrict connectivity and access between stations
 - Used Zagster as their bike share provider
 - County retains 93% of the operation's bike share rental fees
- Howard County, Maryland
 - Population: 287,085
 - Density: 1,279 residents per sq. mile
 - Nine stations and 78 bike system throughout two of its largest cities
 - Had over 4,800 rides in its first year of operation
 - Used Bewegen Technologies as their contractor
- Anne Arundel County, Maryland
 - Population: 573,235
 - Density: 1,381 residents per sq. mile
 - Dockless bike share program focusing primarily around attractions and locations in Annapolis
 - Attracted 700+ members in the first three months
 - Used Pace as their bike share provider

Memorandum of Understanding Regarding Bike Sharing Services

This Memorandum of Understanding (“MOU”) is made by and between the Kane County Division of Transportation (the “County”) and _____ (the “Local Public Agency”).

TERMS OF UNDERSTANDING

1. Definitions.
 - a. “Bicycles” means all standard pedal bicycles and/or other mobility vehicles that might be included within the program.
 - b. “Kane County Bike Share Program” means the future bike share program being organized by the Kane County Division of Transportation.

PROGRAM OUTLINE

The Local Public Agency has expressed interest in participating in the Kane County Bike Share Program. This memorandum of understanding is meant to serve as an outline of key elements and dates of the Kane County Bike Share Program as it would relate to the Local Public Agency’s participation therein.

1. *Purpose of the Program.* Bike sharing will be an affordable, healthy, safe, environmentally sustainable, and popular addition to Kane County’s transportation options. Implementing a top-quality system will boost the County’s transportation network and further advance Kane County’s position as a high quality destination and place to live. Bike share will help connect residents, commuters and visitors to more of Kane County’s businesses, institutions and attractions. Requests from neighboring counties and municipalities therein for participation in the Kane County System offer the possibility for expansion beyond the initial service area.
2. *Goals and Objectives.* The following goals and objectives have been identified for the initial launch of the system:
 - Create a quality user experience with a convenient and easy-to-use system,
 - Enhance connections to the public transit network and park-and-ride lots,
 - Increase physical activity to benefit public health,
 - Promote travel to landmarks, parks, trails and shopping districts (among residents and visitors),
 - Increase access to job opportunities and education,
 - Expand mobility options for low-income residents,
 - Improve connectivity between existing hubs of activity, and
 - Leverage bike share to help shift local culture towards multi-modal transportation
3. *The Local Public Agency’s Responsibilities.* Following the Request for Proposal process, the Local Public Agency will be responsible for all financial obligations for the stations that they purchase from the chosen contractor. The County will not own or operate any stations. The Local Public Agency will be responsible for negotiating the revenue sharing

breakdown, station location placement, and other financial matters with the chosen contractor.

OBLIGATIONS

This MOU does not create any financial or legal obligations, nor does it signal any commitment of funds from either Party.

In order to move forward, The Parties will sign this Memorandum of Understanding that will be executed no later than Friday, August 23, 2019.

This MOU shall automatically terminate if the Memorandum of Understanding is not entered into, for any reason, on or before August 23, 2019.

In order to be eligible to offer comments on the forthcoming Request for Proposals that will be used to procure a bike sharing contractor, this MOU must be signed before August 23, 2019. Those who do not wish to sign the MOU before the aforementioned deadline will not be eligible to offer comments to the RFP.

We look forward to working with the municipalities, agencies, and organizations of Kane County to improve mobility for its residents and visitors. It is the hope of Kane County Division of Transportation that this program will help create more liveable cities, enhanced recreational offerings, additional transportation options and a reduced carbon footprint.

[Remainder of page intentionally blank. Signatures follow on subsequent page.]

The Local Public Agency

Kane County

By: _____
Name: _____
Title: _____
Date: _____

By: _____
Name: _____
Title: _____
Date: _____
Attest: _____
Clerk of the Board

Approved as to form
and legal sufficiency

Assistant County Attorney



Reviewed By:	
Legal	<input checked="" type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input checked="" type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #10

Tracking Number

EDC 2019-68

Agenda Item Summary Memo

Title: Unified Development Ordinance – Change Order Request

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: Approval of a fee change order regarding the Unified Development Ordinance.

Council Action Previously Taken:

Date of Action: 2/26/2019 Action Taken: Approval of RFP Award

Item Number: EDC 2019-16

Type of Vote Required: Majority

Council Action Requested: Approval

Submitted by: Krysti J. Barksdale-Noble

Community Development

Name

Department

Agenda Item Notes:

Have a question or comment about this agenda item?

Call us Monday-Friday, 8:00am to 4:30pm at 630-553-4350, email us at agendas@yorkville.il.us, post at www.facebook.com/CityofYorkville, tweet us at @CityofYorkville, and/or contact any of your elected officials at http://www.yorkville.il.us/gov_officials.php



Memorandum

To: Bart Olson, City Administrator
 From: Krysti Barksdale-Noble, Community Dev. Dir.
 CC: Jason Engberg, Senior Planner
 Date: April 22, 2019
 Subject: **Unified Development Ordinance (UDO)**
 Change Order request regarding fees for enCode Plus

Summary:

The purpose of this memo is to request a change order to the approved Unified Development Ordinance (UDO) project for a budget increase of \$7,125. The overall total project cost would increase from \$148,360 to \$155,485.

By definition, a Change Order, is a written order to the Contractor authorizing an addition, deletion or revision in the work within the general scope of the contract documents or authorizing an increase or decrease in the contract price or contract time. In paragraph E of the attached executed contract with Houseal Lavigne, the contract consultant for the UDO project, any such changes to the contract, including increases or decreases in amount of compensation, which are mutually agreed upon, requires a written amendment to the agreement.

Background:

In December 2018, the City solicited request for proposals (RFP) from qualified consulting firms for a Unified Development Ordinance (UDO) which would to modernize and consolidate the various existing City ordinances into a cohesive, single-reference, graphic-based document. As part of the scope of services in the UDO RFP, the City specifically requested “an online digital smart code with interactive site specific zoning analysis capable of rendering calculations for zoning criteria such as minimum parking requirements for proposed land use, maximum lot coverage, required setbacks and transition yards, and volumetric limits (FAR, height, etc.) based upon ESRI ARC GIS base layer data provided by the City.

After submittals from two (2) firms in response to the RFP, the City Council approved a contract with Houseal Lavigne in February 2019 for the commencement of a Unified Development Ordinance (UDO). The agreement for the contract was valued at **\$148,360** which included a suite of features from enCode Plus, a web-based document presentation and management system developed by planners and code writers, as part of the Advanced 180 package (\$5,250 billed annually) with an embedded calculator for a one-time fee of \$2,500 (*see below*). It was staff’s understanding at the time of the contract and interview with Houseal Lavigne that the total cost included all zoning calculators enumerated in the scope of services requested as part of the deliverables in the RFP. No other information detailing enCode Plus fees were provided in the proposal.

Yearly enCodePlus Fee/Optional Features (after adoption)

enCodePlus Yearly Maintenance Fees		Total
Standard 90		\$4,250
Advanced 180		\$5,250
Premium 360		\$7,250

enCodePlus Optional Features		Total
Embedded Calculator (no annual fee)		\$2,500
GIS Interactive Map (Advanced) Build and Launch		\$4,500
	Plus Annual Fee	\$3,500
GIS Interactive Map (Premium) Build and Launch		\$6,500
	Plus Annual Fee	\$4,550

enCode Plus Fees:

As the subconsultant, enCode Plus, who was not present at the interview with Houseal Lavigne, would be designing the platform for the dynamic online smart-code.

During the kick-off meeting with Houseal Lavigne, after the award of the contract, staff was provided the attached service agreement from enCode Plus. It was then relayed to staff by enCode Plus, that although the Advanced 180 package comes with a suite of up to seven (7) features/calculators, there would be a one-time set-up fee of \$2,500 per calculator or \$17,500. The calculators offered are as follows:

1. Parking Calculator¹ – calculates the minimum required number of off-street parking spaces based on use by zoning district.
2. Shared Use Parking Calculator - calculates the minimum required number of off-street parking spaces when two or more land uses share the same parking area.
3. Landscape Calculator² - calculates the minimum required landscaping material by zoning district, based upon lot size and adjacent land uses.
4. Bufferyard Calculator³ - calculates the minimum required landscaped bufferyard area and vegetation type by zoning district, based upon lot size and adjacent land uses.
5. Signage Calculator⁴- calculates the minimum required number, area, setback, distance separation and maximum height for signage based on zoning district and sign type.
6. Project Fee Calculator – calculates the fees for land use applications based upon request type (annexation, rezoning, special use, etc.) and land area.
7. Yield Calculator⁵ – calculates the maximum total buildable dwelling units and/or maximum total buildable non-residential square feet by zoning district.

Additionally, the built-in calculators will also have the ability for the user to export the data or to save the data entered into the calculator. Real world examples of communities currently utilizing the enCode Plus calculators as part of their online codes are provided as links in the footnotes at the bottom on this page.

Request:

To acquire all the above-mentioned calculator features, which are available in the Advance 180 package, the City will need to authorize a change order since the one-time set up fee for the calculators requested was not clearly articulated to us by Houseal Lavigne during the RFP review process.

As originally proposed, Houseal Lavigne listed the pricing for only one (1) calculator at a cost of \$2,500.00. However, in order to purchase the additional six (6) calculators offered in the Advance 180 package, there is a one-time set-up fee of \$2,500 per calculator or \$15,000 of which \$2,625 (15%) is discounted for a total of \$12,375.

Since discovering the miscommunication about the start-up fee for the calculators, Bret Keast of EnCode Plus has subsequently agreed to remove the initial licensing fee of \$1,875 included in Houseal Lavigne's proposal. Additionally, Houseal Lavigne has agreed to amend their hours for the UDO preparation by consolidating two (2) meetings (Step #6b and #6c), thereby lessening their billable hours by \$3,375.

¹ <http://online.encodeplus.com/regs/greensboro-nc/calculators.aspx>

<http://online.encodeplus.com/regs/johnston-ia/calculators.aspx>

² <http://online.encodeplus.com/regs/greensboro-nc/calculators.aspx>

<http://online.encodeplus.com/regs/johnston-ia/calculators.aspx>

³ <http://online.encodeplus.com/regs/stmary-la/webtools/landscape/bufferrequirements.aspx>

⁴ <http://online.encodeplus.com/regs/greensboro-nc/calculators.aspx>

<http://online.encodeplus.com/regs/johnston-ia/calculators.aspx>

⁵ <http://online.encodeplus.com/regs/valparaiso-in/doc-viewer.aspx#secid-490>

Therefore, the requested change order amount would be for \$7,125. This represents the difference between the total HL & enCode plus fee (\$155,485) and the approved project budget (\$148,360).

The following chart has been prepared to clearly explain and layout the fee difference proposed as part of this change order request.

Approved UDO Project Budget Houseal Lavigne (HL) RFP		Proposed enCode Plus Additional Calculator Fees & Discounts	
Houseal Lavigne RFP	\$138,735	One-Time Build Fee for additional six (6) calculators	\$15,000
enCode Plus Initial Licensing Fee	\$1,875	15% Discount Fee	-\$2,625
enCode Plus Advance 180 (1 st year)	\$5,250	Discount of enCode Plus Initial Licensing Fee*	-\$1,875
enCode Plus one (1) embedded calculator	\$2,500	Discount of Houseal Lavigne Billable Hours	-\$3,375
TOTAL	\$148,360.00	TOTAL	\$7,125.00
TOTAL proposed new UDO Project Budget	\$155,485		

Staff Comments:

Staff supports the change order request for a budget increase of \$7,125 and strongly believes, to realize the full potential of the interactive, site specific zoning analysis capabilities offered by enCode Plus in the smart-code, purchasing the suite of embedded calculators is required. The added calculator features will provide clarity to the code’s regulations for any end user (resident and developer) regarding bulk standards (setbacks, buffers, maximum height, etc.), as well as reduce staff review time of submitted permits and proposals.

Although staff had requested the capabilities of all these calculators as part of our initial RFP request, the quote provided of \$2,500 was not clearly stated in the HL proposal, nor fully understood by the consultant themselves, that this price was for a single calculator. Based upon that, enCode Plus has discounted the initial licensing fee of \$1,875.00 from the original quote and the consultant has adjusted their billable hours to reduce the contracted fee by \$3,375.

Attached is the revised License Agreement from enCode Plus for the purchase of additional embedded calculators within the “smart code” and the amended scope of work with Houseal Lavigne indicating the meeting schedule adjustment. The City Attorney has also reviewed both of the revised agreements. Staff will be available to Tuesday night’s meeting to discuss this matter in greater detail.

**A RESOLUTION OF THE UNITED CITY OF YORKVILLE, ILLINOIS
APPROVING A PROFESSIONAL SERVICES AGREEMENT
WITH HOUSEAL LAVIGNE ASSOCIATES, LLC.**

WHEREAS, the United City of Yorkville (the "City") is a duly organized and validly existing non home-rule municipality created in accordance with the Constitution of the State of Illinois of 1970 and the laws of the State; and,

WHEREAS, the City sought proposals for technical and professional services for the creation of a unified development ordinance for the City; and,

WHEREAS, after a thorough review of the proposals received, the City Council has determined that the proposal presented by Houseal Lavigne Associates, LLC. is in the best interest of the City and hereby approves an agreement for such services.

NOW, THEREFORE, BE IT RESOLVED by the Mayor and City Council of the United City of Yorkville, Kendall County, Illinois, as follows:

Section 1: That the *AGREEMENT FOR PROFESSIONAL SERVICES BETWEEN THE UNITED CITY OF YORKVILLE, ILLINOIS AND HOUSEAL LAVIGNE ASSOCIATES, LLC.* between the City and Houseal Lavigne Associates, LLC. attached hereto as Exhibit A and made a part hereof by reference, is hereby approved, and the City Administrator be and is hereby authorized to execute said Agreement on behalf of the United City of Yorkville.

Section 2: This Resolution shall be in full force and effect upon its passage and approval as provided by law.

Passed by the City Council of the United City of Yorkville, Kendall County, Illinois this 26 day of February, 2019.

Beth Warner
CITY CLERK

CARLO COLOSIMO [Signature]
JACKIE MILSCHEWSKI [Signature]
CHRIS FUNKHOUSER [Signature]
SEAVER TARULIS [Signature]

KEN KOCH [Signature]
ARDEN JOE PLOCHER [Signature]
JOEL FRIEDERS [Signature]
JASON PETERSON [Signature]

Approved by me, as Mayor of the United City of Yorkville, Kendall County, Illinois, this
6 day of MARCH, 2019.


MAYOR

**PROFESSIONAL SERVICES
AGREEMENT**

MARCH 8, 2019

BETWEEN

THE UNITED CITY OF YORKVILLE, ILLINOIS

AND

HOUSEAL LAVIGNE ASSOCIATES, LLC.

AGREEMENT FOR PROFESSIONAL SERVICES
BETWEEN THE UNITED CITY OF YORKVILLE, ILLINOIS
AND

HOUSEAL LAVIGNE ASSOCIATES, LLC.

THIS AGREEMENT, made and entered into this 8 day of MARCH, 2019, by and between HOUSEAL LAVIGNE ASSOCIATES LLC., an Illinois Limited Liability Company with principal offices at 188 W. Randolph, Suite 200, Chicago, IL 60601 (hereinafter referred to as the "CONSULTANT"), and the UNITED CITY OF YORKVILLE, ILLINOIS, a municipal corporation of the State of ILLINOIS, whose mailing address is 800 Game Farm Road, Yorkville, Illinois 60560 (hereinafter referred to as the "CLIENT").

WITNESSETH THAT:

WHEREAS, the CLIENT desires to engage the services of the CONSULTANT to furnish technical and professional assistance in connection with the preparation of the *Yorkville Unified Development Ordinance* (hereinafter referred to as the "PROJECT") and the CONSULTANT has signified its willingness to furnish technical and professional service to the CLIENT; and WHEREAS, the CONSULTANT represents to the CLIENT that it has sufficient expertise and resources to enable it to provide such advice and assistance to the CLIENT; and

NOW, THEREFORE, the parties do mutually agree as follows:

A. Scope of Consultant's Services

The CONSULTANT agrees to perform in a good and professional manner those services described in Attachment A, *Scope of Services*, a copy of which is attached hereto and incorporated in this AGREEMENT. All documents, work papers, maps, and study materials produced by the CONSULTANT in the performance of these services become the property of the CLIENT during and upon completion of the services to be performed under this AGREEMENT.

B. Services to be Provided by the Client

All existing information, data, reports and records which are useful for carrying out the work on this PROJECT and which are owned or controlled by the CLIENT shall be furnished to the CONSULTANT in a timely manner. The completion of the services to be performed by the CONSULTANT under this AGREEMENT is contingent upon the receipt from the CLIENT, at no cost to the CONSULTANT, the data and reports and other material as described in Attachment A, Section 1, in a timely manner. If, by reason of any fault of CLIENT, the information, data, reports and records to be provided by the CLIENT are not made available to the CONSULTANT in a timely manner, the CONSULTANT may, at its option, stop work on the PROJECT until such materials are provided.

C. Meetings and Consultant Visits

The CONSULTANT will attend meetings associated with the following tasks, and any additional meetings deemed reasonable and needed to complete the PROJECT, as outlined in Attachment A, *Scope of Services*.

- 1a: Project Initiation – Staff Meeting & Review of Preliminary Issues
- 1b: Planning and Zoning Commission Workshop
- 1c: Residential Areas Zoning Workshop
- 1d: Commercial and Industrial Areas Zoning Workshop

- 1e: Subdivision Ordinance Focus Group
- 2d: City Staff Working Session
- 2e: PZC Meeting
- 3e: Staff Review Meeting
- 3f: PZC Meeting
- 4b: Staff Review Meeting
- 5c: Staff Review Meeting
- 5d: PZC Meeting
- 6b: Staff Review and Meeting
- 6c: PZC Meeting
- 7b: Staff Review Meeting
- 7c: PZC Meeting
- 7e: Public Hearing
- 7f: City Council Presentation and Adoption

A meeting within the body of this AGREEMENT shall mean a gathering requiring the attendance of the CONSULTANT or CONSULTANT's staff, including workshops, formal presentations and meetings. Public meetings shall be scheduled at least seven (7) to fifteen (15) days in advance, and public hearings shall be scheduled with sufficient advance notice to comply with state and local notice requirements. Attendance at additional meetings will be subject to the provisions of Article M (Extra Work) of this AGREEMENT. The CONSULTANT may conduct "site visits" to gather information, data, and perform field reconnaissance. These "site visits" shall not be counted as meetings under this AGREEMENT.

D. Deliverables

CONSULTANT agrees to provide products to the CLIENT as follows:

- 1: Workshop Summaries
- 1: Project Website and online outreach platform (map.social) (optional)
- 2: Summary and presentation of outreach, research, and assessment
- 3: Draft Residential and Agricultural District Standards
- 3: Draft Commercial and Industrial District Standards
- 3: Draft Special District Standards (OS, PUD, Downtown Overlay)
- 3: Proposed Zoning Districts Map
- 4: Prelim Amendments for Parking, Landscaping, Development Standards, etc
- 5: Draft Modified Subdivision Design/Improvement Regulations
- 6: Draft Administrative, Applications, Approval, and Procedures
- 7: Draft UDO Ordinance

- 7: Revised UDO & Final Legal Review
- 7: City Council Presentation and Adoption
- 8: enCode Plus ready Yorkville UDO Document

The CONSULTANT shall provide all deliverables at least five (5) days in advance of all public meetings. All deliverables become the property of the CLIENT, including all hard copies and electronic file copies.

E. Changes

The CLIENT may, from time to time, request changes in Attachment A, *Scope of Services*, of the services to be performed by the CONSULTANT hereunder. Such changes, including any appropriate increase or decrease in the amount of compensation, which are mutually agreed upon, shall be incorporated in written amendments to this AGREEMENT.

F. Consultant's Compensation

The CONSULTANT shall be compensated for services rendered under the terms of this AGREEMENT on the basis of the CONSULTANT's hourly rates as stated under Article G (Hourly Rates) and Attachment A for the staff time devoted to the PROJECT, and for directly related project expenses. The maximum cost for CONSULTANT services under this AGREEMENT is \$140,610, including directly related job expenses. Directly related job expenses include, but are not limited to: travel, printing, graphic reproduction, mailing, the purchase of additional maps, plans and reports and other out-of-pocket expenses that are related to carrying out services under this AGREEMENT. Any reimbursable expenses that are not enumerated above must be identified by the CONSULTANT and approved by the CLIENT.

The CONSULTANT will not exceed the "not to exceed amount" without specific written authorization from the CLIENT or an amendment to this AGREEMENT. The

CONSULTANT represents and warrants that absent Extra Work, as referenced in Article M, all work to be performed under this AGREEMENT can and will be performed without exceeding the maximum compensation amount and directly related job expense amount, both set forth above.

G. Hourly Rates

The CONSULTANT reserves the right to increase the hourly charge rates, but only after one calendar year from the date of this AGREEMENT, provided however that the maximum CONSULTANT costs as provided in Section F shall not be exceeded. The CLIENT shall be notified at least 30 days in advance and increase must be agreed to in writing by both parties. Hourly rates in effect for purposes of this AGREEMENT are provided in Attachment A.

H. Method of Payment

The CONSULTANT will submit invoices for services performed and directly related job expenses incurred on the PROJECT during the billing period. The CONSULTANT will submit monthly invoices for services performed and directly related job expenses incurred on the PROJECT during the billing period. Invoices are due and payable no later than thirty (30) days from the date of CLIENT's receipt of the invoice and in accordance with the governmental prompt payment act.

I. Time of Performance

The services of the CONSULTANT will begin upon delivery to the CONSULTANT of an executed copy of this AGREEMENT, and shall, absent causes beyond the reasonable control of the CONSULTANT, be completed within twenty-four (24) months of delivery of

said executed AGREEMENT. The completion of services by the CONSULTANT shall be, among other things, contingent upon the timely receipt of the services, data, and other reports described in Attachment A, *Scope of Services* and upon the timely conduct by the CLIENT of meetings and decisions required for its purposes in the execution of Attachment A. For the purpose of this AGREEMENT, timely shall mean that decisions and choices be made within ten (10) working days for CLIENT staff review of CONSULTANT submittals, services, data, and reports as are delivered to the CLIENT's representative; and twenty-one (21) calendar days for such decisions and choices to be made by the City Council, or other elected or appointed bodies of the CLIENT. If the CLIENT requests that CONSULTANT perform Extra Work as defined in Article M such as is not now included in Attachment A, the CONSULTANT, if agreed to by the CLIENT, may suspend work on the PROJECT or a portion of the PROJECT, and may extend the period of time allotted to perform the services identified in Attachment A under this AGREEMENT, to a mutually agreed upon period of time necessary to compensate for Extra Work. Where the CLIENT and CONSULTANT mutually agree to extend the period of time to perform services under this AGREEMENT, the hourly rates may not be increased beyond those set forth in Section G. of this AGREEMENT, provided that the cause or reasons of such extension(s) are not the fault of the CLIENT.

J. Excusable Delays

The CONSULTANT shall not be in breach of this AGREEMENT by reason of any failure in performance of this AGREEMENT in accordance with its terms if such failure arises out of causes beyond the reasonable control and without the fault or negligence of the CONSULTANT. Such causes may include, but are not restricted or limited to, acts of God or of the public enemy, acts of government in either its sovereign or contractual capacity,

fires, floods, strikes, and unusually severe weather, but in every case, so long as the failure to perform is beyond the reasonable control and without the fault or negligence of the CONSULTANT, the CONSULTANT shall not be deemed to be in breach of this AGREEMENT.

K. Termination

The CLIENT shall have the right to terminate this AGREEMENT by written prior notice to the CONSULTANT at least five (5) working days before the specified effective date of such termination. In such event, documents and work papers prepared by the CONSULTANT under this AGREEMENT shall become the property of the CLIENT. On receipt of said documents and work papers by the CLIENT, the CONSULTANT shall receive compensation and reimbursement for the work actually performed before the date of termination, in accordance with Article F, CONSULTANT's Compensation, of this AGREEMENT, less payment for services and expenses previously paid.

L. Non-discrimination

The CONSULTANT has an Affirmative Action program and shall engage in lawful employment practices. The CONSULTANT shall not fail, refuse to hire, discharge, or otherwise discriminate against any individual with respect to his or her compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, sex, national origin, or handicap unrelated to the individual's ability to perform the duties of the position.

M. Extra Work

If requested and agreed to in writing by the CLIENT and CONSULTANT, the CONSULTANT will be available to furnish, or obtain from others, Extra Work of the following types:

1. Extra work or extended services due to changes in the general scope or timing of the PROJECT, including, but not limited to; changes in size, complexity or character of the work items; acceleration of the work schedule involving services beyond normal working hours; non-delivery of any materials, data, or other information to be furnished by the CLIENT not within the reasonable control of the CONSULTANT.
2. Additional or extended services, including PROJECT administration due to the prolongation of the period of delivery of services specified in this AGREEMENT time through no fault of the CONSULTANT.
3. Attendance at additional meetings beyond those made part of the AGREEMENT.
4. Other additional services requested and agreed to by the CLIENT and CONSULTANT, which are not otherwise provided for under this AGREEMENT.

The compensation and schedule for completing Extra Work authorized by the CLIENT shall be subject to negotiation between the CLIENT and the CONSULTANT in accordance with the provision of Article E (Changes) of this AGREEMENT. However, the hourly rate in effect at the time of any change authorizing Extra Work will continue to be in effect for such Extra Work.

N. Entire Agreement

This agreement, including the attachments to this agreement, contains the entire agreement of the parties. It may not be changed orally but only by an amendment in writing executed by the parties to this AGREEMENT.

O. Governing Law

This AGREEMENT will be governed by and construed in accordance with the laws of the State of Illinois and within the jurisdiction of Kendall County.

P. Client Representative to Consultant

The CLIENT designates Krysti J. Barksdale-Noble to act as its representative with respect to the work to be performed under this AGREEMENT, and such person shall have authority to transmit instructions, receive information, interpret and define CLIENT's policies and provide decisions in a timely manner pertinent to the work covered by this AGREEMENT until the CONSULTANT has been advised in writing by the CLIENT that such authority has been revoked. The CONSULTANT designates John Houseal and Nik Davis as the CONSULTANT's representatives to the CLIENT.

Q. Conflict of Interest

The CONSULTANT certifies that to the best of its knowledge no person associated with the CONSULTANT has any interest that would conflict in any manner or degree with the performance of the AGREEMENT.

R. Default

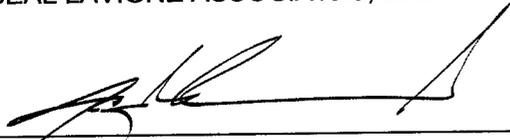
If it should appear at any time that the CONSULTANT has failed, refused, or delayed to perform or satisfy the PROJECT or any other requirement of this Agreement ("Event of Default"), and fails to cure any such Event of Default within ten business days after the CONSULTANT's receipt of written notice of such Event of Default from the City, then the City shall have the right, without prejudice to any other remedies provided by law or equity, to pursue any one or more of the following remedies:

1. Cure by CONSULTANT. The City may require the CONSULTANT, within a reasonable time, to complete or correct all or any part of the PROJECT that is the subject of the Event of Default; and to take any or all other action necessary to bring the CONSULTANT and the PROJECT into compliance with this Agreement.
2. Termination of Agreement by City. The City may terminate this Agreement pursuant to Article K of this Agreement.
3. Withholding of Payment by City. The City may withhold from any payment, whether or not previously approved, or may recover from the CONSULTANT, any and all costs, including attorneys' fees and administrative expenses, incurred by the City as the result of any Event of Default by the Consultant or as a result of actions taken by the City in response to any Event of Default by the CONSULTANT.

IN WITNESS WHEREOF, the CLIENT and the CONSULTANT have executed this AGREEMENT on the date and year first above written.

CONSULTANT:

HOUSEAL LAVIGNE ASSOCIATES, LLC.

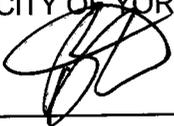
X  _____

John A. Houseal, AICP
Principal

Date: March 4, 2019

CLIENT:

UNITED CITY OF YORKVILLE

X  _____

Name/Title: Bart Olson, City Administrator

Date: 3-8-2019

ATTACHMENT A

SCOPE OF SERVICES and HOURLY RATES

This section describes the Scope of Services for preparing the Unified Development Ordinance for the United City of Yorkville.

Section 1

Whereas the scope of services will be undertaken by the CONSULTANT, it is understood and agreed that the CLIENT will provide the following assistance to the CONSULTANT:

1. The CLIENT, with the CONSULTANT's assistance, will schedule and arrange and provide notices for all meetings and workshops including contacting agencies, individuals and citizens to be invited to meetings.
2. The CLIENT, with the CONSULTANT'S assistance, will collect and compile previously prepared and available reports, projects, studies, maps and other data owned or in control of the CLIENT and that might be useful for the project.
3. The CLIENT will provide to the CONSULTANT an up-to-date base map (electronic and hard copy) for the City, including GIS files and information.

Section 2

SCOPE OF WORK

Below is a detailed, step-by-step, Scope of Work for completing the United City of Yorkville's *Unified Development Ordinance (UDO)* assignment. The Scope of Work contains several steps, including staff meetings; public workshops; "best practices" research and applicability assessment; evaluation of neighborhoods, corridors, and districts; Planning and Zoning Commission and City Commission workshops and meetings, public hearings, development of "user-friendly" formats, and more.

Step 1: Project Initiation and Outreach

Step 1 represents the kick-off of the project and is designed to provide City staff and the consultant team with opportunities to set project expectations and protocols, convey issues and concerns with the existing code, and engage those who administer and are impacted by the code.

1a: Project Initiation – Staff Meeting & Review of Preliminary Issues

This step will include a meeting with City staff to review various aspects of the project, including the schedule, expectations of local appointed and elected leaders, and public outreach and adoption process. The meeting will also provide an opportunity for staff and consultants to discuss any issues related to the present ordinances already identified by staff. If necessary, staff may lead a driving tour of the City to point out specific instances where current zoning and subdivision regulations either conflict with or complement community development objectives.

1b: Planning and Zoning Commission Workshop

This step will include a workshop with City staff and the Planning and Zoning Commission (PZC) to identify, review, and discuss zoning related matters within the City. The PZC members have a unique perspective and insight into local zoning, planning, and development issues and will provide vital feedback and project focus. They also can articulate frustrations with current ordinances as policy tools, such as difficulties in navigating the document, unclear regulations, inconsistent references or definitions, issues with procedures and enforcement, etc.

1c: Residential Areas Zoning Workshop

A workshop will be conducted with City residents to identify, review, and discuss zoning and ordinance related matters pertaining to the City's residential areas/neighborhoods. Attendees could include residents, neighborhood groups, local builders, local residential architects, and more.

1d: Commercial and Industrial Areas Zoning Workshop

A workshop will be conducted with business owners and managers in the City. This workshop will allow the local business community to provide input regarding the impact of zoning and subdivision controls on their ability to invest in Yorkville, successfully operate a business, and meet the goals that the City has set forth for development. Attendees should include local business owners and managers, developers, local commercial and industrial property owners, and more.

1e: Subdivision Ordinance Focus Group

We will work with City staff and officials to identify various stakeholders with extensive experience and understanding of the existing Subdivision Ordinance, including members of the development community, engineers, and significant property owners. We will reconcile preliminary findings with stakeholder input to ensure that recommended changes to subdivision regulations are vetted with respect to development realities.

1f: Project Website

Houseal Lavigne is recognized as a leader in technology-based outreach and community engagement. For this assignment, we will work with City staff to determine requirements for a project website that can be used to provide information regarding the zoning update process and documents available for review and comment through the course of the project. As an alternative, we can work with existing City protocol to distribute materials for review through the PZC. The most appropriate approach will be determined through conversations with City staff during project initiation.

1g: map.social (Online Map-Based Engagement Platform) (Optional)

Should Yorkville see value in implementing a unique project website, as described in step 1F, we will feature map.social, an interactive web-based community issues mapping tool, on the project website. Developed by Houseal Lavigne, this tool allows users to identify, map, and comment on geographic areas of concern and valued community amenities. map.social simplifies the mapping process and familiarizes users with areas the project may affect in a manner that is exciting, interactive, and effective. Input from users allows us to create a composite map of community issues to assist with the development of the UDO from the perspective of the community.

Step 2: Technical Analysis & Best Practices

Step 2 includes a full assessment of the current code, as well as the development of a conceptual approach to ensure that the new code addresses local issues and meets statutory requirements.

2a: Assessment of Existing Land Use Regulations

A detailed and thorough review of the City's existing ordinances, including Zoning and Subdivision Control Ordinances, Landscape Ordinance, Stormwater Management Ordinance, Appearance Code, Downtown Overlay Districts (adoption pending), and any other relevant ordinances, will be undertaken as a starting point for preparing the new ordinance. The existing code will also be evaluated with respect to the recommendations and objectives of the City's Comprehensive Plan and other adopted policies. This step will highlight areas where the existing zoning district boundaries and related regulations are inconsistent with either what is currently built or what is envisioned as expressed in adopted plans and policies. This will establish a framework for the calibration of regulations to meet these conditions.

2b: "Best Practices" Research and Assessment

Extensive research will be undertaken to assess the best practices from around the region and country relating to zoning, development, and subdivision regulations. These best practices will be evaluated for appropriateness and applicability to the Yorkville community. Traditional zoning, form-based codes, hybrid zoning, incentive zoning, planned unit development

ordinances, processes and procedures will all be examined for potential application to the City, its developed areas, and its anticipated growth areas. The result of this task will be a memorandum identifying relevant best practices and their potential application in Yorkville. The memorandum will include; a summary of issues identified through workshops conducted in Steps 1a-1d, a summary of the assessment of the current code conducted in Step 2a, and a conceptual approach to the new code that will address topics related to:

- District regulatory metrics and thresholds
- Structure and navigability of the code document
- The integration of tables, graphics and other tools to clarify regulations and standards
- The use of references to other portions of the City's municipal code
- Administrative procedures and processes, permits and approvals

2c: Form-based Code Applicability Analysis – Evaluation of Neighborhoods, Corridors, Districts

The United City of Yorkville is currently preparing a Downtown Overlay District with form-based regulations for Downtown Yorkville. This task will entail the evaluation and analysis of how applicable a form-based zoning approach may be to other areas of the City, such as commercial areas, corridors, residential areas and neighborhoods. It will consider the existing character of various residential and commercial areas, the vision as articulated in the Comprehensive Plan, and other factors that may impact the viability of form-based regulations and their implementation. In any case, this step will result in specific recommendations regarding the applicability and implementation of form-based regulations that will be highlighted in dialogue with the PZC as a part of Step 2e.

2d: City Staff Working Session

We will conduct a working session with City staff to 1) review input and findings resulting from various workshops, staff comments and direction, the technical analysis of the existing code, and the legal review of the code, and 2) comprehensively review the existing code and identify an overall direction and outline to present to the PZC.

2e: PZC Meeting

This step entails a summary presentation to the PZC of the input received and issues identified in preceding steps. A preliminary approach will be also discussed, and the PZC will be invited to provide feedback regarding the proposed conceptual direction of the new code, and its responsiveness to local issues discussed thus far. This meeting will also include a conversation regarding the recommendations developed as a part of step 2c pertaining to the applicability of further form-based regulations.

Step 3: Draft District Standards & Concepts (existing Title 10: Ch. 3, 5 – 13)

Step 3 includes the development of draft zoning language for all districts identified in the existing code. During this step, we anticipate regular contact with City staff to review incremental recommendations and technical language. The PZC will be engaged at key points to provide input regarding general district regulations, then other regulations that complement the functionality and character of various districts. The creation of new districts and the elimination of some existing districts may be a part of this step.

3a: Draft Residential and Agricultural District Standards

Preliminary Residential and Agricultural District Standards will be prepared for local consideration and discussion, based on community feedback, research and assessment, and community goals and objectives. This phase of the UDO process will focus on district issues and will be presented in an easy to use manner—enabling residents, builders, City officials, and staff to effectively implement the new regulations. This step will include use classifications and standards related to height, coverage, setback, and more.

3b: Draft Commercial and Industrial District Standards

Preliminary Commercial and Industrial District Standards will be prepared, similar to Step 3a, however this phase of the UDO process will focus on the City's non-residential areas. These draft standards are intended to respond to the needs of business owners in a manner that is balanced with other City objectives. Both the form and function of the City's commercial and industrial areas will be effectively addressed with the new regulations, in a manner consistent with the priorities of the 2016 Comprehensive Plan. This step will include use classifications and standards related to height, coverage, setback, and more.

3c: Special District Standards (OS, Downtown Overlay)

Preliminary District Standards will be prepared for special districts including Open Space, Downtown Overlay districts, and any new overlays. We will work with City staff to carefully review these districts to identify specific problems or concerns that need to be addressed and then prepare standards that improve the efficiency and effectiveness of the special districts as tools to help the City implement its priorities and Comprehensive Plan recommendations.

3d. Proposed Zoning Districts Map

Consistent with proposed zoning districts, an official zoning map will be prepared for consideration. The map could reflect new districts, boundary changes to existing districts, or the elimination of some districts.

3e: Staff Review Meeting

This step will include a meeting or conference call with City staff to review the draft district regulations, and any proposed map changes. Appropriate modifications will be made prior to presentation to the PZC.

3f: PZC Meeting

A meeting will be conducted with the PZC to review and discuss the Draft district standards. Appropriate revisions will be made based on PZC review and discussion.

Step 4: General Development Standards

(exist Title 10: Ch. 14, 16-20, Landscaping, Stormwater Management, Appearance Codes)

Step 4 includes the drafting of sections of the code related to "development standards of general applicability". These will include the drafting of sections of the code and regulations that impact all districts, including regulations related to general site development standards, landscaping standards, use-specific regulations, parking design and capacity, and design and location standards. Each set of standards and regulations will include the creation of relevant graphics to illustrate regulatory concepts and standards.

4a: Preliminary Amendments for Parking, Landscaping, Development Standards, etc.

Preliminary amendments for several “non-district” sections of the code will be prepared for local consideration and discussion. These will likely include but not be limited to parking, loading, landscaping, telecommunications infrastructure, alternative energy, definitions, and more. This step will also involve updates and revisions to the sign ordinance.

4b: Staff Review Meeting

This task will include a meeting or conference call with City staff to review the draft standards for general development. Appropriate modifications will be made prior to presentation to the PZC.

Step 5: Draft Subdivision Design/Improvements (existing Title 11)

Step 5 includes modifications to the subdivision code to ensure that it aligns with zoning regulations, the goals of the Comprehensive Plan, and the direction set forth for the new Unified Development Ordinance.

5a: Review of Subdivision Code for Compatibility and Best Practices

This Step includes the review of the subdivision control ordinance to ensure that its procedures, requirements and standards align with the Comprehensive Plan and national best practices, as it relates to parks and infrastructure provisions, street and lot dimensions, review and permitting procedures, and approval and appeals, and more.

5b: Draft Modified Subdivision Design/Improvement Regulations

This step includes the drafting of a modified subdivision code that reflects input from previous steps.

5c: Staff Review Meeting

This step will include a meeting or conference call with City staff to review revisions to subdivision design/improvement regulations prior to incorporation into the Unified Development Ordinance. Appropriate modifications will be made prior to presentation to the PZC.

5d: PZC Meeting

A meeting will be conducted with City staff and the PZC to review and discuss draft language developed as part of Steps 4 and 5. At this point in the process, the PZC will be engaged to discuss the regulations guiding development in each district, supplemental regulations, and subdivision regulations applicable throughout the City. Appropriate revisions will be made based on PZC review and discussion.

Step 6: Administrative and Procedural Standards (existing Title 10: Ch. 1 – 2, 4, 15)

Step 6 includes the drafting of sections of the code related to administrative processes and support materials. This section will seek to maximize the efficiency of zoning review so that development quality in Yorkville can be improved and permitted, including updates to non-conformity regulations, and procedural requirements for applicants, review standards, and processes for various zoning adjustments (variations, amendments, special uses, planned unit developments, and more). This step also includes the drafting of definitions that will support regulatory concepts and minimize the likelihood of inconsistent interpretation of regulations. Additionally, any relevant graphics will be created to illustrate procedural steps or concepts that

could otherwise be subject to interpretation.

6a: Draft Administrative, Applications, Approval, and Procedures

This step will result in recommendations for application and approval procedures. It will also include flow charts and diagrams clearly articulating such procedures in order to ensure that applicants and code administrators understand who is responsible for various recommendations or approvals.

6b: Staff Review Meeting

This step will include a meeting or conference call with City staff to review the draft administrative and procedural standards. Appropriate modifications will be made prior to presentation to the PZC.

6c: PZC Meeting

A meeting will be conducted with City staff and the PZC to review and discuss draft language developed as part of Step 6, as it applies to processes and procedures. Appropriate revisions will be made based on PZC review and discussion.

Step 7: Draft and Final UDO

Step 7 includes the delivery of the Draft Unified Development Ordinance, review and revisions to the Draft UDO, and presentation and adoption of the Final UDO.

7a: Draft UDO Ordinance

The draft Unified Development Ordinance will be prepared for local consideration and discussion, based on feedback from previous steps in the planning process.

7b: Staff Review and Meeting

A meeting or conference call will be conducted with City staff to review and discuss the Draft UDO. Appropriate revisions will be made based on staff review and discussion. A revised Draft UDO will be prepared for PZC review and discussion.

7c: PZC Meeting

A meeting will be conducted with the PZC to review and discuss the Draft Unified Development Ordinance. Appropriate revisions will be made based on PZC feedback.

7d: Revised UDO & Final Legal Review

Based on discussion and feedback from staff and the PZC, the revised draft ordinance will be prepared for public hearing. At this time, the City Attorney will also conduct a final legal review to ensure that any modifications are in line with statutory requirements.

7e: Public Hearing

A Planning and Zoning Commission public hearing will be conducted consider and recommend action on the draft Unified Development Ordinance.

7f: City Council Presentation and Adoption

Following the public hearing and the recommendation of the PZC, appropriate revisions will be

made, and the Final Unified Development Ordinance will be presented to the City Council for adoption.

Step 8: Web-Based “Smart Code” Integration

Step 8 includes the creation of an online web-based “smart code” version of the UDO with interactive, site specific zoning analysis capabilities. Houseal Lavigne proposes the following web-based “smart code” option, detailed below.

enCode Plus

enCode Plus is a web-based document presentation and content management system that is designed to ease navigation, understanding, and use of zoning regulations. Pricing for the use of this system after the UDO is adopted varies based on yearly maintenance fees and optional features. Fees associated with these features will be directly billed from encode plus.

Section 3

HOURLY RATES

Houseal Lavigne Associates Hourly Rates

John Houseal.....	\$205
Nik Davis.....	\$185
Carly Petersen.....	\$120
Jackie Wells.....	\$110
Trisha Stevens.....	\$110

United City of Yorkville

Unified Development Ordinance (UDO)

Houseal Lavigne Associates - Scope of Services

Note: Scope is amended to include Advisory Committee workshops (in place of PZC)

Step 1: Project Initiation and Outreach

Step 1 represents the kick-off of the project and is designed to provide City staff and the consultant team with opportunities to set project expectations and protocols, convey issues and concerns with the existing code, and engage those who administer and are impacted by the code.

1a: Project Initiation – Staff Meeting & Review of Preliminary Issues

This step will include a meeting with City staff to review various aspects of the project, including the schedule, expectations of local appointed and elected leaders, and public outreach and adoption process. The meeting will also provide an opportunity for staff and consultants to discuss any issues related to the present ordinances already identified by staff. If necessary, staff may lead a driving tour of the City to point out specific instances where current zoning and subdivision regulations either conflict with or complement community development objectives.

1b: UDO Advisory Committee Workshop

This step will include a workshop with City staff and a Council-approved *UDO Advisory Committee* to identify, review, and discuss zoning related matters within the City. The UDO Advisory Committee members should bring a unique perspective and insight into local zoning, planning, and development issues and will provide vital feedback and project focus. They also can articulate frustrations with current ordinances as policy tools, such as difficulties in navigating the document, unclear regulations, inconsistent references or definitions, issues with procedures and enforcement, etc.

1c: Residential Areas Zoning Workshop

A workshop will be conducted with City residents to identify, review, and discuss zoning and ordinance related matters pertaining to the City's residential areas/neighborhoods. Attendees could include residents, neighborhood groups, local builders, local residential architects, and more.

1d: Commercial and Industrial Areas Zoning Workshop

A workshop will be conducted with business owners and managers in the City. This workshop will allow the local business community to provide input regarding the impact of zoning and subdivision controls on their ability to invest in Yorkville, successfully operate a business, and meet the goals that the City has set forth for development. Attendees should include local business owners and managers, developers, local commercial and industrial property owners, and more.

1e: Subdivision Ordinance Focus Group

We will work with City staff and officials to identify various stakeholders with extensive experience and understanding of the existing Subdivision Ordinance, including members of the development community, engineers, and significant property owners. We will reconcile preliminary findings with stakeholder input to ensure that recommended changes to subdivision regulations are vetted with respect to development realities.

1f: Project Website

Houseal Lavigne is recognized as a leader in technology-based outreach and community engagement. For this assignment, we will work with City staff to determine requirements for a project website that can be used to

provide information regarding the zoning update process and documents available for review and comment through the course of the project. As an alternative, we can work with existing City protocol to distribute materials for review through the Planning and Zoning Commission (PZC). The most appropriate approach will be determined through conversations with City staff during project initiation.

1g: map.social (Online Map-Based Engagement Platform) (Optional)

Should Yorkville see value in implementing a unique project website, as described in step 1F, we will feature map.social, an interactive web-based community issues mapping tool, on the project website. Developed by Houseal Lavigne, this tool allows users to identify, map, and comment on geographic areas of concern and valued community amenities. map.social simplifies the mapping process and familiarizes users with areas the project may affect in a manner that is exciting, interactive, and effective. Input from users allows us to create a composite map of community issues to assist with the development of the UDO from the perspective of the community.

Step 2: Technical Analysis & Best Practices

Step 2 includes a full assessment of the current code, as well as the development of a conceptual approach to ensure that the new code addresses local issues and meets statutory requirements.

2a: Assessment of Existing Land Use Regulations

A detailed and thorough review of the City's existing ordinances, including Zoning and Subdivision Control Ordinances, Landscape Ordinance, Stormwater Management Ordinance, Appearance Code, Downtown Overlay Districts (adoption pending), and any other relevant ordinances, will be undertaken as a starting point for preparing the new ordinance. The existing code will also be evaluated with respect to the recommendations and objectives of the City's Comprehensive Plan and other adopted policies. This step will highlight areas where the existing zoning district boundaries and related regulations are inconsistent with either what is currently built or what is envisioned as expressed in adopted plans and policies. This will establish a framework for the calibration of regulations to meet these conditions.

2b: "Best Practices" Research and Assessment

Extensive research will be undertaken to assess the best practices from around the region and country relating to zoning, development, and subdivision regulations. These best practices will be evaluated for appropriateness and applicability to the Yorkville community. Traditional zoning, form-based codes, hybrid zoning, incentive zoning, planned unit development ordinances, processes and procedures will all be examined for potential application to the City, its developed areas, and its anticipated growth areas. The result of this task will be a memorandum identifying relevant best practices and their potential application in Yorkville. The memorandum will include; a summary of issues identified through workshops conducted in Steps 1a-1d, a summary of the assessment of the current code conducted in Step 2a, and a conceptual approach to the new code that will address topics related to:

- District regulatory metrics and thresholds
- Structure and navigability of the code document
- The integration of tables, graphics and other tools to clarify regulations and standards
- The use of references to other portions of the City's municipal code
- Administrative procedures and processes, permits and approvals

2c: Form-based Code Applicability Analysis – Evaluation of Neighborhoods, Corridors, Districts

The United City of Yorkville is currently preparing a Downtown Overlay District with form-based regulations for Downtown Yorkville. This task will entail the evaluation and analysis of how applicable a form-based zoning approach may be to other areas of the City, such as commercial areas, corridors, residential areas and

neighborhoods. It will consider the existing character of various residential and commercial areas, the vision as articulated in the Comprehensive Plan, and other factors that may impact the viability of form-based regulations and their implementation. In any case, this step will result in specific recommendations regarding the applicability and implementation of form-based regulations that will be highlighted in dialogue with the UDO Advisory Committee as a part of Step 2e.

2d: City Staff Working Session

We will conduct a working session with City staff to 1) review input and findings resulting from various workshops, staff comments and direction, the technical analysis of the existing code, and the legal review of the code, and 2) comprehensively review the existing code and identify an overall direction and outline to present to the UDO Advisory Committee.

2e: UDO Advisory Committee Meeting

This step entails a summary presentation to the UDO Advisory Committee of the input received and issues identified in preceding steps. A preliminary approach will be also discussed, and the committee will be invited to provide feedback regarding the proposed conceptual direction of the new code, and its responsiveness to local issues discussed thus far. This meeting will also include a conversation regarding the recommendations developed as a part of step 2c pertaining to the applicability of further form-based regulations.

Step 3: Draft District Standards & Concepts

Step 3 includes the development of draft zoning language for all districts identified in the existing code. During this step, we anticipate regular contact with City staff to review incremental recommendations and technical language. The UDO Advisory Committee will be engaged at key points to provide input regarding general district regulations, then other regulations that complement the functionality and character of various districts. The creation of new districts and the elimination of some existing districts may be a part of this step.

3a: Draft Residential and Agricultural District Standards

Preliminary Residential and Agricultural District Standards will be prepared for local consideration and discussion, based on community feedback, research and assessment, and community goals and objectives. This phase of the UDO process will focus on district issues and will be presented in an easy to use manner—enabling residents, builders, City officials, and staff to effectively implement the new regulations. This step will include use classifications and standards related to height, coverage, setback, and more.

3b: Draft Commercial and Industrial District Standards

Preliminary Commercial and Industrial District Standards will be prepared, similar to Step 3a, however this phase of the UDO process will focus on the City's non-residential areas. These draft standards are intended to respond to the needs of business owners in a manner that is balanced with other City objectives. Both the form and function of the City's commercial and industrial areas will be effectively addressed with the new regulations, in a manner consistent with the priorities of the 2016 Comprehensive Plan. This step will include use classifications and standards related to height, coverage, setback, and more.

3c: Special District Standards (OS, Downtown Overlay)

Preliminary District Standards will be prepared for special districts including Open Space, Downtown Overlay districts, and any new overlays. We will work with City staff to carefully review these districts to identify specific problems or concerns that need to be addressed and then prepare standards that improve the efficiency and effectiveness of the special districts as tools to help the City implement its priorities and Comprehensive Plan recommendations.

3d. Proposed Zoning Districts Map

Consistent with proposed zoning districts, an official zoning map will be prepared for consideration. The map could reflect new districts, boundary changes to existing districts, or the elimination of some districts.

3e: Staff Review Meeting

This step will include a meeting or conference call with City staff to review the draft district regulations, and any proposed map changes. Appropriate modifications will be made prior to presentation to the UDO Advisory Committee.

3f: UDO Advisory Committee Meeting

A meeting will be conducted with the UDO Advisory Committee to review and discuss the Draft district standards. Appropriate revisions will be made based on committee review and discussion.

Step 4: General Development Standards

Step 4 includes the drafting of sections of the code related to “development standards of general applicability”. These will include the drafting of sections of the code and regulations that impact all districts, including regulations related to general site development standards, landscaping standards, use-specific regulations, parking design and capacity, and design and location standards. Each set of standards and regulations will include the creation of relevant graphics to illustrate regulatory concepts and standards.

4a: Preliminary Amendments for Parking, Landscaping, Development Standards, etc.

Preliminary amendments for several “non-district” sections of the code will be prepared for local consideration and discussion. These will likely include but not be limited to parking, loading, landscaping, telecommunications infrastructure, alternative energy, definitions, and more. This step will also involve updates and revisions to the sign ordinance.

4b: Staff Review Meeting

This task will include a meeting or conference call with City staff to review the draft standards for general development. Appropriate modifications will be made prior to presentation to the UDO Advisory Committee.

Step 5: Draft Subdivision Design/Improvements

Step 5 includes modifications to the subdivision code to ensure that it aligns with zoning regulations, the goals of the Comprehensive Plan, and the direction set forth for the new Unified Development Ordinance.

5a: Review of Subdivision Code for Compatibility and Best Practices

This Step includes the review of the subdivision control ordinance to ensure that its procedures, requirements and standards align with the Comprehensive Plan and national best practices, as it relates to parks and infrastructure provisions, street and lot dimensions, review and permitting procedures, and approval and appeals, and more.

5b: Draft Modified Subdivision Design/Improvement Regulations

This step includes the drafting of a modified subdivision code that reflects input from previous steps.

5c: Staff Review Meeting

This step will include a meeting or conference call with City staff to review revisions to subdivision design/improvement regulations prior to incorporation into the Unified Development Ordinance. Appropriate modifications will be made prior to presentation to the UDO Advisory Committee.

5d: UDO Advisory Committee Meeting

A meeting will be conducted with City staff and the UDO Advisory Committee to review and discuss draft language developed as part of Steps 4 and 5. At this point in the process, the committee will be engaged to discuss the regulations guiding development in each district, supplemental regulations, and subdivision regulations applicable throughout the City. Appropriate revisions will be made based on committee review and discussion.

Step 6: Administrative and Procedural Standards

Step 6 includes the drafting of sections of the code related to administrative processes and support materials. This section will seek to maximize the efficiency of zoning review so that development quality in Yorkville can be improved and permitted, including updates to non-conformity regulations, and procedural requirements for applicants, review standards, and processes for various zoning adjustments (variations, amendments, special uses, planned unit developments, and more). This step also includes the drafting of definitions that will support regulatory concepts and minimize the likelihood of inconsistent interpretation of regulations. Additionally, any relevant graphics will be created to illustrate procedural steps or concepts that could otherwise be subject to interpretation.

6a: Draft Administrative, Applications, Approval, and Procedures

This step will result in recommendations for application and approval procedures. It will also include flow charts and diagrams clearly articulating such procedures in order to ensure that applicants and code administrators understand who is responsible for various recommendations or approvals.

6b: Staff Review Meeting

This step will include a meeting or conference call with City staff to review the draft administrative and procedural standards. Appropriate modifications will be made prior to presentation to the UDO Advisory Committee.

6c: UDO Advisory Committee Meeting

A meeting will be conducted with City staff and the UDO Advisory Committee to review and discuss draft language developed as part of Step 6, as it applies to processes and procedures. Appropriate revisions will be made based on committee review and discussion.

Step 7: Draft and Final UDO

Step 7 includes the delivery of the Draft Unified Development Ordinance, review and revisions to the Draft UDO, and presentation and adoption of the Final UDO.

7a: Draft UDO Ordinance

The draft Unified Development Ordinance will be prepared for local consideration and discussion, based on feedback from previous steps in the planning process.

7b: Staff Review and Meeting

A meeting or conference call will be conducted with City staff to review and discuss the Draft UDO. Appropriate revisions will be made based on staff review and discussion. A revised Draft UDO will be prepared for UDO Advisory Committee review and discussion.

7c: UDO Advisory Committee Meeting

A meeting will be conducted with the UDO Advisory Committee to review and discuss the Draft Unified Development Ordinance. Appropriate revisions will be made based on committee feedback.

7d: Revised UDO & Final Legal Review

Based on discussion and feedback from staff and the UDO Advisory Committee, the revised draft ordinance will be prepared for public hearing. At this time, the City Attorney will also conduct a final legal review to ensure that any modifications are in line with statutory requirements.

7e: PZC Public Hearing

A Planning and Zoning Commission public hearing will be conducted consider and recommend action on the draft Unified Development Ordinance.

7f: City Council Presentation and Adoption

Following the public hearing and the recommendation of the PZC, appropriate revisions will be made, and the Final Unified Development Ordinance will be presented to the City Council for adoption.

Step 8: Web-Based “Smart Code” Integration

Step 8 includes the creation of an online web-based “smart code” version of the UDO with interactive, site specific zoning analysis capabilities. Houseal Lavigne proposes the following web-based “smart code” option, detailed below.

enCode Plus

enCode Plus is a web-based document presentation and content management system that is designed to ease navigation, understanding, and use of zoning regulations. Pricing for the use of this system after the UDO is adopted varies based on yearly maintenance fees and optional features. Fees associated with these features will be directly billed from enCode Plus.

enCodePlus™
Software License
Advanced 180°

LICENSEE: YORKVILLE, ILLINOIS

This Software License (“Agreement” or “License”) is made as of the date of the last signature below, between the United City of Yorkville, an Illinois Municipal Corporation, hereinafter referred to as the “Licensee” and enCodePlus, LLC, a Texas Limited Liability Company, hereinafter referred to as “Licensor”, do hereby make and enter into the following license agreement.

- 1) **License Grant.** This is an Agreement between Licensor and Licensee, with a term as set out in Section 3, below. Licensor grants licensee a non-transferable, non-exclusive, limited, non-assignable license with no right to sublicense, to use the software covered by this Agreement pursuant to the terms of this Agreement including payment of all applicable License Fees. This License gives only certain rights to Licensee. All other rights are reserved to Licensor.
- 2) **Software Covered by this License.** The terms of this Agreement apply to Licensee’s use of the following software: Advanced 180°, including unlimited licenses seats and four hours of annual technical support.
- 3) **Term of License.**
 - a) The term of this Agreement is the period from **April 15, 2019 through April 14, 2020**, and is automatically renewable annually thereafter.
 - b) Termination, where permitted under this agreement, shall be effected by providing the other party with written notice of termination delivered at least 30 days prior to the date of termination for an early termination pursuant to this subparagraph 3.b, or two (2) months prior to the date of termination for a termination pursuant to subparagraph 3.c. Upon receipt of notice pursuant to this subparagraph to terminate this Agreement, fees for such maintenance and support paid in advance, if any, shall be returned to the Licensee on a pro rata basis. Early termination shall mean, for purposes of this Agreement, termination during the initial term of the Agreement or, in the event of an automatic renewal, termination more than six (6) months prior to the next automatic annual renewal. Early termination is permitted by either party in the event of a material breach by the other party by giving notice of termination as required in subparagraph 3.b, provided, however, no such termination shall become effective if the breaching party cures the breach within 30 days of the termination notice or, if it is not possible to fully cure the breach within said time period, commences cure and diligently pursues the same to conclusion. Licensor if, in Licensor’s sole discretion, changes in Internet technologies (including software platforms, web browsers, and other technologies that allow the Software to operate) make Licensor’s performance of this Agreement technically infeasible.

- c) Licensee may terminate this Agreement at any time and for any reason if notice of termination is delivered at least two (2) months prior to the date of termination.
- 4) Upon termination of this Agreement, Licensor is not obligated to return Licensee's content to Licensee. Licensee has full, unlimited access to the content through its password-protected maintenance module. This access allows the City to export the content to Microsoft Word and Adobe PDF. Also, as set out in Section 15, Security and Backup, the Licensee may save the document at any time in HTML format or as a web archive file. Licensee is advised to keep backups of its content. For an additional fee based on Licensor's then-current technical support rates, Licensor may offer export services to convert content into other file formats and deliver them to Licensee electronically or on optical or other solid-state media.
- 5) **Build Fees.** The fee of \$1,875 for Houseal Lavigne (Consultant Partner) to draft the UDO in the online format is waived with the upgrade. The one-time build fee for the Advanced 180° features (parking, shared parking, landscaping, bufferyard, signage, development yield, and fee calculators) is \$17,500, less a \$2,625 discount (15%) for a total build amount of \$14,875. The build fees are due as follows:
- a) Execution of Agreement: 50% (\$7,438)
 - b) Delivery: 50% (\$7,437)
- 6) **Exclusions.** This license excludes the following features: AppTrak+, Custom Site Design, Project Website, GIS and MLS integration, Escrow, MuniPro, Cloud Library, Definition Library, and Auto-Notification.
- 7) **License Fees.** The annual license fee, payable upon execution of this License and each subsequent year that this License is renewed, is \$5,250.
- 8) **Subsequent years:** The license fee for the 2020-2021 license year and subsequent years will be the standard license fee that enCodePlus, LLC charges for comparable Advanced 180° implementations.
- a) Subsequent Years: Licensor will notify the City in writing at least two months in advance of the first date of the contract term regarding the amount of the annual license fee for subsequent license years. If Licensor fails to provide such timely notice, then the Licensee shall pay either the amount identified in subparagraph 7, or the amount identified in Licensor's defective notice for such one-year term, whichever is less.
 - b) As applicable, the annual escrow fee payment set forth in Section 19 is due and payable with the annual license fee.
 - c) If any payment due under this Agreement ("Payment Due") is more than 15 days past due, a fee of 10% (ten percent) of the Payment Due shall be paid by Licensee as an Administrative Fee in addition to the Payment Due. If Payment Due is more than 30 days late, the Administrative Fee shall be 20% in addition to the Payment Due. If Payment Due is more than 45 days late, the enCodePlus site will be taken off line, and will not be restored until the Payments Due and any Administrative Fees associated with each Payment Due has been paid in full.

- 9) **Scope of License / Use of Software.** The Software is an Internet-based information processing and publication service that uses content created by Licensee, or by Licensor under separate agreement with Licensee. Licensor will provide Licensee access to the Internet-based interfaces, including password-protected access to maintenance features, as may be applicable to the Software. Licensee may use the Software through its Internet-based interfaces as follows:
- a) **enCodePlus™** may be used to develop, view, access, use, print, copy, edit, publish, utilize, administer, amend, repeal, and solicit, and process comments on the Unified Development Ordinance; and
 - b) Other uses as prescribed by this or subsequent agreements between Licensor and Licensee.
- 10) **Limitations on Use of Software.** Licensee **must not:**
- a) Reverse engineer, reverse assemble, decompile or disassemble the Software, or otherwise attempt to derive source code from the Software or any component thereof;
 - b) Publish or distribute materials for which Licensee does not have intellectual property rights, or which are outside of the scope of this License;
 - c) Access the server-side programs and source code upon which the Software is based, except to use the Internet-based interfaces provided by Licensor, and except for the HTML and browser-based script code that the server-side programs generate and deliver to end-users;
 - d) Alter the functionality of the software including by composition or injection of unapproved software or services;
 - e) Copy, reproduce, modify, sell, lease, sub-license, market, or commercially exploit in any way the Software or any component thereof (including the further distribution or blank forms or templates) other than as expressly agreed to in this Agreement; and
 - f) Use, or permit the use of, the Software except within the scope set out in Section 8. Licensee agrees that it shall not provide access to or perform services for third parties using the Software including, but not limited to, any service bureau, time-sharing, lease, distribution or re-sale, rental, application service provider arrangement, or any other arrangement.
- 11) **Warranty Against Infringement.**
- a) Licensor warrants that the copyright in and to the Software is owned by Licensor or is distributed by Licensor under a valid current license, that it has the right to license the Software, and that there are no pending liens, claims, or encumbrances against the Software or Licensor pertaining to the Software. Licensor agrees to notify Licensee of any actual or anticipated claims made against it or its customers for patent or copyright infringement in the use of the Software. Licensor agrees to indemnify, hold harmless, and defend Licensee against any and all patent or copyright infringement claims that may be brought against Licensee as the result of its use of the Software as authorized by this License. Licensee shall have the right to participate in the defense of any such claims for patent or copyright infringement at Licensee's cost and expense. In the event that it is determined that the Software infringes on any patent or copyright,

Licensor may (i) procure for the Licensee the right to continue using the Software; (ii) modify the Software so that it becomes non-infringing but continues to provide the same functionality as the original Software; (iii) replace the software with non-infringing software that continues to provide the same functionality as the original Software; or (iv) if modification or replacement cannot be accomplished in a manner that continues to provide the same functionality, Licensee may terminate this License and Licensor will refund to Licensee its pro-rated license fee for the balance of the year in which the infringement was determined.

- b) Licensee warrants that the copyright in and to the Content created and/or provided by Licensee for Licensor to use in the Software ("Licensee Content") is owned by Licensee or is published or distributed by Licensee under a valid current license, that Licensee has the right to license, use, publish or distribute the Licensee Content it provides to Licensor for use in the Software, and that there are no pending liens, claims, or encumbrances against the Licensee Content or Licensee pertaining to the Licensee Content. Licensee grants to Licensor a royalty-free, non-exclusive license: to use the Licensee Content for any purposes, including, without limitation, using, accessing, reproducing or displaying the Licensee Content within the Licensor's **enCodePlus™** Software; to reproduce the Licensee Content; to prepare derivative works based upon the Licensee Content; to distribute copies of the Licensee Content; to publish the Licensee Content; and to display the Licensee Content. Licensee agrees to notify Licensor of any actual or anticipated claims made against it or its customers for patent or copyright infringement in the use of the Licensee Content. Licensor shall have the right to participate in the defense of any claims for patent or copyright infringement at Licensor's cost and expense. In the event that it is determined that the Licensee Content infringes on any patent or copyright, Licensee may (i) procure for the Licensor the right to continue using the Licensee Content; (ii) modify the Licensee Content so that it becomes non-infringing but continues to provide the same functionality as the original Licensee Content; (iii) replace the Licensee Content with non-infringing content that continues to provide the same functionality as the original content; or (iv) if modification or replacement cannot be accomplished in a manner that continues to provide the same functionality, Licensor may terminate this License.

12) **Limitations of Software.** The Software is provided to facilitate access to information via the Internet. The Software is not a substitute for human judgment, which is necessary for administration of ordinances, plans, and administrative rules.

13) **Technical Support.**

- a) Included with the Advanced 180° license is four hours of annual technical support. Licensor will provide technical support at a rate of \$100 per hour (\$175 per hour for GIS technical support) for additional hours of support. Thereafter, technical support fees are subject to annual adjustment, and will be based on the standard schedule of professional fees for the year in which the support occurs.
- b) Licensor agrees to provide telephone or web-based technical support regarding the use of service to Licensee within two business days of telephone contact by Licensee during Licensor's business hours, or at a mutually agreeable time. Licensor makes no promises as to the duration of a resolution, except that

Licensor shall expedite the resolution to the best of its reasonable ability, and Licensor may deem a request as unresolvable.

- c) With respect to addressing errors which may arise in the Software, Licensor agrees to commence its best efforts to resolve such errors as soon as reasonably possible after notice by Licensee. Generally, Licensor will commence error resolution on the same day as the notice, and if reasonably practicable, will provide Licensee with Licensor's best good-faith estimate of the time which will be required to resolve the error. Licensee shall not be charged any technical support fee or other charge for addressing or correcting errors in the Software itself.
- d) Failure of Licensor to achieve the technical support response times set out in Subsections 12.b. and 12.c. shall not constitute a material breach of this Agreement.

14) Annual License Fee Discounts for Major Service Interruptions Due to Changes by Licensor.

- a) There will be reductions in the annual license fee if there is a complete inability to use both the user module and the maintenance module (hereinafter "major service interruption") of the Software as a result of changes to the Software made by Licensor, as follows:
 - i. If a major service interruption lasts more than two, but less than five consecutive days, the annual license fee for the following license year will be reduced by 10 percent.
 - ii. If a major service interruption lasts more than five days, but less than 14 consecutive days, the annual license fee for the following license year will be reduced by 25 percent.
 - iii. If a major service interruption lasts more than 14 days, but less than 30 consecutive days, the annual license fee for the following license year will be reduced by 50 percent.
- b) If a major service interruption lasts more than 30 consecutive days, the annual license fee for the following license year shall be waived.
- c) The reductions of license fees that are provided by this Section are not cumulative. If more than one service interruption occurs during a license year, then only the discount that applies to the longest of the service interruptions shall be applied.
- d) This Section does not apply to outages that are attributable to failures of a third-party Internet Service Provider that hosts the Software.
- e) If a failure by a third-party Internet Service Provider impacts the use of the Software for more than five consecutive days, Licensor will seek another Internet Service Provider to host the Software. The selection of a new Internet Service Provider will be in Licensor's sole discretion, however, Licensor will seek quality providers with secure data centers, geographic redundancy, the ability to bring up new instances in a short time frame, and excluding planned downtime, either 99.9 percent or greater guarantee for storage uptime or a strong track-record of service reliability.

15) Updates and Upgrades.

- a) Licensor will keep the Software in operational condition on a server hosted by Licensor or a third-party Internet Service Provider. Although Licensor selects its Internet Service Providers based on their reputation for reliability and service, Licensor does not guarantee 100 percent uptime. Licensor will promptly notify the Licensor's contact listed in Section 24 by electronic mail if its Internet Service Provider or third-party hosting service experiences an outage that prevents or severely limits access to the Software for a period in excess of 15 minutes per day or cumulative period in excess of 15 minutes per day.
- b) Licensor may, at its sole option, apply minor updates from time to time and will provide notice to Licensee within two business days of applying minor updates. Generally, minor updates are those updates which ensure compatibility with server or browser technologies or provide additional data security or enhance system stability or operability of the software.
- c) Licensor may develop major upgrades to the functionality or interfaces of the software from time to time and will provide notice to Licensee within two business days before applying major upgrades. Such major upgrades that affect data security, system compatibility or stability, or operability will be offered at no additional cost during the term of this Agreement. Major upgrades that include functional changes, such as new or customized features, may be offered to Licensee for an additional fee (which may include an additional annual license fee) to be determined by Licensor when they are released. The Licensee may choose to accept or deny major upgrades that involve new or customized features requiring an additional fee. If Licensee declines a major upgrade, Licensee's software version will be maintained intact through the license term.
- d) The Software requires the use of an Internet browser. As of the effective date of this Software License, the Software is fully operable using Microsoft Internet Explorer (IE) 10 and 11. The browser version is supported by the current and one previous major version. On an additional fee basis, Licensee may include a rider to this Agreement to maintain the operability of previous versions other than those provided for in this Agreement. Other browsers supported for desktop editions include FireFox, Chrome, and Safari. Licensor does not guarantee full operability in all browsers, because browser technology changes from time to time.
- e) Licensor may from time to time, in its sole discretion, change some or all of the functionality or any component of the Software or make any modification for the purpose of improving the performance, service quality, error correction or to maintain the responsiveness of the Software. Such changes shall not negatively impair the overall functionality of the Software.

16) Security and Backup.

- a) Licensor provides password-level security to the maintenance module of the Software, which allows Licensee to edit regulatory text, upload and link to graphics and multimedia components, create usernames and passwords for Licensee's staff, and review and respond to public comments. Licensee is responsible

for the securing of passwords and preventing their unauthorized use. Licensee may not select a username which is identical to that used by another person or use a username which is in the sole opinion of Licensor offensive or inappropriate. Licensee shall be solely responsible for maintaining the confidentiality of their password. Licensee is solely responsible for all usage or activity on Licensee's account, including but not limited to use of Licensee's account, Licensee's user name, and Licensee's password by any third party. Any fraudulent, abusive, or otherwise illegal activity may be grounds for termination of Licensee's account, in Licensor's sole discretion, and Licensor may refer Licensee to appropriate law enforcement agencies.

- b) Licensee is advised to maintain a current off-line backup of the data stored in the Software. A complete version of the published code document is available at: <http://online.enCodePlus.com/regs/yorkville-il/maintain/allcodepub.asp>. This document may be saved as a web archive file.

17) **Ownership of Content.**

- a) Licensee is the sole owner of all documents and information provided by Licensee that will be accessed by Licensee and others through use of the Software. Nothing in this License grants Licensor any interest in said documents and information and the Licensee has the sole rights to use and to authorize others to use the documents and information regardless of whether this License remains in effect.
- b) Licensor may post "terms of service" on web pages that are used to access the Software, which may disclaim: (1) any interest of Licensor in Licensee's contents; (2) any liability for use of Licensor's contents; and (3) any damages that may occur due to malware, viruses, or other malicious code that may be accessible through Licensee's content.
- c) Licensor will not charge for public access to Licensee's content. However, Licensor may develop fee-based services, such as automatic notification of code updates. If Licensor develops such services, they will be considered a "major upgrade," and Licensor will negotiate with Licensee with respect to pricing and distribution of proceeds.

18) **Intellectual Property Rights.** Licensor shall retain all ownership, title, copyright, patent, trademark, and other proprietary rights in and to the Software and any component thereof, and all content, features, and functionalities of the Software. Licensee does not acquire any rights, express or implied, in the Software, other than those specified in this Agreement.

19) **Software Escrow.** If selected as an option and reflected in Paragraph 5, Build Fees, Licensor shall place all source code constituting and relating to the Software into an escrow account pursuant to the terms of an escrow agreement (the "Escrow Agreement") to be entered into between Licensor and the escrow agent (the "Escrow Agent") which shall name Licensee as a beneficiary; provided, however, that the Escrow Agreement shall contain substantially the following conditions with respect to release of escrow to Licensee: (i) Licensor is adjudged bankrupt, and/or (ii) in the event of a catastrophic loss which terminates Licensor's operations, then Licensee shall have a non-exclusive, royalty-free, perpetual, worldwide license to use the source code released

from the escrow in its sole discretion, solely for its own internal use and not for any resell, license, or sub-license. In consideration of Licensor placing the source code in escrow, Licensee will pay an annual escrow fee of \$1,000, plus a \$500 escrow initiation fee, for the first license year and an annual source code escrow fee of \$1,000 thereafter. All charges for additional services which may be requested from the escrow company by Licensee as a beneficiary to the escrow agreement shall be payable by Licensee.

- 20) **LIMITATION OF LIABILITY.** Neither Licensor nor Licensee shall be liable to the other party for special, incidental, indirect, or consequential damages for any loss or claim by either party.
- 21) **LIMITATION OF DAMAGES.** Except as provided in Sections 9, 10 and 11, and except for bodily injury or violations of Section 21, it is understood and agreed that neither party's liability, whether in contract or tort, shall exceed the amount received by Licensor from Licensee for the license year in which a cause of action arose, the license fee stated in Subsection 7 is consideration in limiting damages available to each party.
- 22) **Confidential Information.**
- a) For purposes of this Agreement, "Confidential Information" means any software program(s) for electronic data processing and the documentation thereof, and plans, designs, drawings, specifications and trade secrets prepared by Licensor and which remain the property of Licensor.
 - b) Each party acknowledges that Confidential Information may be exchanged between the parties pursuant to this Agreement. Each party shall use no less than the same means it uses to protect its similar confidential and proprietary information, but in any event not less than reasonable means, to prevent the disclosure and to protect the confidentiality of the Confidential Information of the other party. Each party agrees that it will not disclose or use the Confidential Information of the other party except for the purposes of this Agreement. Licensee expressly agrees that, unless ordered by a court of competent jurisdiction, it will not disclose any confidential information, as defined herein, without first obtaining the approval of Licensor in writing. Licensee will promptly report to Licensor any disclosure of Licensor's Confidential Information that the Licensee becomes aware of and provide reasonable assistance to Licensor in the investigation and prosecution of any such unauthorized use or disclosure.
 - c) Notwithstanding the foregoing, the recipient of Confidential Information may use or disclose the Confidential Information to the extent that such Confidential Information is: (i) already known by the recipient without an obligation of confidentiality, (ii) publicly known or becomes publicly known through no unauthorized act of the recipient, (iii) rightfully received from a third party without any obligation of confidentiality, (iv) independently developed by the recipient without use of the Confidential Information of the disclosing party, (v) approved by the disclosing party for disclosure, or (vi) required to be disclosed pursuant to a requirement of a governmental agency or law so long as the recipient provides the disclosing party with notice of such requirement prior to any such disclosure and takes reasonable steps available to maintain the information in confidence, including allowing the disclosing party to participate in proceedings to defend the confidentiality of the Confidential Information should the Licensee decide to do so in its discretion.

- d) Licensor acknowledges that Licensee is a public entity subject to the requirements of the Illinois Freedom of Information Act and that Licensee shall endeavor to maintain the confidentiality of the Confidential Information of Licensor subject to the requirements of the Illinois Freedom of Information Act and except to the extent, Licensee’s employees have a need to access such Licensor Confidential Information in order to enable the Licensee to exercise its rights under this Agreement. Nothing in this Paragraph shall be construed to relieve Licensee of any of the duties and provisions of Subsection 22.b., above. Licensee agrees to notify the Licensor of its intent to provide information relating to this Agreement or the software pursuant to a public information request which the Licensee has determined does not constitute confidential information. It is the responsibility of Licensor to seek injunctive relief if the Licensor and Licensee disagree on what constitutes confidential information which Licensee shall not disclose in response to an open records request.
- e) Licensee agrees not to use, or allow its employees or independent contractors to use, the Software and / or Licensor’s Confidential Information to create any computer software or documentation that is substantially similar to the Software or its related documentation.

23) **Return of Confidential Information.** Upon the termination of this Agreement for any reason whatsoever, each party may request of the other that all documents, information, data, and/or software however recorded, which contain any of the other’s Confidential Information be returned, provided that the party shall be entitled to charge a reasonable fees and materials charge for doing so. If no request is received for the return of Confidential Information within thirty (30) days of the termination of this Agreement, the Confidential Information shall be destroyed within a reasonable time thereafter and shall not be used for any purpose whatsoever.

24) **Notices.**

- a) *Generally.* Except as provided in Subsection 22.b., notices delivered pursuant to this Agreement shall be sent to the people and addresses shown in this Section. Generally, notices shall be delivered by First Class Mail or courier service. Such notice shall be deemed to have been given when deposited in the United States Mail or courier service properly addressed to the intended recipient.

To Licensor	To Licensee
<p>Bret C. Keast, AICP, President enCodePlus, LLC 1415 Highway 6, Suite A-300 Sugar Land, TX 77478 Tel: (281) 302-5847 Email: bret@enCodePlus.com</p> <p><i>With copy to:</i> William G. Harger, Attorney at Law William G. Harger & Associates, PLLC 704 Main Street</p>	<p>Krysti J. Barksdale-Noble, AICP, CD Director United City of Yorkville 800 Game Farm Road Yorkville, Illinois 60560 Tel: (630) 553-8573 Email: noble@yorkville.il.us</p> <p><i>With a copy to:</i> Kathleen Field Orr Kathleen Field Orr & Associates 53 West Jackson Blvd.</p>

Richmond, Texas 77469 Tel: (281) 202-6000 Fax: (281) 715-4343 fax Email: harger@hargerlaw.com m	Suite 964 Chicago, Illinois 60604 Tel: (312) 382-2113 Email: kfo@kfoassoc.com
--	--

- b) *Upgrades, Maintenance, Service Interruptions, and Planned Outages.* Notices regarding upgrades, maintenance, service interruptions, and planned outages shall be delivered by electronic mail to the Senior Planner or designee. Licensee may change the individuals who receive notice pursuant to this subsection by electronic mail notice to Licensor.
- 25) **Assignment.** Licensor may not assign its rights and obligations under this Agreement without prior written notice to Licensee no less than 30 days prior to assignment. Licensee may thereafter continue with the terms of this Agreement attempt to renegotiate with the assignee, or terminate this license pursuant to the provisions of Section 3, Term of License.
- 26) **Third Party Beneficiaries.** There are no third-party beneficiaries to this Agreement.
- 27) **No Waiver.** The failure of either party to exercise or enforce any right or provision of this Agreement shall not constitute a waiver of such right or provision.
- 28) **Effective Date.** The effective date shall be the date on which the Licensee executes this Agreement, or such other date as may be agreed to by the parties and indicated as “Effective Date” below.
- a) Licensor and Licensee agree that this Agreement shall be construed in accordance with the laws of the State of Illinois. Venue for all actions brought pursuant to this agreement is in Kendall County, Illinois; and all parties consent to Kendall County, Illinois, being the exclusive jurisdiction to resolve said claims or controversies arising pursuant to this agreement. The parties will attempt in good faith to resolve any controversy or claim arising out of or relating to this Agreement promptly by negotiation between senior executives of the parties who have the authority to settle the controversy.
- b) The disputing party shall give the other party written notice of the dispute. The other party shall respond in writing within 10 days after receipt of said notice. The notice and response shall include: (1) a statement of the party’s position and a summary of the facts and arguments supporting its position; and (2) the name and title of the executive who will represent the party. The executives shall meet at a mutually acceptable time and place within 20 days of the date of the disputing party’s notice and thereafter as often as they reasonably deem necessary to exchange relevant information and to attempt to resolve the dispute.
- c) If the matter has not been resolved within 90 days of the commencement of such procedure, parties may bring their claim in a court of law. Venue for all actions brought pursuant to this agreement is in Kendall County, Illinois; and all parties consent to Kendall County, Illinois, being the exclusive jurisdiction to resolve said claims or controversies arising pursuant to this agreement.

- d) Nothing in this Section shall prevent a Party from bringing an action for injunctive relief if such relief is necessary for the protection of a right or property or proprietary information which might be lost absent such relief.
- 29) **Integration.** This Agreement constitutes the entire agreement between the parties with regard to the subject matter hereof and thereof. This Agreement supersedes all previous agreements between or among the parties. There are no agreements, representations, or warranties between or among the parties other than those set forth in this Agreement or the documents and agreements referred to in this Agreement.
- 30) **Force Majeure.** Neither party shall not be deemed in default of this Agreement to the extent that performance of its obligations or attempts to cure any breach are delayed or prevented by reason of any act of God, fire, natural disaster, or act of government provided that written notice thereof is provided to the other party promptly upon discovery thereof and uses its best efforts to cure the delay.
- 31) **Amendments and Modifications.** Together with the separately executed Professional Services Agreement for the Unified Development Ordinance regarding the build fees set out in Paragraph 5, this Agreement constitutes the parties' entire agreement. No amendment, modification, or supplement to this Agreement shall be binding on any of the parties unless it is in writing and signed by the parties.
- 32) **General Interpretation.** The terms of this Agreement have been negotiated by the parties hereto and the language used in this Agreement shall be deemed to be the language chosen by the parties hereto to express their mutual intent. This Agreement shall be construed without regard to any presumption or rule requiring construction against the party causing such instrument or any portion thereof to be drafted, or in favor of the party receiving a particular benefit under the agreement. No rule of strict construction will be applied against any person.
- 33) **Further Assurances.** Each of the parties agree to take such further action to execute and deliver such additional documents as may be reasonably required to them to effectuate the purpose and intent of this Agreement.
- 34) **Severability.** If any term or provision of this Agreement is determined to be illegal, unenforceable, or invalid in whole or in part for any reason, such illegal, unenforceable, or invalid provisions or part thereof shall be stricken from this Agreement, and such provision shall not affect the legality, enforceability, or validity of the remainder of this Agreement. If any provision or part thereof of this Agreement is stricken in accordance with the provisions of this section, then this stricken provision shall be replaced, to the extent possible, with a legal, enforceable, and valid provision that is as similar in tenor to the stricken provision as is legally possible.

April 15, 2019

Effective Date

Licensee

enCodePlus, LLC

Licensor



Authorized Agent Signature

Authorized Agent Signature

Authorized Agent (Typed)

Bret C. Keast, President

Authorized Agent (Typed)

Date

Date

EXHIBIT A
REQUEST FOR USER NAME AND PASSWORD

The City of Yorkville, Illinois (“Licensee”) requests that a new password be issued by **enCodePlus, LLC** (“Licensor”) for the benefit of Licensee to enable the undersigned user (“User”) to access and use the maintenance module and User Guide for the software program. Licensee and the undersigned agree to comply with the below provisions of the license agreement between Licensor and Licensee:

Licensee agrees that each of Licensee’s employees having administrative or maintenance access to the Software, and at the full discretion of Licensor, contract workers and any third parties acting on behalf of Licensee or any other person or entity acting as an agent for Licensee in any capacity, shall be identified to Licensor and issued a unique password. Each person receiving a password must first complete and sign the user acknowledgment form below. Submission of such acknowledgment form will constitute a request by Licensee for issuance of a new password for a particular individual, and upon receipt of a completed acknowledgment form, Licensor will issue a password for that individual user. **Passwords are not transferable.** When Licensee becomes aware that a user will no longer need a password, for any reason, Licensee agrees to immediately notify Licensor that the password is no longer active. Any replacement users will be issued a new password. Licensee assumes full responsibility for any improper use of a password issued at the request of Licensee, and indemnifies Licensor from any loss by or damage to Licensor that may occur as a result of an improper use of any such password.

The undersigned supervisor and each licensed user agree to the terms of this Agreement and are responsible for the User’s compliance.

SUPERVISOR

Printed name: _____

Date: _____



USER NO. 1

Printed name: _____

Date: _____

User's address: _____

User's relationship to Licensee: _____

Username: _____

User's password: _____

USER NO. 2

Printed name: _____

Date: _____

User's address: _____

User's relationship to Licensee: _____

Username: _____

User's password: _____



USER NO. 3

Printed name: _____

Date: _____

User's address: _____

User's relationship to Licensee: _____

Username: _____

User's password: _____

USER NO. 4

Printed name: _____

Date: _____

User's address: _____

User's relationship to Licensee: _____

Username: _____

User's password: _____



Reviewed By:	
Legal	<input type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

New Business #11

Tracking Number

EDC 2019-69

Agenda Item Summary Memo

Title: Old Jail Term Sheet

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: See attached memo.

Council Action Previously Taken:

Date of Action: _____ Action Taken: _____

Item Number: _____

Type of Vote Required: _____

Council Action Requested: _____

Submitted by: Bart Olson Administration
Name Department

Agenda Item Notes:



Memorandum

To: Economic Development Committee
From: Bart Olson, City Administrator
CC:
Date: July 30, 2019
Subject: Jail sale term sheet

Summary

Review of a term sheet for sale of the Historical Jail to KCJ Restoration, LLC (McKnight proposal).

Background

This item was last discussed by the City Council at the July 23rd meeting. At that meeting, the City Council heard a status update from staff that the negotiations for sale of the jail were ongoing. At this time, the majority of the development, sale, and TIF proposals have been agreed upon in concept. The narratives for those proposals are included in the attached term sheet. If the Economic Development Committee is ok with these terms, it is the intent to get a purchase, sale, and TIF agreement ready for the August 13th City Council meeting. While the developer has no hard deadline for purchase and close of the property, the building continues to take on water during weather events, and the proposer is concerned about moisture observed on the floor joists in the historical section.

The outline of the term sheet is as follows:

Section 1 – Developer name

Section 2 – Property boundaries. The exact legal description will be created at a later date, but the sale will not include the parking lot. Staff and the developer will decide upon an agreeable east-west property subdivision between the building and the parking lot to the north. The subdivision of the building from the parking lot may or may not require City Council action, depending on if the lots meet the Plat Act exemptions. The agreed upon purchase price will be the \$1,000 as originally proposed by the developer in their first submittal.

Section 3 – Right of first refusals, etc.

- The City has a right of first refusal on the property, until the project is complete. This protects the City from a scenario where the project is not completed and/or abandoned. Assuming the developer decides to sell the property during that stage, the City would have the option to match the offer and complete the project itself or sell to another developer.
- The developer has right of first refusal on the parking lot for the next 20 years. The building site has no available on-site parking, and so the developer is concerned that a successful project and continued success in the downtown on other properties will create a parking shortage. On the off-chance the City thinks the parking lot should be sold in the future, the developer would like the ability to match the offer and own the parking lot themselves.

- The City will add a deed restriction that the historic, eastern part of the building may never be torn down. The garage portion of the building has no such restriction going forward. This deed restriction could be lifted by mutual agreement between a future landowner and a future City Council.

Section 4 – Zoning issues

- The property is currently zoned B-1, which does not permit residential apartments on the ground floor. The property will need to be rezoned to R-3 or R-4 immediately following the approval of the agreement, which the City staff would support. However, if the City's Form Based Code is approved, the code would allow apartments on this property with no further City Council action.
- The commercial/historic side of the building is currently proposed to remain as B-1, although the City staff would support a more intense B-zoning district, if needed. The current permitted uses in B-1 will be referenced in this agreement (ex. bakeries, bookstores, coffee shops, community centers, offices, microbrewery/brewpub/microdistillery/microwineries, photography studio, restaurant, retail store, etc.) to protect the developer from future zoning changes. If the City's Form Based Code is approved, the code would allow more commercial uses such as hotel/motels.

Section 5 – Shows the intent of the developer, per their proposal.

Section 6 – Shows the intent of the project budget from the developer, per their proposal. The agreement will not contain any commitment to these dollar amounts, other than the cumulative TIF incentives paid out under Section 7 will never be able to exceed the TIF eligible expenses contained within this project budget.

Section 7 – Incentives. Finance Director Fredrickson has prepared the attached budget spreadsheet to show the long-term impacts of the incentives on Downtown TIF 2. In general:

- The City will pay the developer \$30,000 in TIF incentive after the roof has been fixed, and \$50,000 after the asbestos and lead-based paint is remediated. This combined \$80,000 is the equivalent to the cost of the City tearing down the building. We anticipate both payouts to occur by the end of calendar year 2019.
- The City will pay the developer \$17,500 in TIF incentive when the residential units in the building are fully complete. This money is coming from the entirety of the TIF 2 budget, and represents the City's risk in the TIF deal. We anticipate this payout to occur by the end of calendar year 2020.
- The City will pay the developer \$17,500 in TIF incentive when the commercial units are fully complete (no tenant required). This money is coming from the entirety of the TIF 2 budget, and represents the City's risk in the TIF deal. We anticipate this payout to occur by the end of 2021.
- During the construction of the building and for two years after the building is complete, the City will rebate 100% of all incremental property taxes on the jail property to the developer. We anticipate this payout to occur in FY 22, FY 23, and FY 24. This money will come from the incremental property taxes on the property and not from other funds within the TIF 2 budget, and this money is not guaranteed. For example, if the assessment on the property remains at zero all

the way through construction, no property taxes will be paid nor will any property taxes be rebated.

- After the 100% construction/post-construction rebates are satisfied above, the City will rebate the following percentages of incremental property taxes for 10 years or until the total payouts under this section reach an actual value of \$150,000:
 - o 90% for a period of 4 years and then
 - o 85% for a period of 3 years and then
 - o 80% for a period of 3 years

This money will come from the incremental property taxes on the property and not from other funds within the TIF 2 budget, and this money is not guaranteed. For example, if the incremental property taxes on the property only equate to \$5,000, the developer would receive 90%/85%/80% of that \$5,000. The developer's estimate is that the property will generate ~\$14,000 in property taxes when fully built out. For purposes of the attached budget illustration (referenced above), Rob and I assumed an inflationary growth to those property taxes over the life of the TIF.

- For the entire length of any TIF payments to the developer, if there are any students living on the property, the state law requires the City to set aside some of the incremental property taxes for the school district. For the section of TIF incentives referenced immediately above, the TIF incentives would be calculated from the remaining net amount of the incremental property taxes (i.e. after the school district set aside is done).
- All of the above is subject to the normal TIF requirement that we can only provide the above TIF payments if the developer has an equivalent or greater amount of TIF eligible expenses. We anticipate that almost all of their entire renovation costs will be TIF eligible under the law.
- The City will waive all building permit fees, plan review fees, water meter fees, water connection fees, and sewer connection fees for any part of the property for 5 years, and for the historic part of the property (jail section) for 10 years. These are foregone revenues and will not be reflected as an expenditure anywhere in the budget.
- The City will install water, sewer, and residential electric services into the building. The City has employees with the in-house capabilities to complete these projects. The City's out of pocket hard costs will be approximately \$7,500 for the water and sewer service lines and approximately \$2,500 for the electrical services. The agreement will be drafted in such a way that the approximate cost for a contractor to complete the work will be referenced in the agreement, such that if the scope of the expected work for all of the services changes, the City and the developer will be able to re-address the City's participation in the work; in exchange, the value of the water and sewer service at \$15,000 and the electrical service at \$5,000 will be converted into a TIF incentive (this would require an amendment to the TIF agreement).
- The City and the developer will work together to extend the existing nose-in diagonal parking on Madison St, further to the east. Staff thinks that the ADA spot in this area can be reassigned elsewhere, freeing up two parking spaces at minimal cost, and that the parking area could be extended several spots to the east (with County participation). The developer has agreed to pay for the cost of the parking spots to be added, at a cost of \$2,000 per space up to a maximum of \$10,000 in exchange for lease-use of those new parking spaces. Attorney Orr has opined that a

long-term lease of these new parking spaces is possible, but it would require future City Council action. In this scenario, the City would likely be paving the spots with in-house equipment and staff, and the \$2,000 cost to the developer will cover the City's supply / hard-costs of the installation.

Section 8 – Parking issues

- Sections 8a and 8b dictate that there is ample public parking in the immediate area surrounding the property, and that the parking requirements under the zoning code for the developers most intense possible use under the zoning code are satisfied by the existing public parking. The developer is very concerned that future actions by the City and the County to sell off the public parking will result in adverse actions against the development should they change their business use. Section 8B(iv) clarifies that no further parking requirements will be placed against the property unless the property is rezoned by the developer.

Recommendation

Staff requests feedback on the above terms and recommends approval of an agreement in accordance with the term sheet. Pending positive feedback from the committee, we are optimistic that a final agreement could be ready for City Council consideration on August 13th.

TERM SHEET

United City of Yorkville
111 West Madison Street
Old Kendall County Jail

1. ***Developer:***

Peter McKnight and Cary Coles, a Limited Liability Company of Illinois to be incorporated for this project
659 North Carpenter
Chicago, Illinois 60642

2. ***Property:***

111 West Madison Street
Yorkville, Illinois 60560, exclusive of the parking lot.

Legal Description:

Lots 1 and 2 in Block 28 except any part taken for the people of the State of Illinois, of the original Village of Yorkville, Kendall County, Illinois

Purchase Price: \$1,000.00

3. ***Property Encumbrances:***

A. The Deed of conveyance to the Developer shall be subject to the Right of First Refusal in favor of the City in the event of any subsequent conveyance, sale, transfer, gift or exchange of the Property by the Developer until final completion of the project and the City has issued certificates of occupancy for the residential units and the commercial area.

B. The City shall grant the Developer the Right of First Refusal in the event of the conveyance, sale, transfer, gift or exchange of Lots 3 and 4 of Block 28 (the parking lot) for a term of twenty (20) years.

C. The Deed of conveyance shall be subject to a covenant running with the land that the historic portion (i.e. jail, not the garage) of the building may never demolished by any private owner, without consent of the City Council.

4. ***Proposed Project.***

Development of a Mixed-Use Property

Options include the following:

- Five (5) residential Beautiful Modern Market Rate Units (in two-story structure/old garage building)
 - The property is located within the B-1 zoning district. The current zoning ordinance does allow multi-family residential as a permitted use, however only above the first floor in a building and no more than two (2) units. A rezoning of the parcel to an R-3 or R-4 zoning designation would be required for apartment style residential use on both lower and upper floors.
 - The proposed Yorkville Form-Based Code (FBC) permits all residential dwellings as a permitted use under the “B Street” District. If the FBC is approved, no additional City Council approvals for apartment style dwellings are required.

- Plus 2,000 square foot (*of finished*) Commercial Space (up to 4,240 square foot) Hospitality/Retail/Office space
 - The property is located within the B-1 zoning district. The current zoning ordinance does allow for mixed use developments with commercial and/or office.

- The current permitted uses under zoning code and district are as listed on *Exhibit A* attached hereto.

5. **Project Vision**

Our plan is to work with the state, county and city government stakeholders as well as the local community and not for profits to repair, remediate, restore the Kendall County Jail site to an income producing property while adhering to the preservation and retention of the relevant historic elements of the building.

6. **Project Budget**

Pre-Construction/Emergency (1st Phase)

Immediate Protection of Roof (tarping)	\$ 7,410
Site Protection (grounds/building) (cameras/alarms/safety)	\$ 1,800
Erect Construction fencing & baracades	\$ 3,500
Portable site sanitation (rental)	\$ 2,400
<i>Sub-Total</i>	\$15,110

Environmental Remediation (1st Phase)

Asbestos	\$30,000
Lead Based Paint	\$16,400
<i>Sub-Total</i>	\$46,400

EXTERIOR PHASE

Miscellaneous Exterior

Demolition (general & interiors)		\$ 18,000
Sallyport Demo	(1 st Phase)	\$ 15,000
New Roof	(1 st Phase)	\$ 30,000
Fascia, Downspouts, Misc.		\$ 8,000
Masonry Repairs		\$ 54,319
New Windows		\$ 40,000
Exterior Doors		\$ 11,000
Exterior Concrete Work		\$ 16,000
Misc. Structural Repairs		\$ 5,500
Landscaping		\$ 6,000
	<i>Sub-Total</i>	\$203,810

INTERIOR PHASE

Mechanical Phase

Electrical Service Updates		\$ 30,000
Commercial Space Electrical		\$ 15,000
HVAC (residential portion)		\$ 30,000
HVAC (commercial space)		\$ 25,000
	<i>Sub-Total</i>	\$100,000

Plumbing

Residential		\$35,000
Commercial		\$30,000
New H2O Service		\$10,000
	<i>Sub-Total</i>	\$75,000

Interior Finish Phase

Rough Carpentry		\$ 10,000
Finish Carpentry		\$ 10,000
Finish Plumbing		\$ 10,000
Finish Electrical		\$ 4,000
Flooring		\$ 30,000
Drywall		\$ 50,000
Plaster Restoration		\$ 8,000

Historic wood work restoration (historic porches)	\$ 16,000
Insulation	\$ 10,000
Paint	\$ 7,000
Appliances (Fridge/range/mwave-hood/DW/WD)	\$ 12,500
Lighting	\$ 6,000
<i>Sub-Total</i>	\$173,500

Miscellaneous

Porches (new-in residential section)	\$30,000
Contingency	\$40,000
<i>Sub-Total</i>	\$70,000

Fees and Soft Costs

General Contractors Fees	\$ 49,425
General Conditions	\$ 15,000
Insurance (during construction phase)	\$ 8,000
Utilities (during construction phase)	\$ 2,200
Architectural fees	\$ 35,000
Engineering fees	\$ 5,000
Permit fee	\$ 3,000
Loan fees (bank)	\$ 5,000
Interest Carry (construction period)	\$ 21,000
Legal Fees	\$ 15,000
<i>Sub-Total</i>	\$158,625

TOTAL ESTIMATED CONSTRUCTION BUDGET \$842,445

7. TIF Assistance

- A. City will pay developer \$30,000 in TIF incentive after:
- i. Completion of new roof installation in accordance with applicable code requirements on or before December 1, 2019 or 90 days after permit issuance, whichever comes later.
 - ii. Payment shall be made within 30 days of satisfaction of (i) above.

AND

- B. City will pay developer the lesser of: (i) the cost of abatement of asbestos materials and remediation of lead-based paint; or, (ii) \$50,000 in TIF incentive after

abatement of asbestos materials and remediation of lead-based paint has been completed in accordance with all applicable building codes.

AND

- C. City will pay developer \$17,500 in TIF incentive upon issuance of certificates of occupancy for all of the residential units in the building.

AND

- D. City will pay developer \$17,500 in TIF incentive after completion of the construction of all of the commercial space in the building in accordance with all applicable building codes (no certificate of occupancy required until tenant is prepared to occupy).

AND

- E. The City shall rebate 100% of all incremental property taxes on the building:
 - i. During the construction of the building; AND
 - ii. For two tax years after completion of the building permit; AND
 - iii. Subject to there being no property maintenance or building code violations on the property; AND
 - iv. Subject to there being no registered students living on the property, it being understood that if there are students, the amount required to be set aside for the school district distribution shall be taken out of the entire property tax bill, and the TIF rebate shall be 100% of the remaining net amount.

AND

- F. After item E is completed, the City shall rebate the following percentages of incremental property taxes on the building:
 - i. 90% for a period of 4 years AND THEN
 - ii. 85% for a period of 3 years AND THEN
 - iii. 80% for a period of 3 years OR
 - iv. Whenever these item F payouts reach an actual value of \$150,000 (i.e. whichever happens first)
 - v. Subject to there being no registered students living on the property it being understood that if there are students, the amount required to be set aside for the school district distribution shall be taken out of the entire property tax bill, and the TIF rebates shall be 90/85/80% of the remaining net amount

AND

G. All of the above is subject to verification that all payouts to the developers are never cumulatively greater than the amount of TIF eligible expenditures on the property.

AND

H. The City will waive all building permit fees (including plan review fees), water meter fees, water connection fees, and sewer connection fees for any projects on any portion of the building commenced within 5 years from the date of approval of the agreement, and for any projects on the historic (eastern half) of the building commenced within 10 years from the date of approval of the agreement.

I. The City will complete the water and sewer service line extensions into the building

i. As of July 2019, this is contemplated at two 1" water service lines and a 6" sanitary service line, and a maximum construction cost estimate of \$15,000.

J. The City will construct five residential service electrical services into the building

i. As of July 2019, this is contemplated at a maximum construction cost estimate of \$5,000.

K. The City will utilize its best efforts to extend the nose-in diagonal on-street parking adjacent to the property, to the east of the current parking spots to a point yet to be determined near the property line shared with Kendall County, or onto the Kendall County property or adjacent public right-of-way. For each additional parking spot created, the developer's TIF incentive shall be reduced by \$2,000, up to a maximum of \$10,000, to be deducted from the payouts as contemplated in Section 7E above. Parking spots for which the developer's TIF incentive is reduced may be leased for the term of the final redevelopment agreement but only if three-fourths (3/4) of the City Council approve such lease.

8. ***Outstanding Issue, Parking:***

A. Parking lot to the north

(i) Seventeen (17) off-street public parking spaces are currently located north of the existing structure (parking area will be subdivided as a separate parcel upon sale), plus an additional (3) on-street public parking spaces to the south (Madison Street) are immediately available to property. An additional seven (7) striped on-street public parking stalls northeast of the property on W. Van Emmon Street are available, as well as a County-owned off-street public parking lot east of the property on W. Madison Street holding approximately thirteen (13) stalls are available. Total available public parking stalls within 200 feet are forty (40) stalls.

- (ii) Minimum required parking spaces for the commercial and office uses is three (3) spaces per 1,000 of floor area; one (1) space per lodging unit for the hotel/hospitality use; and two (2) spaces per dwelling unit.

B. Parking requirements for proposed use under current zoning codes

- i. Based upon the proposal, under the current zoning ordinance an estimated maximum twenty-three (23) parking stalls (ten (10) stalls for residential and thirteen (13) for all non-residential uses) would be required.
- ii. Per the City's Zoning Ordinance, all required parking spaces are to be located within 1,000 feet of the use served, with the exception for residential or hotel use which should be within 300 feet.
- iii. The adjacent off-street parking lot to the north of the building can be utilized to fulfil the site's residential parking needs (ten (10) spaces). The non-residential parking needs can be fulfilled by the other identified off-street and on-street parking areas within 200 feet of the property.
- iv. The maximum number of parking slots as stated in (i) above, shall not be increased for so long as the property retains its current zoning. If the property is rezoned at any time, the parking requirements under the zoning code as then in effect shall apply.

9. ***Term of Redevelopment Agreement.***

- a. December 31, 2041.

Exhibit A

Permitted Uses in B-1 Zone

- Multi-family dwelling above first for business or live work space with maximum of two apartments
- College, junior college
- Library
- Religious institution
- Advertising Agency
- Antique Sales
- Bakery
- Bank
- Beauty/Barber Shop
- Bookkeeping Services
- Boat Sales
- Bookstore
- Clothes-Pressing and Repair
- Private Club
- Coffee Shop
- Commercial Laboratory
- Trade school
- Detective Agency
- Dressmaker
- Dry Cleaning
- Employment Office
- Funeral Home
- Grocery Store
- Liquor Store
- Massage Establishment
- Medical Clinic
- Microbrewery
- Pawnbrokers
- Photography
- Post Office
- Professional Services
- Radio and Television Studio
- Recreation Center
- Resale Dealers
- Restaurant
- Retail Store
- Shoe Repair
- Tattoo Establishment
- Treatment Center

Downtown TIF Fund II (89)

The Downtown TIF II was created in 2018, in order to help promote downtown redevelopment and support the existing Downtown TIF.

	<u>Unaudited</u>															
	FY 2019	FY 2020	FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	FY 2027	FY 2028	FY 2029	FY 2030	FY 2031	FY 2032	FY 2033	FY 2034
	Actual	Projected	Projected	Projected	Projected	Projected	Projected	Projected	Projected	Projected	Projected	Projected	Projected	Projected	Projected	Projected
Revenue																
Taxes	-	24,597	25,000	33,000	33,000	39,000	39,560	40,142	40,748	41,738	42,033	42,714	43,423	44,160	44,926	45,723
Total Revenue	-	24,597	25,000	33,000	33,000	39,000	39,560	40,142	40,748	41,738	42,033	42,714	43,423	44,160	44,926	45,723
Expenditures																
Contractual Services	2,575	85,000	22,500	30,500	13,000	19,000	18,104	18,628	19,173	19,740	19,478	20,057	20,660	20,328	20,941	21,579
Capital Outlay	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Total Expenditures	2,575	85,000	22,500	30,500	13,000	19,000	18,104	18,628	19,173	19,740	19,478	20,057	20,660	20,328	20,941	21,579
Surplus (Deficit)	(2,575)	(60,403)	2,500	2,500	20,000	20,000	21,456	21,514	21,575	21,998	22,555	22,657	22,763	23,832	23,985	24,144
Ending Fund Balance	(2,575)	(62,978)	(60,478)	(57,978)	(37,978)	(17,978)	3,478	24,992	46,567	68,565	91,120	113,777	136,540	160,372	184,357	208,501



		<u>Unaudited</u>															
		FY 2019	FY 2020	FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	FY 2027	FY 2028	FY 2029	FY 2030	FY 2031	FY 2032	FY 2033	FY 2034
Account Number	Description	Actual	Projected														
<u>Downtown TIF II</u>																	
89-000-40-00-4000	PROPERTY TAXES	-	24,597	25,000	33,000	33,000	39,000	39,560	40,142	40,748	41,738	42,033	42,714	43,423	44,160	44,926	45,723
	Revenue	-	24,597	25,000	33,000	33,000	39,000	39,560	40,142	40,748	41,738	42,033	42,714	43,423	44,160	44,926	45,723
89-890-54-00-5401	ADMINISTRATIVE CHARGEBACK	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
89-890-54-00-5425	TIF INCENTIVE PAYOUT	-	80,000	17,500	25,500	8,000	14,000	13,104	13,628	14,173	14,740	14,478	15,057	15,660	15,328	15,941	16,579
89-890-54-00-5462	PROFESSIONAL SERVICES	2,575	5,000	5,000	5,000	5,000	5,000	5,000	5,000	5,000	5,000	5,000	5,000	5,000	5,000	5,000	5,000
89-890-54-00-5470	FACADE REHAB PROGRAM	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
89-890-60-00-6000	PROJECT COSTS	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
	Expenditures	2,575	85,000	22,500	30,500	13,000	19,000	18,104	18,628	19,173	19,740	19,478	20,057	20,660	20,328	20,941	21,579
	Surplus(Deficit)	(2,575)	(60,403)	2,500	2,500	20,000	20,000	21,456	21,514	21,575	21,998	22,555	22,657	22,763	23,832	23,985	24,144
	Fund Balance	(2,575)	(62,978)	(60,478)	(57,978)	(37,978)	(17,978)	3,478	24,992	46,567	68,565	91,120	113,777	136,540	160,372	184,357	208,501



KCJ Restoration LLC

Old Kendall County Jail Preservation Plan

Adaptive Utilization Of Property

Restoration & Conversion to Mixed Use Property.

Options include the following:

- **5-Residential Beautiful Modern Market Rate Units (*in 2-story structure/old garage building*)**
- **Plus 2,000 Sq Ft* (of finished) Commercial Space (up to 4240 Sq ft). Hospitality/Retail/Office)**

Additional considerations:

- **Single Use for entire property for hospitality concept**
- **Modern Boutique Hotel leveraging historic significance of property.**

Elevations & Renderings



General Notes

DRAW BY	TZ	
No.	Revision/Issue	Date

Print Name and Address

Project Name and Address
APARTMENT
HISTORIC COUNTY JAIL
188 W MADISON ST
YORKVILLE, IL 62560

Project	Sheet
APARTMENT	3
Date	5/24/2019
Scale	AS SHOWS

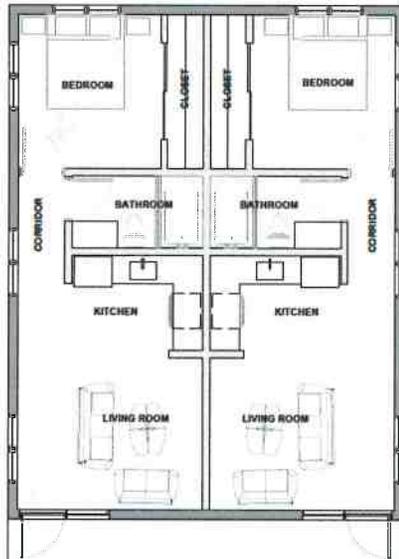
Facade Elevations

Current & Future Benefits of KCJ's Plan:

- **Maintaining all relevant historic elements of the old jail.**
- **Maximum possible property tax revenue for United City of Yorkville(UCY).**
- **Corporate Ownership of subject property.**
- **KCJ Restoration LLC legal structure guarantees that this property will be placed back on the United City of Yorkville's tax role.**
- **KCJ Restoration LLC plan calls for market rate pricing for the new residential & commercial units**
- **Over \$12,000 per year in forecasted new property tax revenue for United City of Yorkville.**
 - *Based on a new assessed valuation of like/kind properties*
- **Job creation via new commercial tenancy**
- **Additional reoccurring tax revenues for UCY via:**
 - **Sales taxes & business license fees,**
 - **Hospitality taxes,**
 - **Occupancy taxes,**
 - **Permit fees.**

MASTER PLAN

New residential Units



HISTORIC JAIL HOUSE

Sally port removal



AREA TO BE DEMOLISHED

MASTER PLAN

General Notes

DRAW BY	TZ	
No.	Revision/Issue	Date

For Name and Address

Project Name and Address
APARTMENT,
HISTORIC COUNTY JAIL
100 W MADISON ST
YORKVILLE, IL 60550

Project	Sheet
APARTMENT	1
Date	5/24/2019
Scale	AS SHOWN

Elevations & Renderings



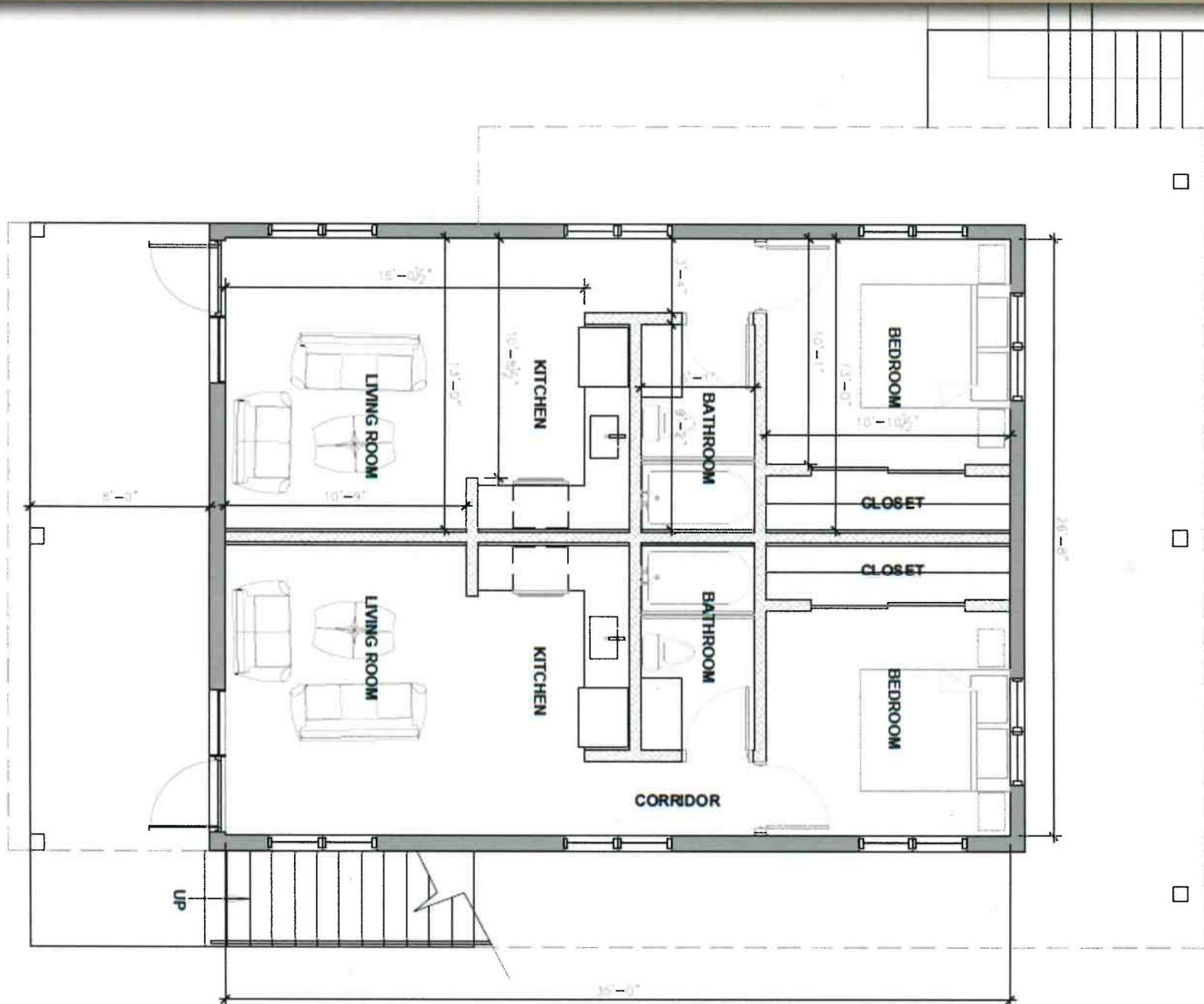
Overhead View of New Residential Unit

Elevations & Renderings



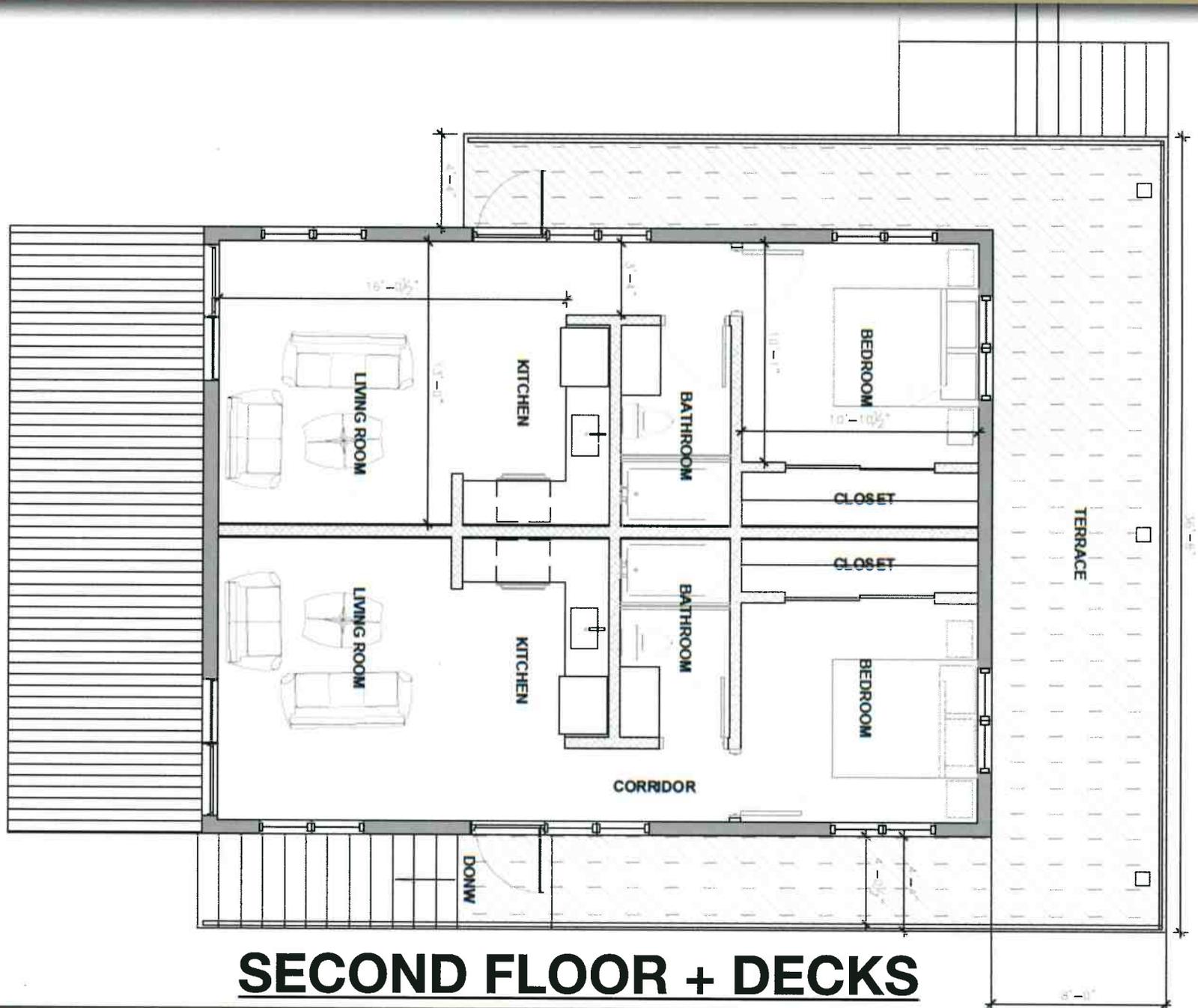
Overhead View of new residential units

Residential Section - Floor Plans

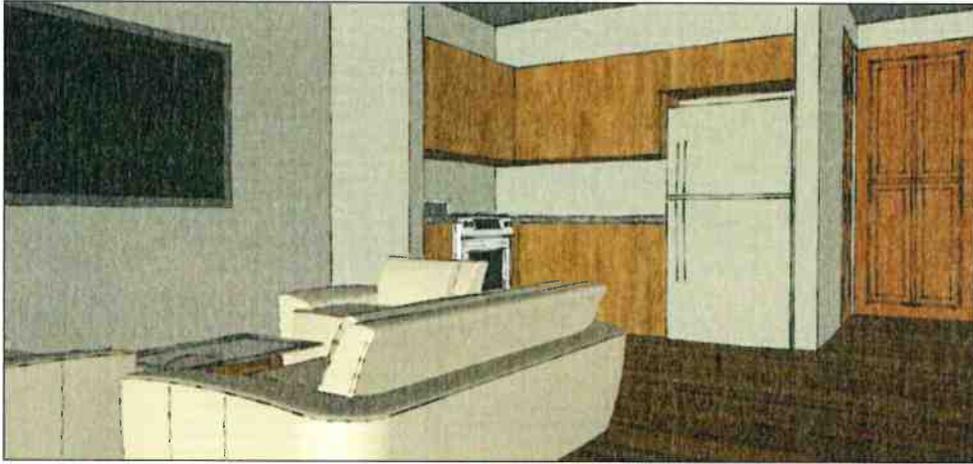


GROUND FLOOR

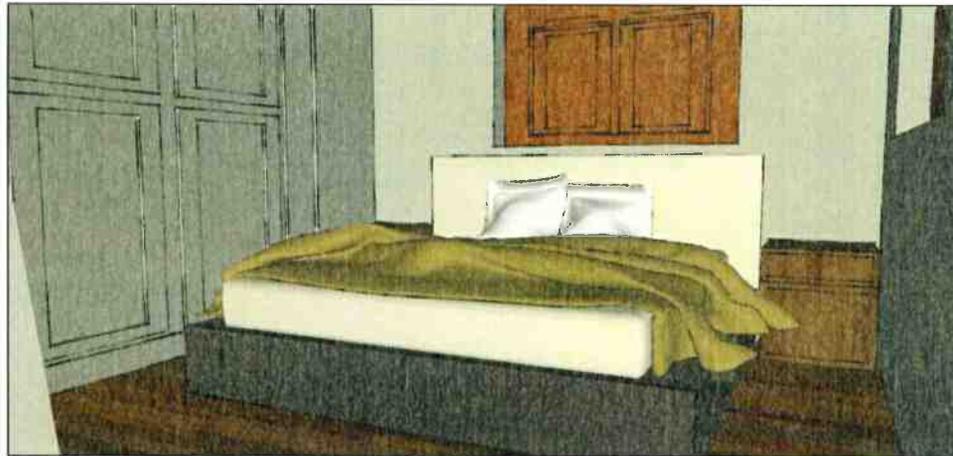
Residential Section - Floor Plans



Elevations & Renderings



LIVING ROOM / KITCHEN



BEDROOM

General Notes

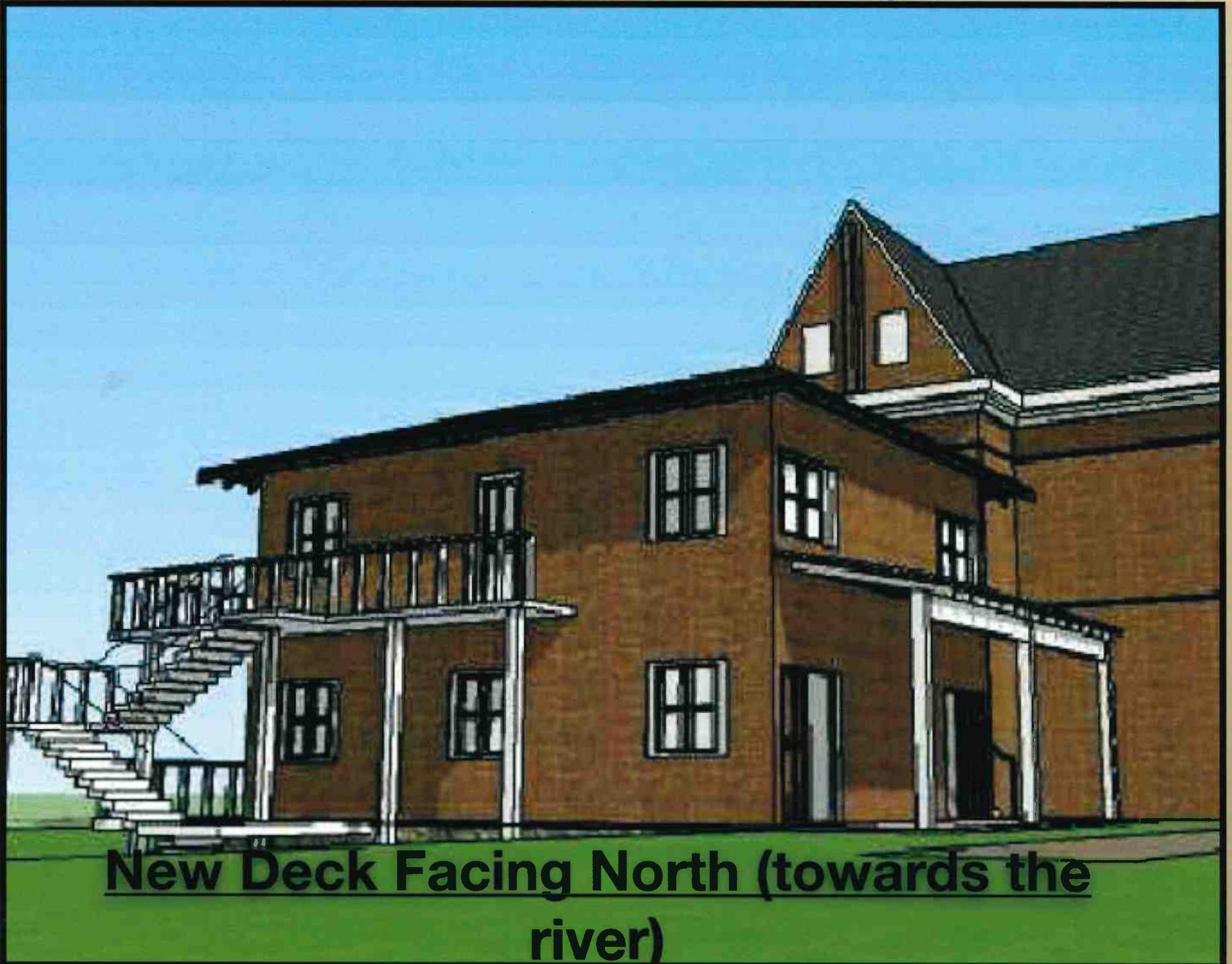
DRAW BY	TZ	
No.	Revision/Issue	Date

Plot Name and Address

Project Name and Address
 APARTMENT
 HISTORIC COUNTY JAIL
 199 W MADISON ST
 YORKVILLE, IL 60590

Project	APARTMENT	Sheet	5
Date	5/24/2019		
Scale	AS SHOWS		5

Elevations & Renderings



New Deck Facing North (towards the river)

Elevations & Renderings



New Deck Facing NE (towards the river)

Elevations & Renderings



Front Exterior Residential (Annex Bldg)

KCJ Restoration LLC Team Core Competencies

- **Proven track record of commercial & residential development**
- **Decades of property management expertise**
- **Extensive experience in historical renovations**
- **50+ years of construction experience**
- **Over 300 units of residential and commercial construction projects completed**
- **Dedicated commitment to community development**

Financial Requirements

- **Cost to rehab and purchase building: \$842,445.00**
- **Projected Gross Monthly income: \$6,600.00**
- **Estimated RE Tax: \$1,200.00**
- **Insurance: \$350.00**
- **Maintenance: \$900.00**
- **Break Even Maximum Revenue \$4,150.00**
- **Estimated lending value 75%(maximum loan potential)**
 - **New loan Amount \$450,000.00**
- **Monthly Mortgage Payment. \$3,038.00**
- **Annual NOI (after debt service) \$12,780.00**
- **-5% Return on cash (assumes \$200k investment)**

KCJ Restoration LLC - Construction Budget

Kendall Co. Jail Renovations Budget (Revised 6-25-2019)

Pre-Construction / Emergency (1st Phase)				Sub Total	Interior Finish Phase				
Immediate protection of roof (tarping)			\$7,410	\$15,110	Rough Carpentry			\$10,000	\$173,500
Site Protection (grounds & building) (cameras, Alarms, Safety)			\$1,800		Finish Carpentry			\$10,000	
Erect Construction fencing & barricades			\$3,500		Finish Plumbing			\$10,000	
Portable site sanitation (rental)			\$2,400		Finish Electrical			\$4,000	
Environmental Remediation (1st Phase)				\$46,400	Flooring			\$30,000	
Asbestos			\$30,000		Drywall			\$50,000	
Lead Based Paint			\$16,400		Plaster Restoration			\$8,000	
EXTERIOR PHASE					Historic wood work restoration (historic porches)			\$16,000	
Misc Exterior					Insulation			\$10,000	
Demolition (general & interiors)			\$18,000	Paint			\$7,000		
Sallyport Demo		(1st Phase)	\$15,000	Appliances (fridge/Range/ mwave-hood/DW/WD/			\$12,500		
New Roof		(1st Phase)	\$30,000	Lighting			\$6,000		
Fascia, Downspouts, Misc			\$8,000	Misc					
Masonry Repairs			\$54,310	Porches (New-in residential section)			\$30,000	\$70,000	
New Windows			\$40,000	Contingency			\$40,000		
Exterior Doors			\$11,000	Fees & Soft Costs:					
Exterior Concrete Work			\$16,000	General Contractors Fees			\$49,425	\$158,625	
Misc Structural Repairs			\$5,500	General Conditions			\$15,000		
Landscaping			\$6,000	Insurance (during construction phase)			\$8,000		
INTERIOR PHASE					Utilities (during construction phase)				\$2,200
Mechanical Phase					Architectural fees				\$35,000
Electrical Service Updates			\$30,000	Engineering fees			\$5,000		
Commercial Space Electrical			\$15,000	Permit Fee			\$3,000		
HVAC (residential portion)			\$30,000	Loan Fees (bank)			\$5,000		
HVAC (commercial space)			\$25,000	Interest Carry (construction period)			\$21,000		
				Legal Fees			\$15,000		
Plumbing					TOTAL ESTIMATED CONSTRUCTION BUDGET				
Residential			\$35,000	\$842,445					
Commercial			\$30,000						
New H2O Service			\$10,000						

KCJ Restoration LLC

Source And Use of Funds

- Total estimated project cost: 842,445.00
- Municipal contribution:
 - 1st Existing demolition funds: 78,000.00
 - 2nd Tiff Funds 228,000.00
- EST Funds From Lender 380,000.00
- EST Funds from KCJ (cash) 156,445.00
- TOTAL FUNDS 842,445.00
- Tax Credits (TBD) Per Annum 12,000.00
- Term TBD

KCJ Restoration LLC

Financial Requirements -1st Disbursement

- Roofing completion: 30,000.00
- Asbestos and LB-Paint encapsulation 46,600.00
- Demolition of Sally Port 15,000.00
- Site preparation, fencing, site-security 15,110.00
- Total prior to initial disbursement: (106,710.00)



Reviewed By:	
Legal	<input checked="" type="checkbox"/>
Finance	<input type="checkbox"/>
Engineer	<input type="checkbox"/>
City Administrator	<input checked="" type="checkbox"/>
Human Resources	<input type="checkbox"/>
Community Development	<input checked="" type="checkbox"/>
Police	<input type="checkbox"/>
Public Works	<input type="checkbox"/>
Parks and Recreation	<input type="checkbox"/>

Agenda Item Number

Old Business #1

Tracking Number

EDC 2019-23

Agenda Item Summary Memo

Title: Downtown Form Based Code and Master Streetscape Plan

Meeting and Date: Economic Development Committee – August 6, 2019

Synopsis: Proposed alternative catenary lighting for the downtown overlay district as presented in the Streetscape Masterplan and Form-Based Code documents.

Council Action Previously Taken:

Date of Action: 7-2-19 Action Taken: Additional Revisions per Committee

Item Number: _____

Type of Vote Required: Vote

Council Action Requested: Feedback

Submitted by: Krysti J. Barksdale-Noble, AICP

Name

Community Development

Department

Agenda Item Notes:

See attached memo.



Memorandum

To: Economic Development Department
From: Krysti J. Barksdale-Noble, Community Development Director
CC: Bart Olson, City Administrator
Jason Engberg, Senior Planner
Date: July 16, 2019
Subject: **Downtown Form Based Code and Streetscape Master Plan**
Alternative Catenary Lighting

SUMMARY:

Review and discussion of the proposed alternative overhead catenary lighting in the downtown as part of the Yorkville Downtown Overlay District Plan consisting of a Streetscape Master Plan and Form-Based Code.

BACKGROUND:

As you may recall, the final draft of the Yorkville Downtown Overlay District Plan was presented at the May 14, 2019 City Council meeting for its first reading prior to adoption. A presentation of the highlights of the Streetscape Master Plan and the Form-Based Code for the overlay district was given at that meeting by the consultant, Doug Farr of Farr & Associates.

The plans are intended to establish the broader guidance for street character and development opportunities within the downtown which can be facilitated through public capital improvement projects and private redevelopment.

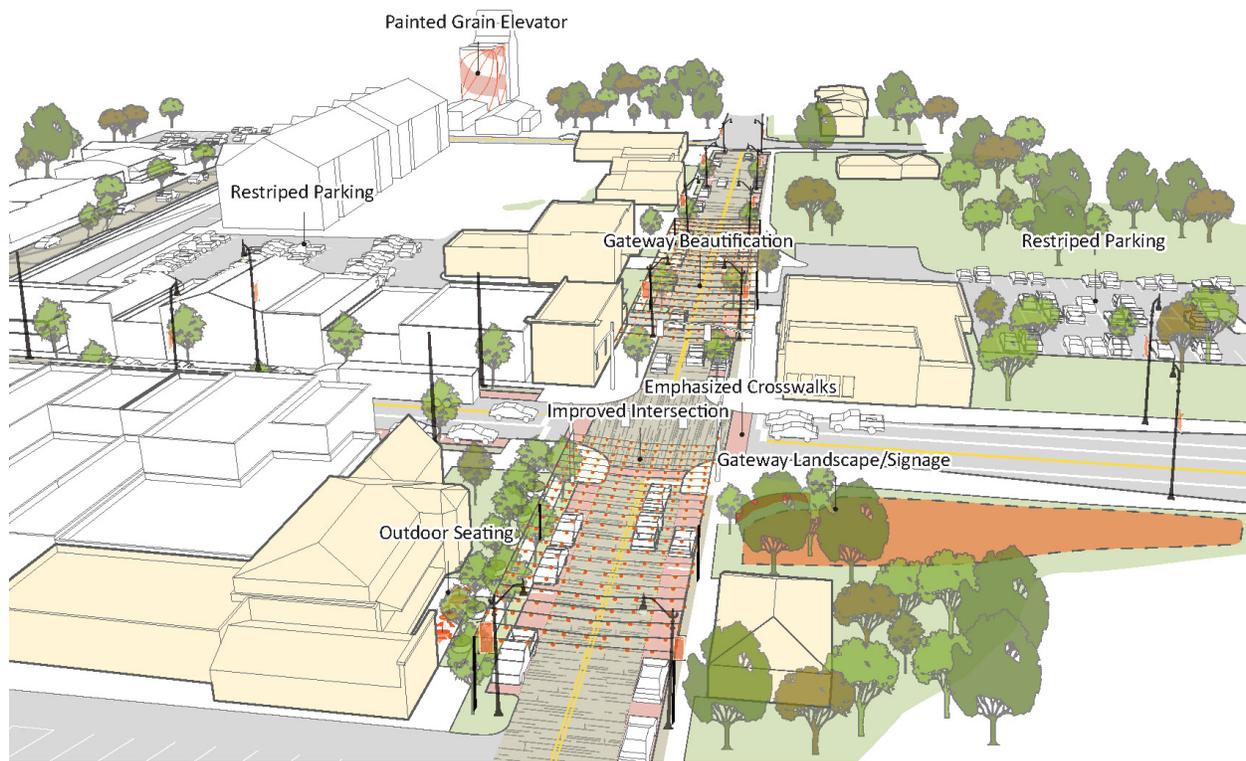
Over the course of 18-months, beginning in January 2018, the consultant and staff hosted a project website; conducted two (2) public workshops; previewed a draft plan at a committee meeting; held a public open house; and had a public hearing before the Planning and Zoning Commission; all to gather input and solicit feedback on the various drafts of the plan. All feedback received during this time was incorporated into the plan. A formal recommendation for adoption was made by the Planning and Zoning Commission on April 10, 2019.

At the June 4, 2019 Economic Development Committee meeting, staff received comments regarding certain aspects of the Streetscape Masterplan, specifically the curb treatment for 'B' Street Residential areas and the proposed overhead catenary lighting on Van Emmon Street. The curb treatment and subsequent edits recommended by Alderman Funkhouser were addressed by staff at the July 2nd meeting.

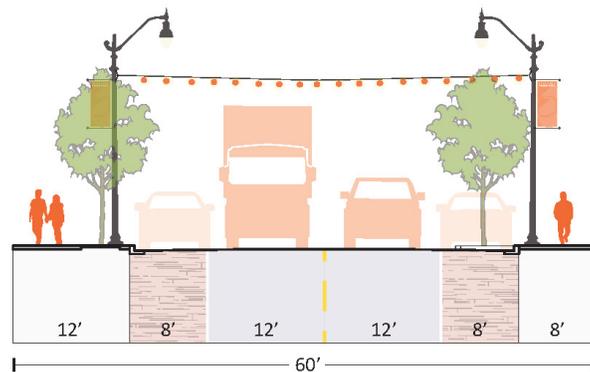
Staff is now addressing the direction given to explore alternative light fixture options to the catenary overhead lights on Van Emmon Street and bring back to the committee for further discussion.

ORIGINALLY PROPOSED PLAN:

In the originally proposed Streetscape Master Plan, overhead catenary lighting was recommended on Van Emmon Street (east and west) as a gateway and atmospheric feature within the downtown, as illustrated on the following page:



Van Emmon Street (Long-Term)



Purpose/Benefit

The introduction of lighting in any streetscape design is to define the public space. In large open public spaces such as Van Emmon Street, lighting from a suspended catenary system can create a passive transition between environments while visually identifying an intended place for gathering. The ambiance created by the cascading light “roof” maximizes the usable footprint of a public area for pedestrians beyond the sidewalk and heightens the awareness of drivers in this shared space. The increased illumination encourages night time usage of the downtown and adds another layer of security.

Design/Location

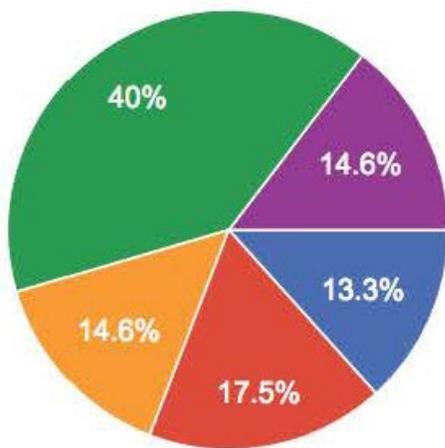
As proposed, the catenary system would have a linear grid light pattern attached to decorative poles suspended between 20 feet to 24 feet above Van Emmon Street and supported with concrete foundations. The lights, individual styled “Edison bulbs”, would extend mid-block on either side of East and West Van Emmon but will not be located within the IL Route 47 (S Bridge Street) right-of-way. The following images depict the originally proposed lighting system with views on both sides of Van Emmon Street:



Survey Results

As part of the public outreach conducted by the consultant in preparing the Yorkville Downtown Overlay District's Streetscape Master Plan, a web survey was implemented to garner feedback from citizens on design elements for the downtown. The survey was taken by nearly 500 respondents. When specifically asked "Which is a preferred gateway element into downtown?", approximately 40% responded that overhead lights should be utilized.

Which is a preferred gateway element into downtown?



- a) Overhead signage
- b) Architectural symmetry
- c) Bridge over Bridge Street
- d) Overhead lights
- e) Unsure or Other

PROPOSED ALTERNATIVES:

During the June and July EDC discussions regarding the overhead lights, staff received mixed commentary and questions on if the catenary lights should be kept in the plan, the cost and future maintenance and we were also provided an alternative light fixture option by Alderman Funkhouser. Staff was further tasked with providing the committee with three (3) alternative light fixture options for formal consideration.

Below are four (4) alternative light fixtures proposed as alternatives to the “Edison Bulb” style depicted in the original proposal:

#1 Single Cable Pendant-Style Lights



- Simplest and most cost efficient with regards to number of light standards needed.
- Recommended for short distances and typically used in alley ways or suspended between buildings or poles where a discrete or delicate lighting effect is desired.

#2 Random Central Ring Globe Lights



- Ideal for large open areas where connections may be spaced irregularly between buildings and possibly poles.
- Depending on the spacing and number of light standards, the random positioning of the lighting can create a celestial or a luminous canopy effect.

#3 Linear Grid Bell-Shade Lights



- The uniformed and geometric net-like system can be used for both small and large spaces which provides greater flexibility when incorporating temporary or seasonal decorations.
- The lighting effect provides the most coverage and the structural form of the net creates a transparent “ceiling” informally defining the space.

#4 Central Spine Singular Dome Lights



- Utilizes one principal lighting or architectural element to form an elegant appearance.
- Lighting effect is pinpoint, offering a “halo” glow at regular intervals.

COST & MAINTENANCE:

The city’s engineering consultant, Engineering Enterprises Inc. (EEI), provided a preliminary estimate of the costs to design and install the catenary lighting system. Per the originally proposed design, the total preliminary estimated cost for engineering and installation on both East and West Van Emmon Street would be approximately \$240,000. Below is a detail of those early estimates:

DOWNTOWN CATENARY LIGHTING - PRELIMINARY COST ESTIMATES

WEST VAN EMMON					EAST VAN EMMON				
<i>Item</i>	<i>Unit</i>	<i>Quantity</i>	<i>Unit Price</i>	<i>Amount</i>	<i>Item</i>	<i>Unit</i>	<i>Quantity</i>	<i>Unit Price</i>	<i>Amount</i>
Decorative Poles	EA	10	\$3,000	\$30,000	Decorative Poles	EA	12	\$3,000	\$36,000
Lighting Cable & Fixtures	FT	1,200	\$25	\$30,000	Lighting Cable & Fixtures	FT	1,450	\$25	\$36,250
Concrete Foundations	FT	70	\$250	\$17,500	Concrete Foundations	FT	84	\$250	\$21,000
Electrical Service	EA	1	\$5,000	\$5,000	Electrical Service	EA	1	\$5,000	\$5,000
Subtotal				\$82,500	Subtotal				\$98,250
Contingency (15%)				\$12,400	Contingency (15%)				\$14,800
Total				\$94,900	Total				\$113,050
Engineering (15%)				\$14,300	Engineering (15%)				\$17,000
TOTAL PRELIMINARY ESTIMATE				\$109,200	TOTAL PRELIMINARY ESTIMATE				\$130,050

The highest percentage of costs associated with the catenary light system are the decorative poles and length of lighting cable and fixtures. These estimates can be adjusted to use less expensive or fewer number of poles in addition to shortening the run of cable or limiting the number of fixtures.

With regard to future maintenance of the catenary lights, according to architectural and structural engineers who design and install such systems, they should require very little upkeep as long as careful consideration of access to the power supply in the design stage is taken. Additionally, maintenance may also include the seasonal and themed decoration changes and the washing and replacement of light fixtures.

STAFF COMMENTS/REQUESTED DIRECTION:

Staff believes the information provided in this memo outlines the comments and feedback received at the June and July Economic Development Committee meetings regarding the proposed catenary lights. Therefore, we are seeking firm direction and consensus from the committee regarding the following:

1. To keep or remove the overhead catenary lighting feature from the Streetscape Master Plan.
2. If to keep, specific catenary lighting fixture preference.

We welcome any additional feedback you may have on the information provided and will be available to answer any questions.

ATTACHMENTS:

1. Draft Revised Yorkville Streetscape Master Plan
2. Draft Revised Yorkville Downtown Form-Based Code

YORKVILLE

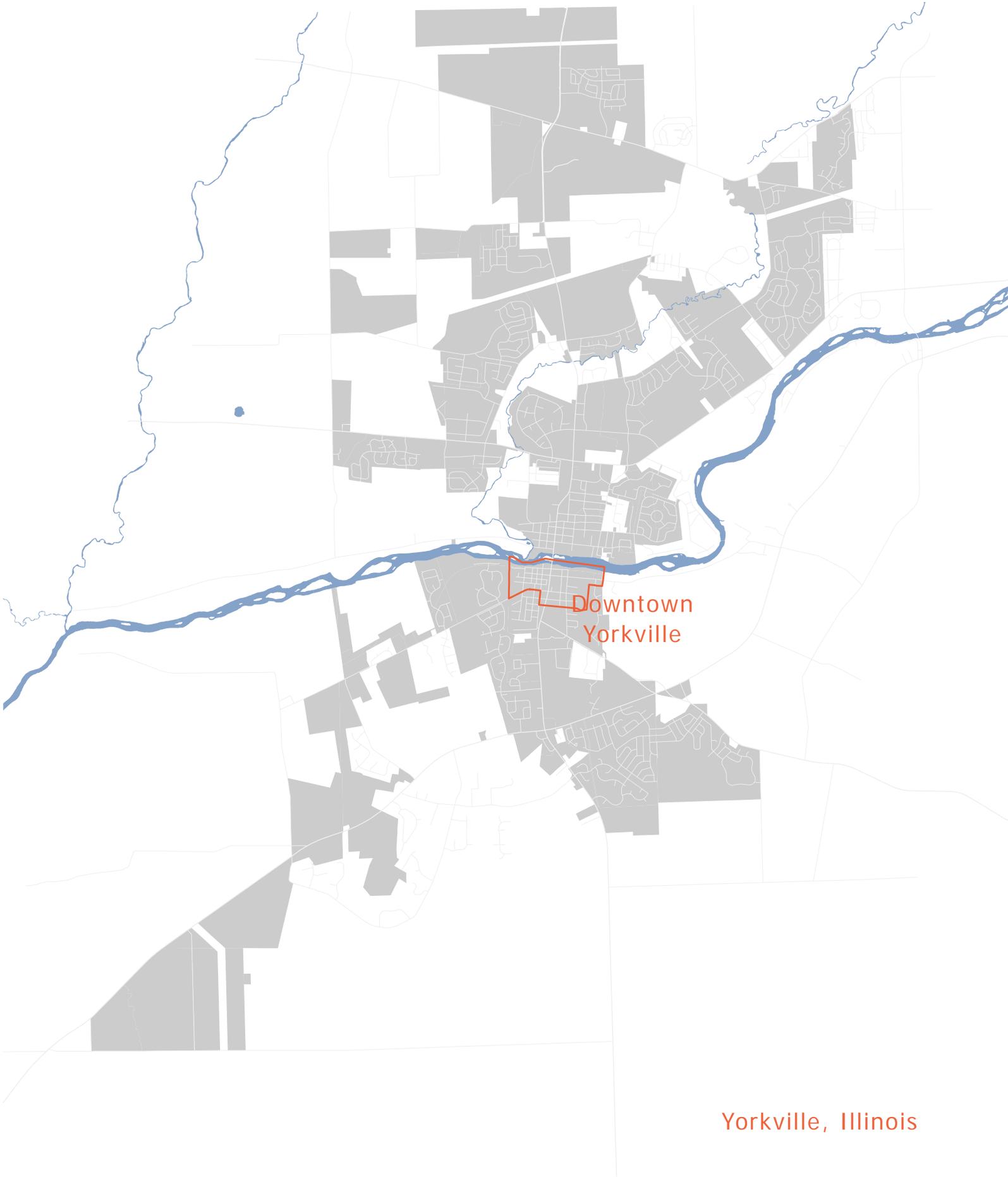
Downtown Overlay District

Streetscape Master Plan



United City of
Yorkville ILLINOIS

FARR ASSOCIATES



Yorkville, Illinois

Table of Contents

Introduction	p. 04
Streetscape Master Plan	p. 06
Bridge Street	p. 10
Hydraulic Street	p. 18
Van Emmon Street	p. 26
'B' Street	p. 34
Appendix	p. 52

Acknowledgements

United City of Yorkville
Farr Associates
Former Mayor Gary J. Golinski
Bart Olson, *City Administrator*
Erin Willrett, *Assistant City Administrator*
Krysti Barksdale-Noble, *Community Development Director*
Jason Engberg, *Senior Planner*
Eric Dhuse, *Public Works Director*
Lisa Pickering, *City Clerk*
Richard T. Hart, *Chief of Police*
Tim Evans, *Director of Parks and Recreation*
Brad Sanderson, *EEl, Engineering Consultant*

City Council
Mayor John Purcell
Joel Frieders, *Alderman*
Chris Funkhouser, *Alderman*
Ken Koch, *Alderman*
Jacquelyn Milschewski, *Alderman*
Jason Peterson, *Alderman*
Arden Joe Plocher, *Alderman*
Seaver Tarulis, *Alderman*
Daniel Transier, *Alderman*

Planning and Zoning Commissioners
Randy Harker, *Chairman*
Reagan Goins, *Commissioner*
Debra Horaz, *Commissioner*
Don Marcum, *Commissioner*
Jeff Olson, *Commissioner*
Richard Vinyard, *Commissioner*
Daniel Williams, *Commissioner*



United City of
Yorkville ILLINOIS



Introduction

A streetscape master plan provides guidance for the direction and character of future street related capital improvement projects. As downtown Yorkville continues to evolve, so too should its streets and public spaces to support the changing land uses over time. Downtown has experienced multiple moments of transition over the years, but recently, downtown has experienced a renaissance of sorts with desirable new restaurants and small local businesses occupying existing structures. An improved Fox River-oriented park and other recreational amenities add another layer to downtown's assets.

At its heart, Yorkville is a small-town on a sleepy river with residents committed to improving the quality of the city for all. What better place to start than improving a downtown that should be the center of the community, where events, festivals, and family gatherings take place regularly. The streets of downtown Yorkville should be the armature that supports these functions and helps contribute to building community and quality of life.

Downtown Overlay District Streetscape Master Plan

Street Type Classification	p. 08
Bridge Street	p. 10
Hydraulic Street	p. 16
Van Emmon Street	p. 24
'B' Street	p. 32
Wayfinding and Signage	p. 40
General Streetscape Guidance	p. 42

Streetscape Master Plan

Why a Streetscape Master Plan?



Figure 1 - Bridge Street (Farr Associates)

Because downtown Yorkville needs one. Investors and property owners interested in improving their downtown assets may think twice if they do not sense a commitment from the City. The downtown TIF I was certainly an effort to encourage redevelopment within downtown and TIF II is an added incentive for owners to invest; however, the lack of an inspiring plan that presents future capital improvement priorities for the City leaves much to be desired. This streetscape master plan is intended to get people excited about the potential of downtown. Knowing that improvements are in the pipeline, investors can get out in front and establish a presence prior to downtown realizing its full potential.

A streetscape master plan's focus is on the public realm - most notably the streets, furnishing zones, and sidewalks. It helps to establish what role each street will play moving forward. For example, it establishes which streets are 'A' Streets; meaning a street that should be accompanied by building frontages, glazing, signage, and activity. They are the

streets that residents come to downtown to stroll along and enjoy a sunny Saturday afternoon. Alternatively, a 'B' Street supports the 'A' Street. Parking access, sides of buildings, and service oriented functions should be accessed off 'B' Streets. Like 'A' Streets, they are critical to the functioning successes of places we love. Accommodating both within downtown, while defining which is which, can help property owners prioritize where their future front entry is located or where that new café tenant should face.

Downtown was identified as a primary concern in the 2016 Yorkville Comprehensive Plan for good reason. Despite its current downfalls, downtown contains exciting assets to build upon. Restaurants, old buildings packed with potential, plenty of infill and redevelopment opportunities, and a fantastic recreational amenity in the Fox River, all bode well for the future of downtown. This plan demonstrates some of the strategies the City can implement to pave the way for the future of downtown.

Providing Framework for the FBC

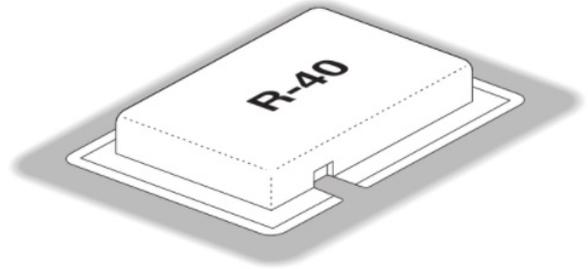
The streetscape master plan is structured to complement the **Downtown Yorkville Form-Based Code**. Form-based codes (FBC) are land development regulations that seek to produce predictable built results that prioritize building form over building use as a distinguishing factor. Often times, a regulatory zone or framework is applied at the block level, much like zoning, where parcels fall into a specific FBC classification.

The **Downtown Yorkville Form-Based Code** uses the street types as a regulatory framework. The parcels that front a specific street type identified in this plan use that street type as the underlying FBC classification. If a parcel fronts more than one street, the FBC articulates the process of discerning which FBC zone takes precedent. The FBC includes further instruction on how to identify a parcel, determine the underlying regulatory zone, and easily interpret the zone's requirements for redevelopment.

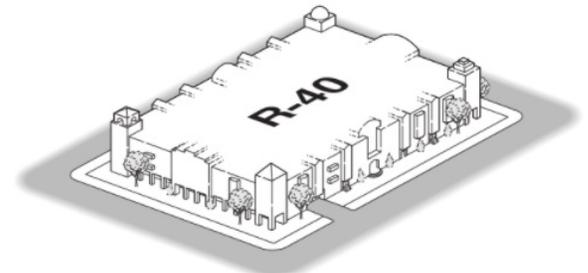
The streetscape master plan brings a visual interpretation of what form and character future capital improvements will exhibit. Though the streetscape master plan and the FBC are intended to be stand-alone documents, they are coordinated efforts that provide layers of detail to collectively envision the future of downtown Yorkville.

The series of diagrams on the right is from the Form-Based Codes Institute (FBCI) and meant to visualize the physical consequences that stem from conventional zoning (top), conventional zoning with supporting design guidelines (middle), and form-based codes (right). Standards that prioritize form over use have the capabilities of encouraging a more fine-grained outcome.

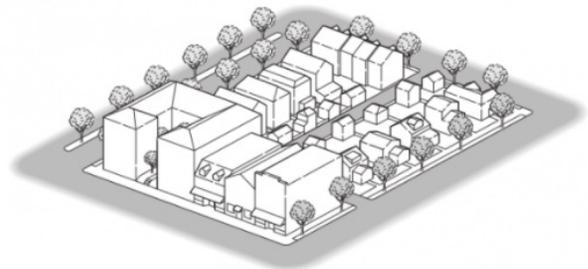
Conventional Zoning (FBCI)



Zoning Design Guidelines (FBCI)



Form-Based Codes (FBCI)



Street Type Classification

Street Type Classification

The following street types represent what will be the guiding framework for the form-based code (FBC) parcel classifications. Parcels fronting their respective street type should follow the form-based guidelines outlined in the Downtown Yorkville Form-Based Code. The different street types are

- 'A' Streets
 - Bridge Street
 - Hydraulic Street
 - Van Emmon
- 'B' Streets

These street types are represented on the following pages with the existing condition, proposed near-term improvements, and proposed long-term vision. The street types are represented at typical segments along key stretches; therefore, minor variations will occur where applicable.

This Streetscape Master Plan is intended to envision the character and role each street contributes to the future of downtown Yorkville and does not represent finalized landscape and construction details.



Figure 2 - Bridge Street (Google Maps)



Figure 3 - Hydraulic Street (Google Maps)



Figure 4 - Van Emmon Street (Google Maps)



Figure 5 - Main Street (Google Maps)



Street Type Classification



Bridge Street (Existing)

IDOT's Improvements

Bridge Street, between Hydraulic Street to the north and Van Emmon Street to the south, was clearly the historic downtown core of Yorkville. Though this stretch is only one block long, it retains much of the scale and character of the past. Bridge Street was historically a two travel-lane street with parallel parking on either side to serve the businesses. Traffic became congested, since Bridge Street (IL 47) is the main truck route through Yorkville. The Illinois Department of Transportation (IDOT) studied widening the street along with other improvements to alleviate the congestion. Many years after the initial plan of a five-lane Bridge Street, the proposal was finally taken to construction. Yorkville residents were anxious to speed up flow through a downtown long removed from representing the heart of the community.

Since the IDOT improvements, the commercial viability of the businesses were challenged. Travel lanes replaced parallel parking and concrete barriers were placed between street and sidewalk. The combination of road widening, increased speed, lack of parallel parking, and other factors drove many of the primary building entries around to the backs of the Bridge Street buildings. The increased speeds and lack of pedestrian traffic along Bridge Street have effectively drained downtown of any potential for vitality. Residents have mixed opinions about the impacts of IDOT's improvements; however, it is clear that the term "improvements" may not be the correct expression for Bridge Street's new character.

This stretch of Bridge Street may be considered the gateway into downtown Yorkville and retains potential to become the iconic stretch that helps draw people into local businesses and displays an attractive image that represents the people of Yorkville.



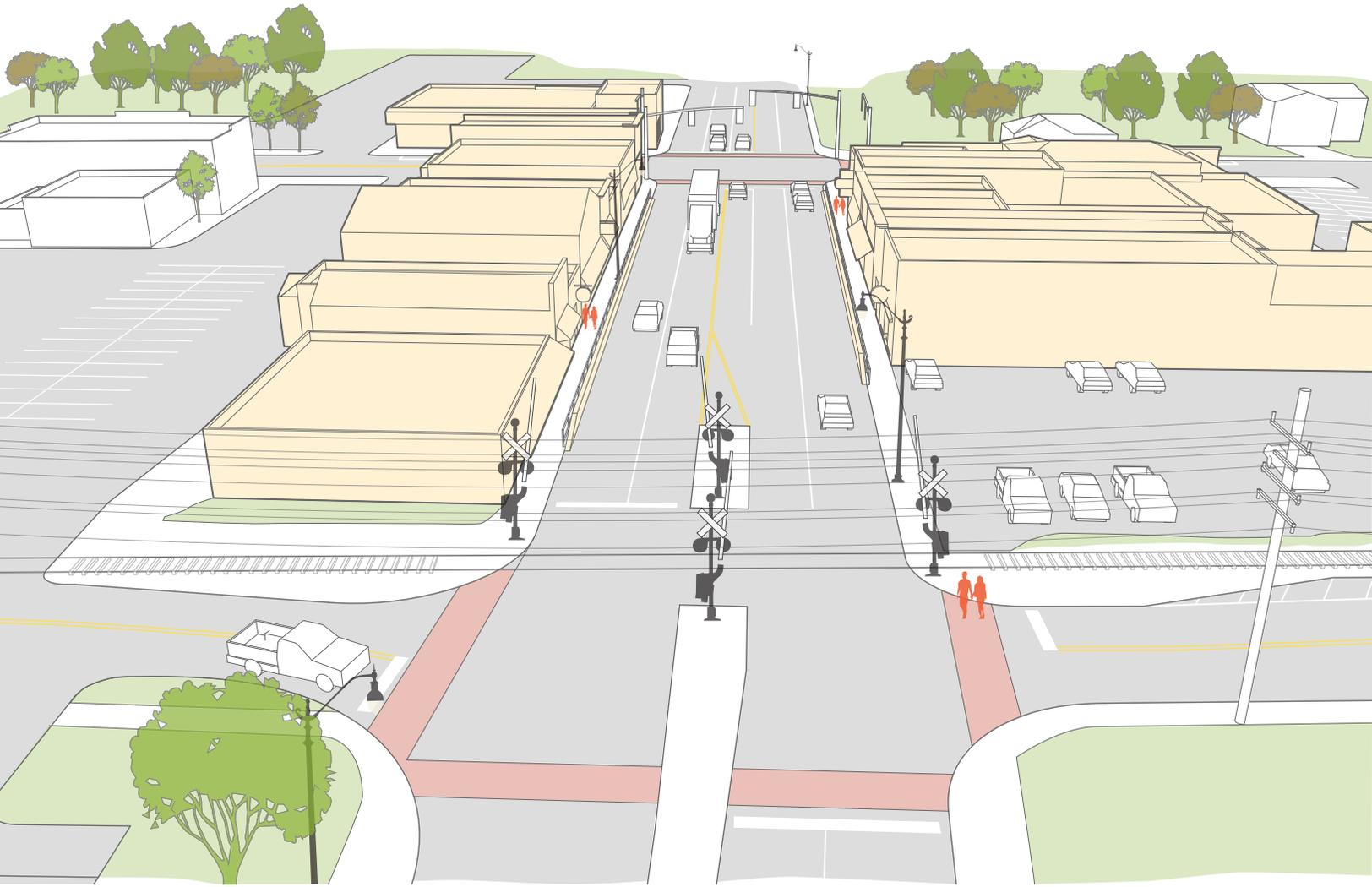
Figure 6 - Bridge Street Facades (Farr Associates)



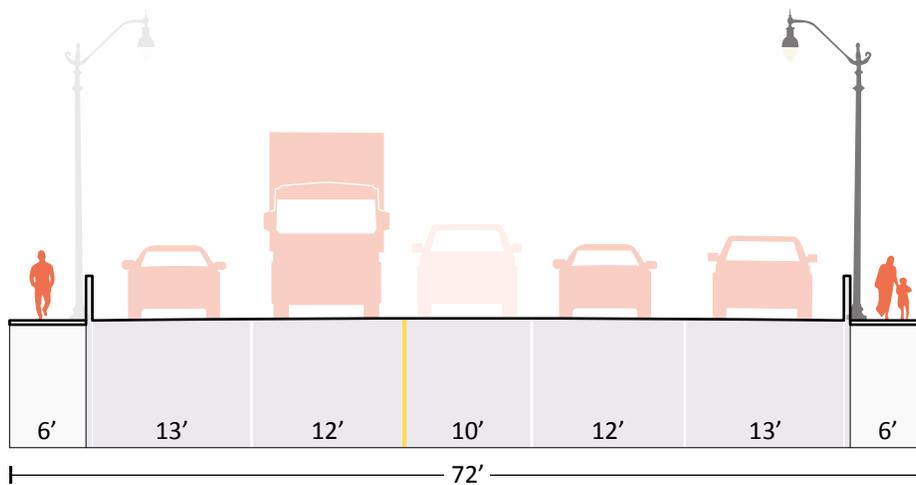
Figure 7 - Bridge Street Blank Wall (Farr Associates)



Figure 8 - Bridge Street Sidewalk/Barrier (Farr Associates)



Bridge Street (Existing)



Bridge Street (Near-Term)

Tactical Interventions

Meaningful measures to display an image of vitality and interest can be taken with a cost sensitive approach. For example, instead of temporarily narrowing traffic lanes or tearing down the now important concrete barriers flanking Bridge Street; beautification strategies might include painting the concrete barriers and hand rails with a custom design or painting large iconic murals on the blank downtown building walls. Each of these interventions could contribute to the overall character of downtown and play a dual role of encouraging vehicular and pedestrian traffic to be cautious and slow down.

Because this segment of Bridge Street is such an important gateway for the City, concentrating multiple interventions on this location within the greater downtown should take priority over other streets and locations. Drivers would recognize that downtown could be worth visiting. The larger scale of these proposed interventions caters to the car, because it is in this brief moment that downtown has to attract the attention of passersby.

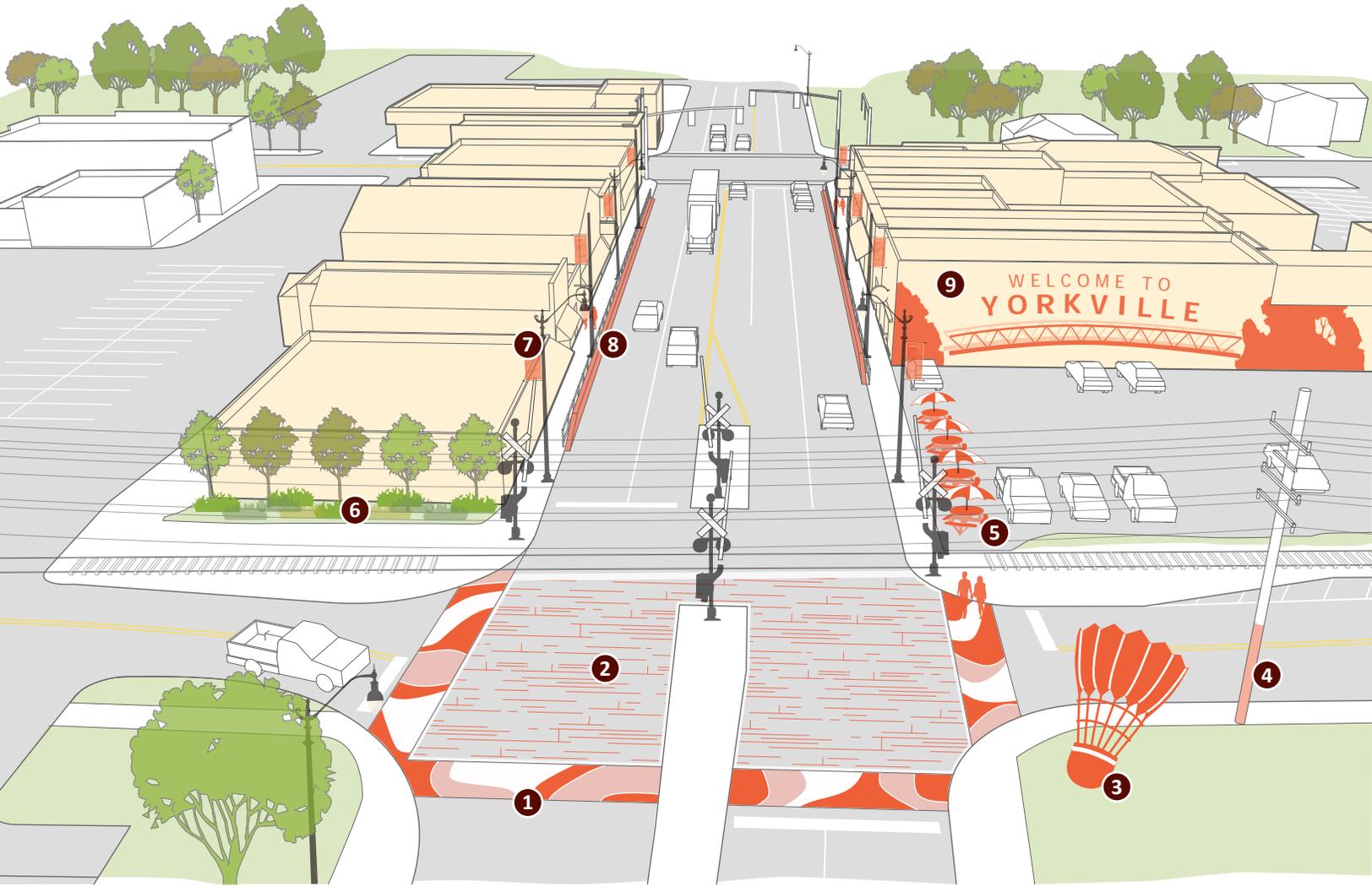
- 1 Painted Crosswalks
- 2 Paving Texture or Material Change
- 3 Public Art/Sculpture
- 4 Painted Light Poles
- 5 Outdoor Restaurant Seating
- 6 Entry Landscape Improvements
- 7 Seasonal Banners
- 8 Painted Bridge Street Barrier/Railing
- 9 Yorkville Entry Wall Mural



Figure 9 - Wall Mural (Philly Magazine)



Figure 5 - Outdoor Restaurant Seating (Pictures Boss)



Bridge Street (Near-Term)



Figure 11 - Painted Concrete Barrier (NYC Parks)



Figure 12 - Public Art / Sculpture (Designboom)

Bridge Street (Long-Term)

Minor Upgrades Go a Long Way

The existing Bridge Street right-of-way affords very little flexibility for major improvements; however, that does not preclude meaningful upgrades from happening. Squeezing in improvements where possible, such as: the addition of seasonal banners to the light poles; repaving the sidewalks with high-quality and interesting materials for pedestrians; or replacing the damaged handrail with a feature handrail that may be an art installation; can make a surprisingly dramatic impact for both drivers and pedestrians. Additionally, if the buildings better engage the sidewalks through accessible entrances, signage, and outdoor seating options, this would improve this highly visible stretch of downtown Yorkville.

A reduction in lane width on Bridge street, which would require a reclassification from IDOT to remove its truck route status, is not currently an option. Therefore, a five-lane street will likely be the long-term reality.

It will be critical for the City to address the perception issues with Bridge Street through near-term solutions that may last many years. Near-term strategies can add value to downtown Yorkville through amplified crosswalks, branding and wayfinding elements, visible outdoor seating, and much more.



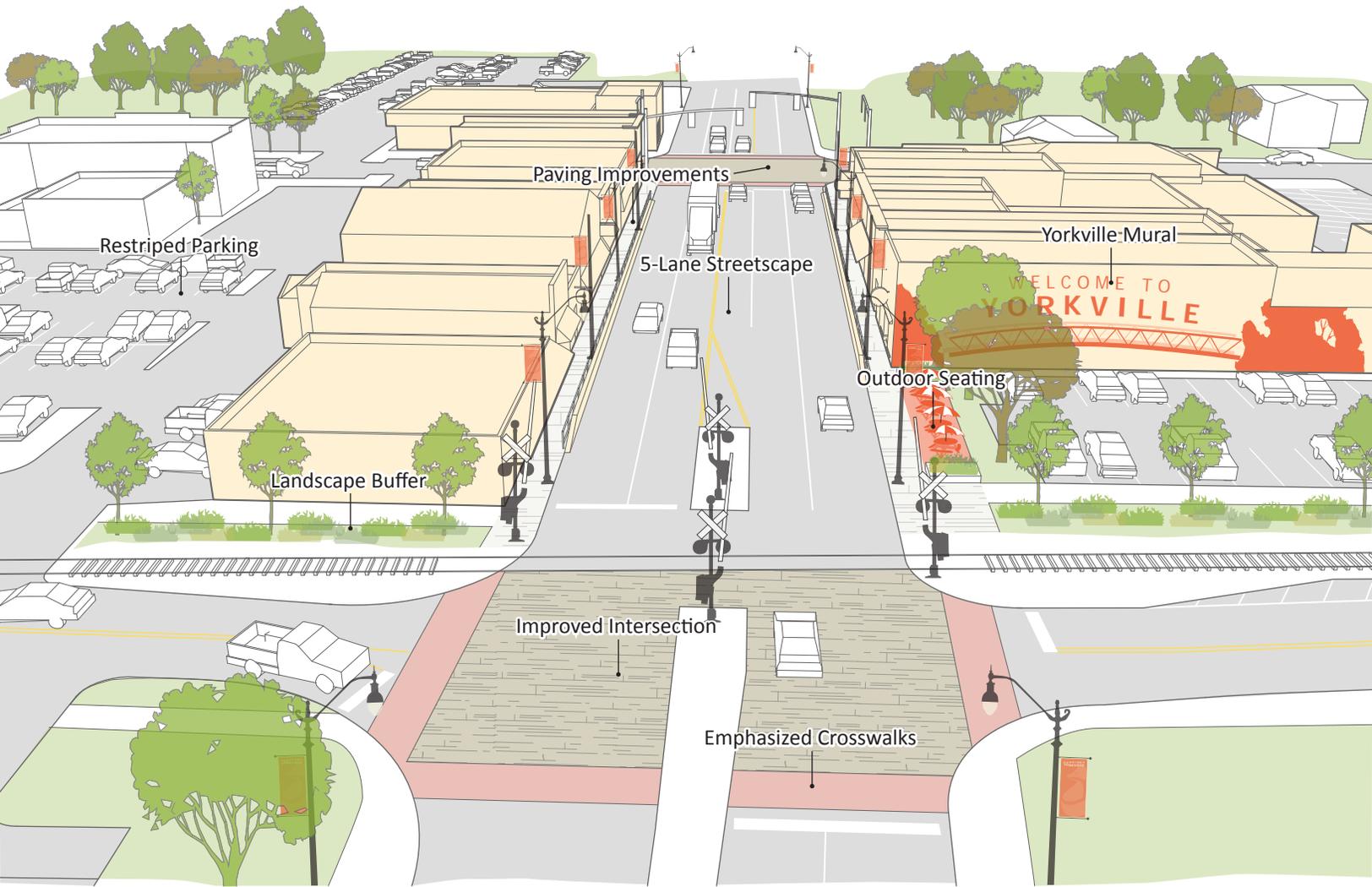
Figure 13 - Improved, Feature Handrail (Hype Science)



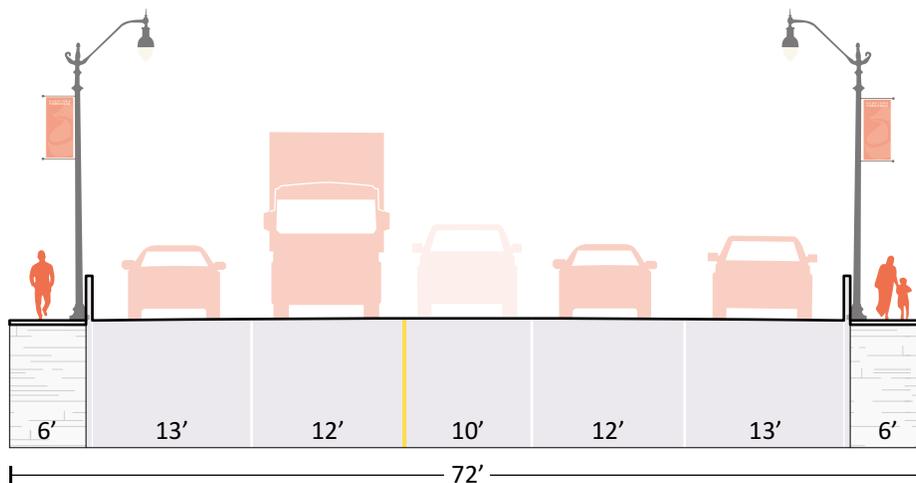
Figure 14 - Seasonal Banners (Farr Associates)



Figure 15 - Improved Sidewalk Pavement (California DOT)



Bridge Street (Long-Term)



Hydraulic Street (Existing)

Unique Industrial Character

Hydraulic Street includes complex conditions, such as utility poles landing in the street; a tapering right-of-way that narrows from west to east; and an active freight rail line that runs parallel to the street surface within the right-of-way. Because it runs parallel to the Fox River, there are multiple access and view corridors that connect pedestrians on Hydraulic Street to one of Yorkville’s most important assets.

The freight rail line tracks are immediately adjacent to the southern edge of the street. This proximity could be a safety issue; however, during the citizen engagement events, residents did not voice much concern over the rail, except the noise complaints and potential for trains to back up traffic along Bridge Street. Though only one rail company utilizes the line, it is important to the natural gas industry because it accesses select sand used for the fracking process. Any expectations of the rail line closure are unrealistic, at least in the near-term. The active rail line will remain something that any redesign plans, current or future, must address.

Hydraulic Street features a unique industrial character as a result of the rail and adjacent buildings and uses. The short, utilitarian buildings, as well as agricultural relics, such as the grain elevator, create an eclectic mix of land uses and character. Multiple popular businesses and parks exist along Hydraulic Street, and the existing character seems to support these types of businesses. As improvements occur to make Hydraulic Street a more attractive, safe, and usable street, a respect for its industrial past and present should be retained.



Figure 16 - Hydraulic Street (Farr Associates)



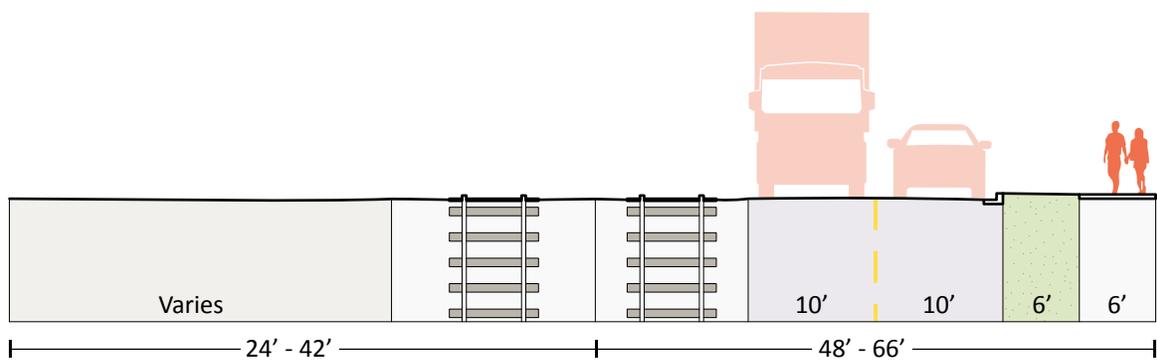
Figure 17 - Hydraulic Street (Farr Associates)



Figure 18 - Inactive Grain Elevator (Farr Associates)



Hydraulic Street (Existing)



Hydraulic Street (Near-Term)

Tactical Interventions

Hydraulic Street character will remain unique and interesting with or without planning interventions. The low traffic counts and limited number of businesses that currently front onto Hydraulic Street suggest that a lower level of resources should be placed in its near-term improvements.

However, a few key improvements that help support the current businesses should be prioritized. This could include allowing outdoor seating areas either in the parking lots or at the edge of the street and sidewalks; painting the existing light poles with unique artwork; adding seasonal banners to the existing light poles; and improving the rear facade of Bridge Street, as this has effectively become the primary entry to many of those businesses. The rear facade of the Bridge Street buildings is highly visible since no structures currently exist to obscure mid-block views.

Additionally, moveable planters with trees and other vegetation could bring life, shade, and color to an otherwise utilitarian Hydraulic Street.

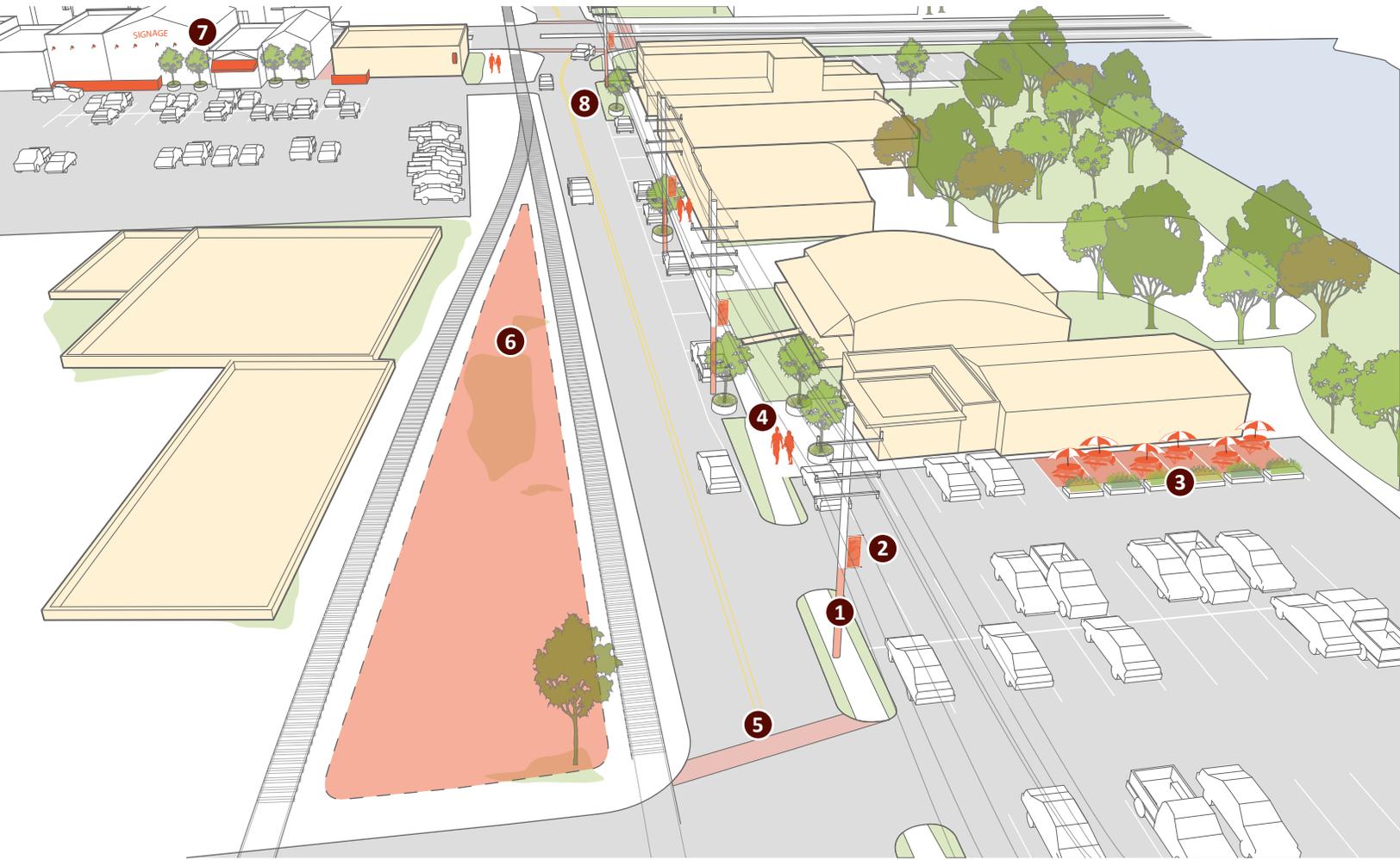
- 1 Painted Light Poles
- 2 Seasonal Banners
- 3 Outdoor Restaurant Seating
- 4 Temporary Tree Planters
- 5 Painted Crosswalks
- 6 Maintain Gravel Between Tracks
- 7 Bridge Street Rear Facade Improvements
- 8 Street Surface Lane Striping



Figure 19 - Painted Light Pole (Tops Images)



Figure 20 - Well Maintained Gravel Surface (Dare Inc.)



Hydraulic Street (Near-Term)

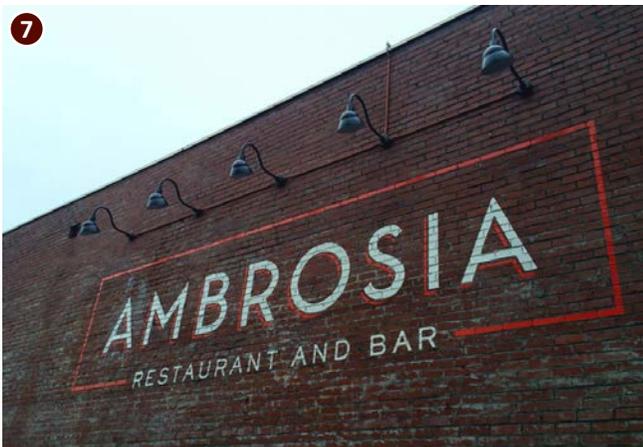


Figure21 - Rear Facade Lighting and Signage (House St. Clair)



Figure22 - Outdoor Restaurant Seating (South Milwaukee)

Hydraulic Street (Long-Term)

Conversion to a Shared Street

The unique qualities of Hydraulic Street with its irregular street section, limited traffic count, paralleling of the Fox River, and already funky, casual character lends itself well to becoming a woonerf, or “shared street”. This long-term vision would allow Hydraulic Street to be closed down for festivals or events between the current driveway aligning with the rear entrances of the Bridge Street buildings and Heustis or Mill Street. Circulation around the block would still be possible through the use of a “slip lane” or access lane that would run along the south edge of the railroad tracks in order to access future redevelopment on those parcels.

Hydraulic Street could take on a unique design such as permeable pavers to add character and stormwater management benefits. Being adjacent to the Fox River, an effort to minimize stormwater runoff and encourage percolation would be an environmentally conscious solution. Continuous pavers spanning between vehicular travel areas and traditional pedestrian areas would effectively blur the line between pedestrian and car right-of-way. This would encourage slow moving traffic on non-event days when Hydraulic Street is open, and add an attractive frontage for the businesses along Hydraulic Street.

A shared street deserves a custom design. When City budget is allocated towards Hydraulic Street capital improvements, an emphasis should be placed on hiring highly-qualified landscape architects experienced in right-of-way redesign.



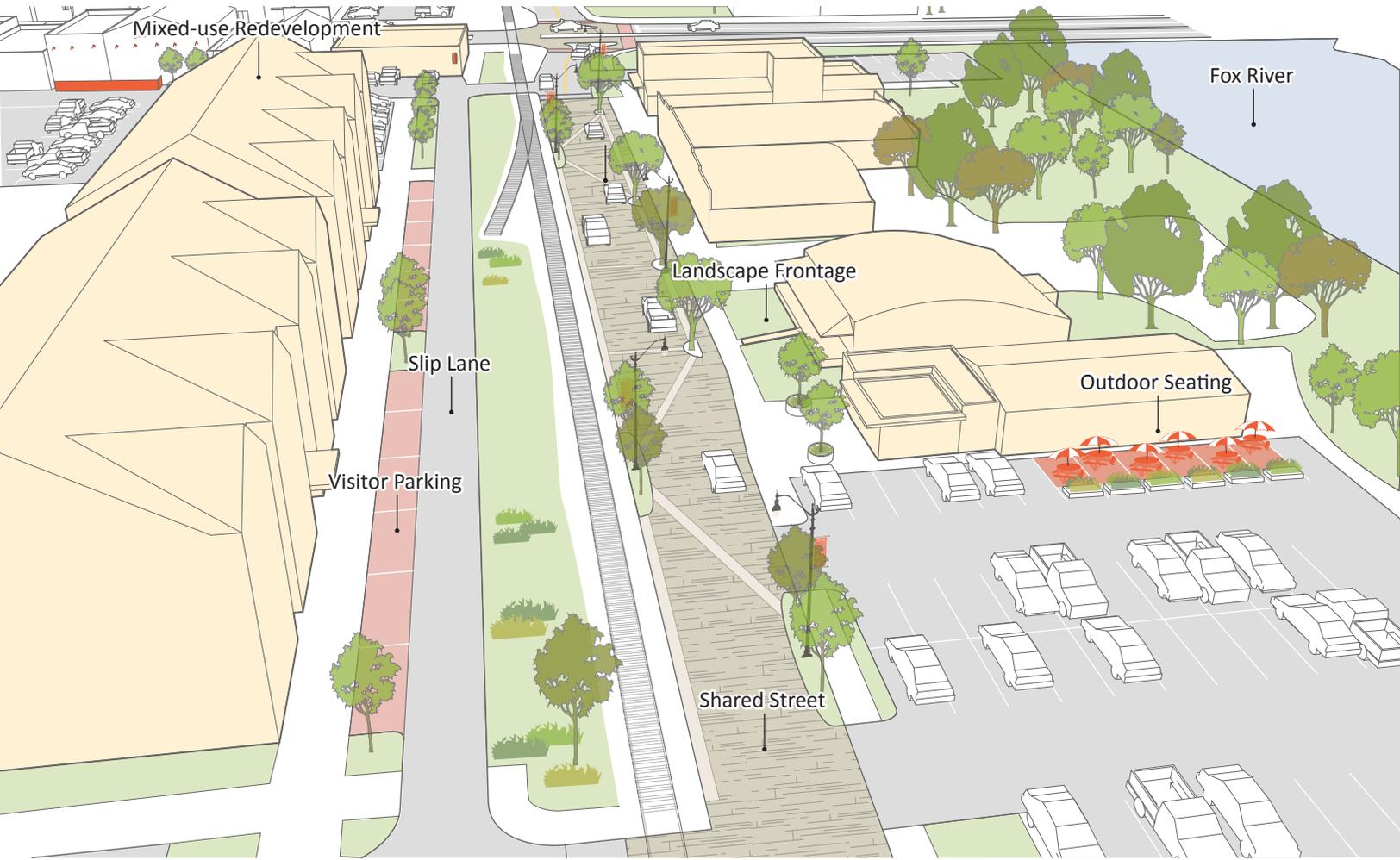
Figure 23 - Chicane Plantings (NACTO)



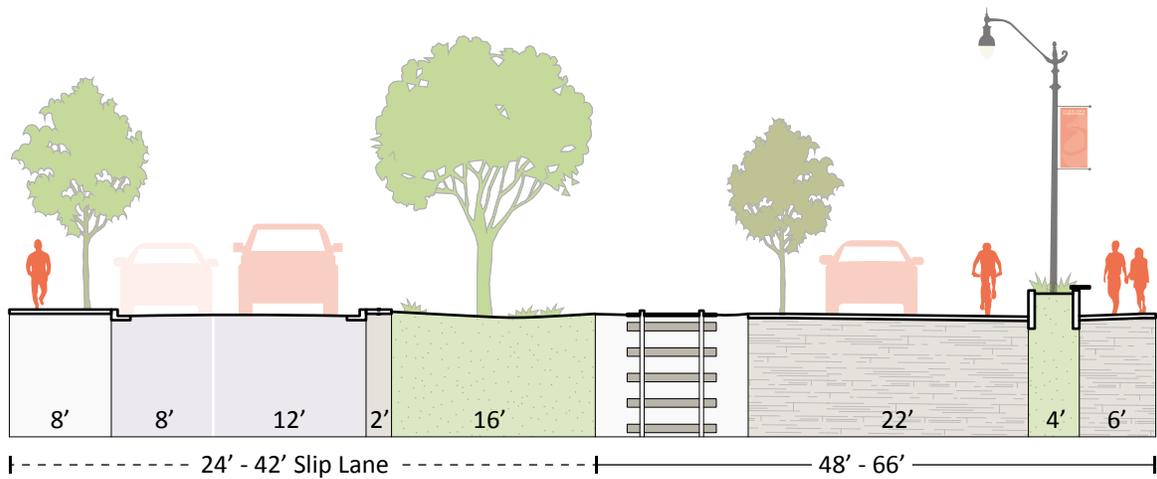
Figure 24 - Funky Outdoor Seating (Asik Site)



Figure 25 - Shared Street (Ithaca College)



Hydraulic Street (Long-Term)



Hydraulic Street Shared Street

As previously mentioned, a shared street should blur the lines between vehicle and pedestrian zones. Subtle changes to paving materials and the use of planters, street furnishings, and markings can define where cars should or should not drive. Since Hydraulic Street runs parallel with the Fox River, a street section that sheet flows stormwater into a continuous drainage channel within paving change is an example of integrated street design. These site design details are important, as this will become a major downtown event location.

- 1 Paving Texture or Material Change
- 2 Chicane Planting Beds
- 3 Maintain Existing Distance from Tracks
- 4 Raised Planting Beds
- 5 Planter Seating Ledge
- 6 Continuous Drainage Channel on One Side

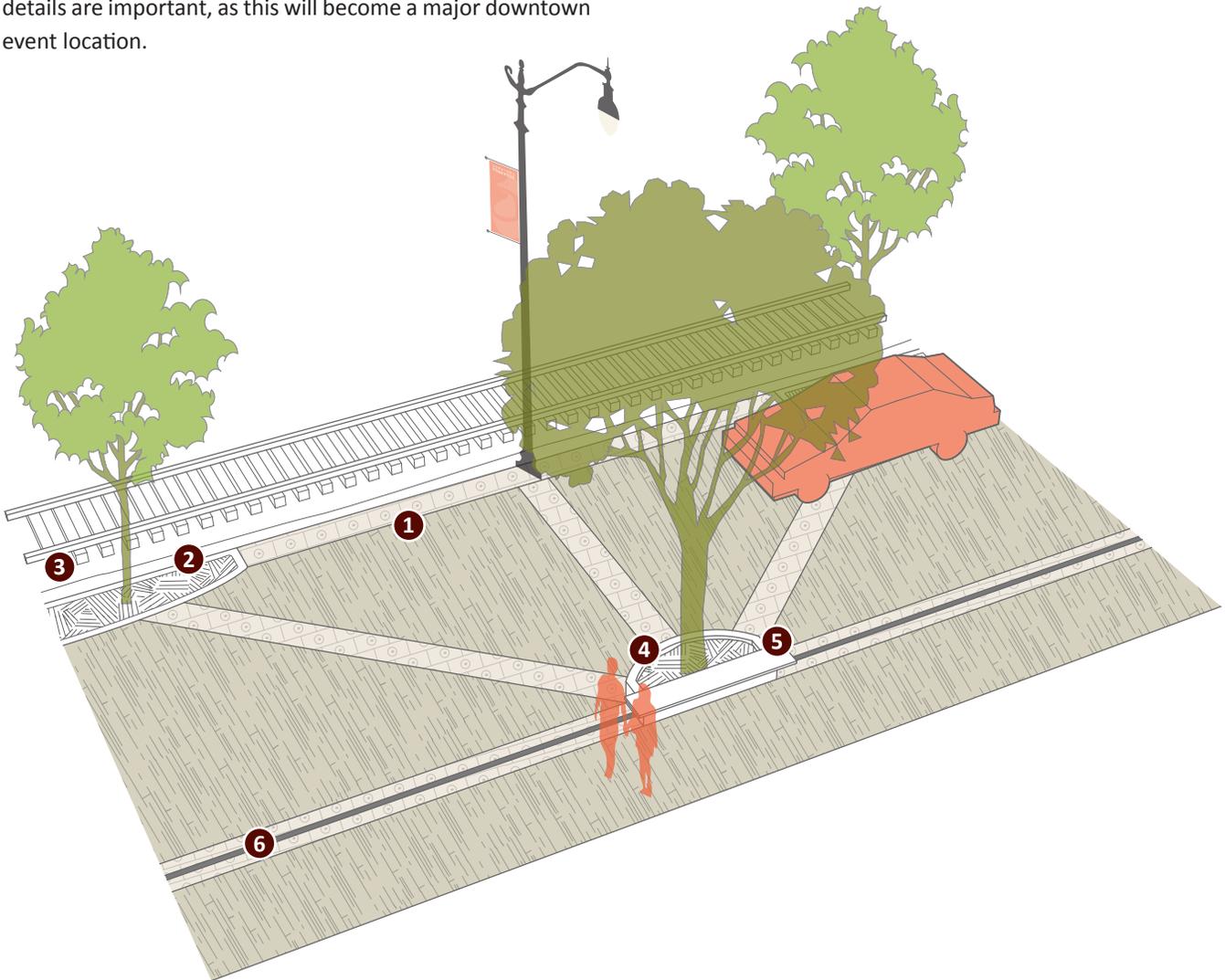


Figure 26 - Hydraulic Street Diagram (Farr Associates)

Material Contrast

Subtle paving details, such as a change in material, texture, or orientation, contribute interest to the pedestrian environment. Minor variations can help make a design unique to a specific location and help brand the place. Paving details may be used to delineate where vehicles are allowed to drive or park. These variations may help inform the location of custom street furniture, emphasize locations for tree plantings and signage locations, or simply add aesthetic appeal.



Figure 27 - Paving Texture Change (Site Design Group)

Street to Woonerf Transition

Shared streets typically have vehicular travel routes and parking at the same elevation as a sidewalk. This means that the street section will not have its typical curb and gutter condition and will also need to reconcile where a regular street meets the woonerf. Raised intersections and crosswalks commonly feature short ramp transition zones to raise travel lanes to the desired shared height. The transition zones can also help alert drivers that they are entering a special area and a heightened awareness for pedestrians is necessary.



Figure 28 - Raised Intersection (NACTO)

Planter Seating Ledge

A custom street design can include custom street planters and seating. In the instance of a shared street, raised planters made of a durable material could house low plantings and street trees; accommodate one or multiple built in seating ledges; and play a role in vehicular circulation by delineating the travel lanes, narrowing travel lanes to encourage slower travel speeds, or creating chicanes that slow-traffic to a greater degree.



Figure 29 - Planter Seating Ledge (Transform KC)

Van Emmon Street (Existing)

Downtown's Second Gateway

Traveling east from downtown Yorkville, Van Emmon Street eventually becomes Van Emmon Road and meets Route 71, which connects Yorkville with Oswego. Van Emmon Street acts as the second gateway into downtown, particularly at its intersection with Bridge Street. Recently, buildings have been torn down along Van Emmon Street and the expectation of future redevelopment is not unreasonable.

The approach traveling west into downtown along Van Emmon does not best represent Yorkville. Vegetation overgrowth between Mill Street and Heustis Street; an imbalanced street section of residential buildings with parking in front; a concrete retaining wall; and multiple “missing teeth” in the urban fabric leave much to be desired. Additionally, the intersection of Van Emmon Street and Bridge Street is not particularly inspiring, as each corner does not activate the intersection. Businesses are making an effort at providing visible programming at the intersection, but improvements are needed to establish the cohesive image the downtown Yorkville deserves.

Van Emmon Street west of Bridge Street has a different character. It generally becomes more residential after the first half block. Naturally, the street section abruptly adjusts as it enters the neighborhood.



Figure 30 - View West Down Van Emmon Street (Google)



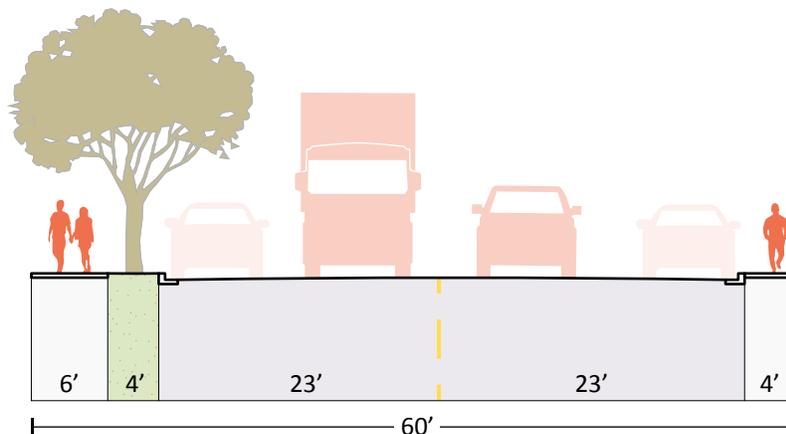
Figure 31 - View West Down Van Emmon Street (Google)



Figure 32 - View West Down Van Emmon Street (Google)



Van Emmon Street (Existing)



Van Emmon Street (Near-Term)

Tactical Interventions

Interventions should be focused at the intersection of Van Emmon Street and Bridge Street. Businesses on either side of the intersection are already considering providing outdoor seating options, which would add visible energy to the downtown when approaching from the south. This is a positive direction and can be amplified with the removal of parking spaces to construct a temporary or permanent parklet, providing a canopy or other form of weather protection, or introducing outdoor space heaters to extend seating months.

This intersection is also the primary crossing for pedestrians and vehicles moving between the east and west sides of Bridge Street due to traffic signalization and pedestrian crossing indicators. Painting the crosswalk and intersection would be an opportunity to brand downtown, enhance safety and visibility for crossing pedestrians, and draw attention to its businesses.

In addition to intersection treatments, the southwest corner of the intersection could be used for signage that greets residents and visitors traveling into downtown. Plans are already underway to improve the County Courthouse slope.

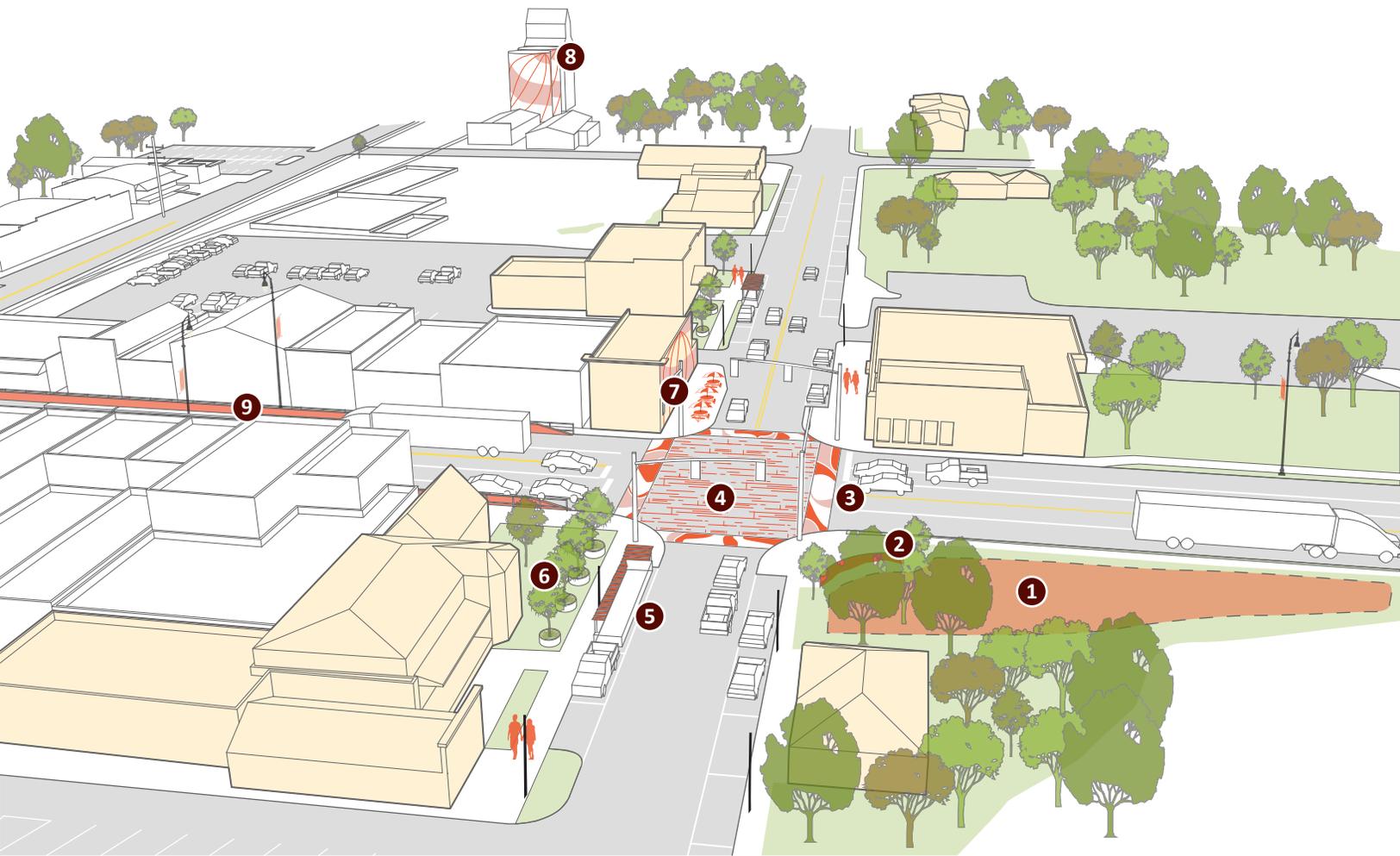
- 1 County Courthouse Landscape Beautification
- 2 Gateway Monument Signage
- 3 Painted Crosswalks
- 4 Painted Intersection
- 5 Parklet
- 6 Temporary Street Trees
- 7 Outdoor Seating
- 8 Painted Grain Elevator
- 9 Painted Bridge Street Barrier/Railing



Figure 33 - Landscape Beautification (Cedrus Landscaping)



Figure 34 - Painted Grain Elevator (News OK)



Van Emmon Street (Near-Term)



Figure 35 - Temporary Street Trees (ASLA)



Figure 36 - Painted Crosswalk (Broward Palm Beach)

Van Emmon Street (Long-Term)

Greeting Visitors with Beauty

Van Emmon Street should be beautiful to establish a first impression to visitors, particularly east of Bridge Street. The mix of topography and building forms can be challenging when the desired outcome is a pleasant street; however, the asymmetry can be accommodated through careful design considerations.

A primary consideration should include where stormwater is flowing. With elevated parcels on the south edge of Van Emmon Street, surface water will flow towards the street and sidewalk. Utilizing rain gardens that temporarily store rainwater before discharge or percolation and/or using permeable pavers that allow water to dissipate through the parking surface, are ways to accommodate the increased runoff. Also helping with stormwater, street trees should be planted on either side of the approach from the east to create a block long gateway when driving into Yorkville from Oswego.

Downtown branding elements, such as seasonal banners and wayfinding signage, can add to the cohesion of downtown. The historic Kendall County Courthouse slope can act as a gateway feature incorporating signage, or some other built element, to establish a four-sided Van Emmon and Bridge Street intersect. These improvements could be completed in the near-term with the expectation that they would remain as other capital improvements take place.



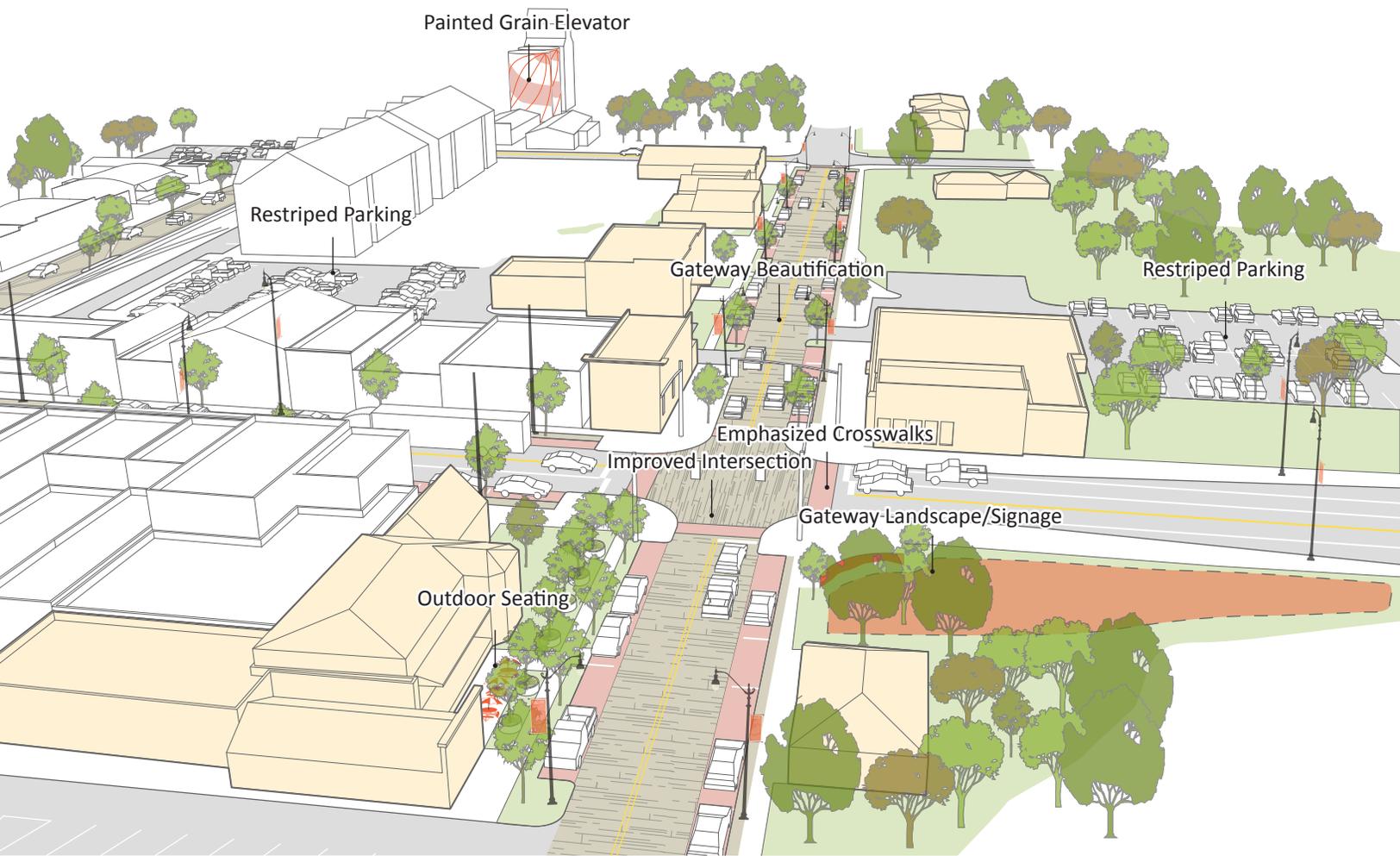
Figure 37 - Stormwater Streetscape (novitalas.com)



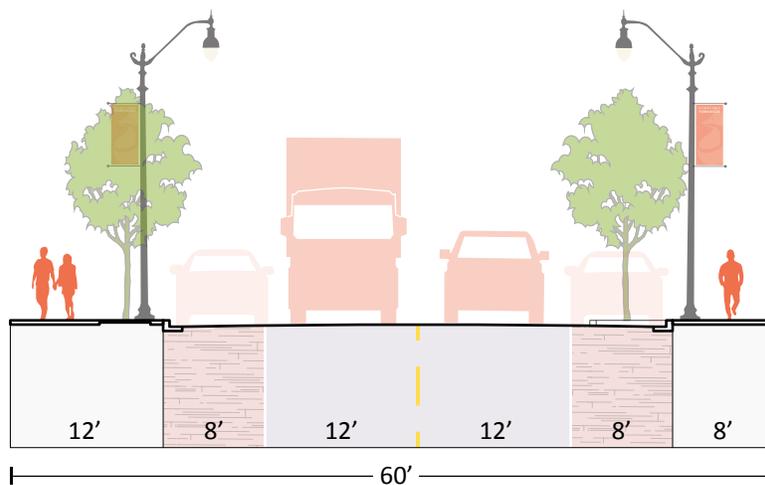
Figure 38 - Intersection Improvements (PicSnapper)



Figure 39 - Permeable Parking Pavers (Village of Shorewood)



Van Emmon Street (Long-Term)



Stormwater Bulb-Outs

Bulb-outs typically narrow roadways to direct traffic, slow speeds down, expand the sidewalk, or simply accommodate plantings. Integrating bulb-outs that also perform stormwater management functions, would be an attractive design feature along Van Emmon Street. Bulb-out curbs should be slotted, or notched with openings, to allow surface stormwater to enter and exit the planting zone of the bulb-out.



Figure 40 - Stormwater Bulb-Out (Toni Best)

Sidewalk Rain Gardens

Stormwater rain gardens could be used to improve runoff quality and provide detention for significant storm events. Like bulb-outs, they can accommodate a variety of planting types and should have inlets allowing water to flow in and out from the street. Trees and a variety of plantings that provide color and texture would add to the beautification of Van Emmon Street.



Figure 41 - Rain Garden (Vava)

Permeable Parking Pavers

Tying designs back to other streets within downtown, Van Emmon Street could feature open grid permeable pavers that both define the parking areas and increase stormwater capacity. Coordinating pavers between Van Emmon Street, Hydraulic Street, and any other location they are used in the greater downtown would support a cohesive design and branding language.



Figure 42 - Permeable Pavers (Terran Capital)

Sustainable Street Section

Van Emmon Street can represent Yorkville's demonstration of a sustainable street section. The right-of-way width provides plenty of flexibility for a two-lane street. Additionally, the asymmetrical street section that has a higher elevation on one side is an ideal candidate to display sustainable stormwater strategies.

- 1 Slotted Curbs
- 2 Stormwater Bulb-Out Planter
- 3 Parallel Parking
- 4 Rain Garden Planter
- 5 Permeable Pavers

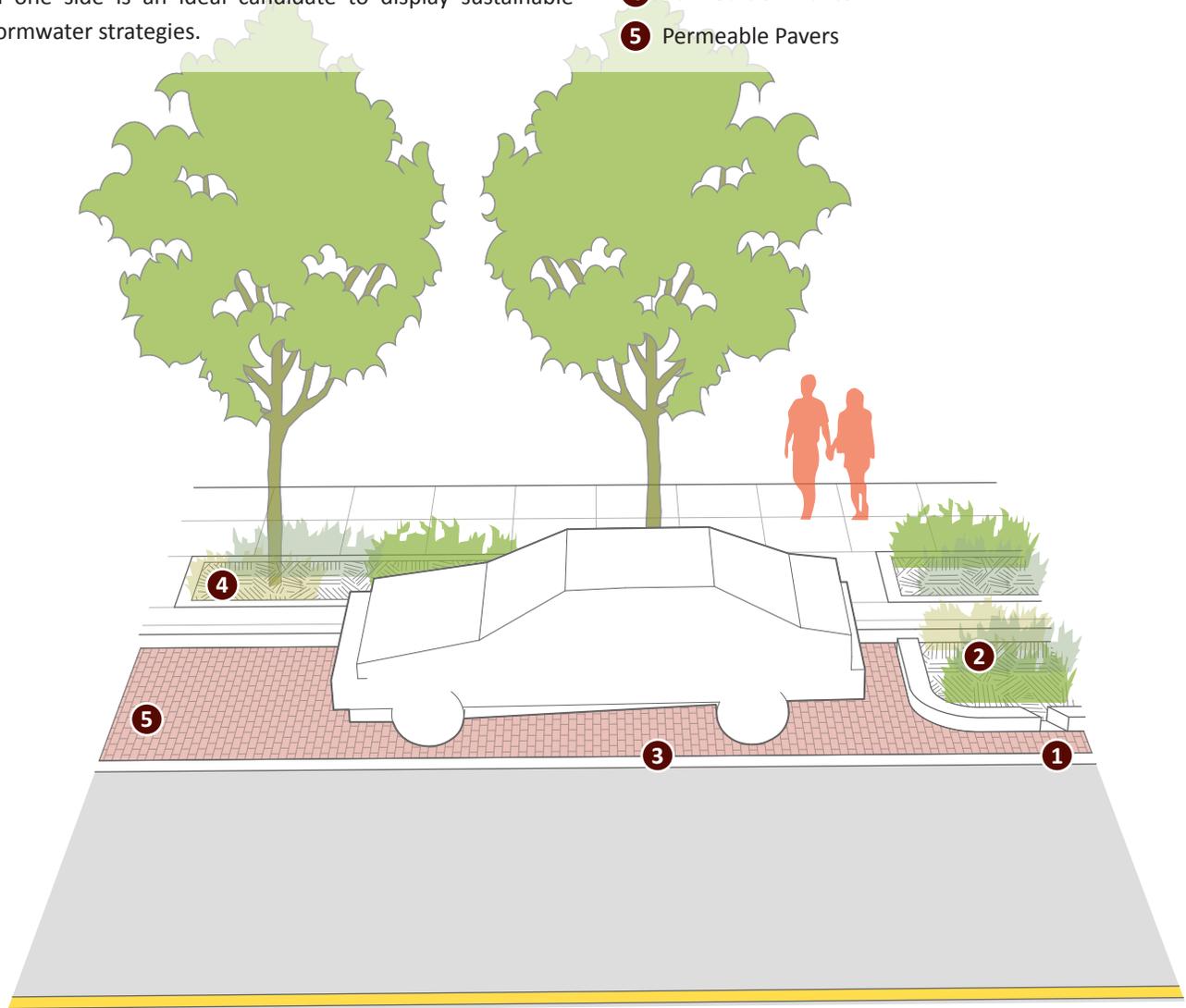


Figure 43 - Van Emmon Street Diagram (Farr Associates)

'B' Street (Existing)

Small-Town Character

One of the most charming aspects of Yorkville, is the historic, small-town character inherent in the City's streets and buildings. These streets may have narrow or no sidewalks, are often curbless, and exhibit a casual feel that may slightly change between each individual property. Many of these streets are both the front door and driveway access to homes and buildings. These types of streets that serve buildings through parking and service access are vital to the functionality of the neighborhoods.

Around downtown the north/south streets that run parallel with Bridge Street act as 'B' Streets. These streets that feature a small-town character should continue to support the downtown uses, as well as provide a framework for future development to take place.

Because future development will likely be limited, to an extent, and single-family homes exist immediately adjacent to the downtown blocks, the 'B' Streets should maintain a small-town character that aligns with many future uses but does not negatively affect existing single-family homes.



Figure 44 - View North Down Main Street (Google)



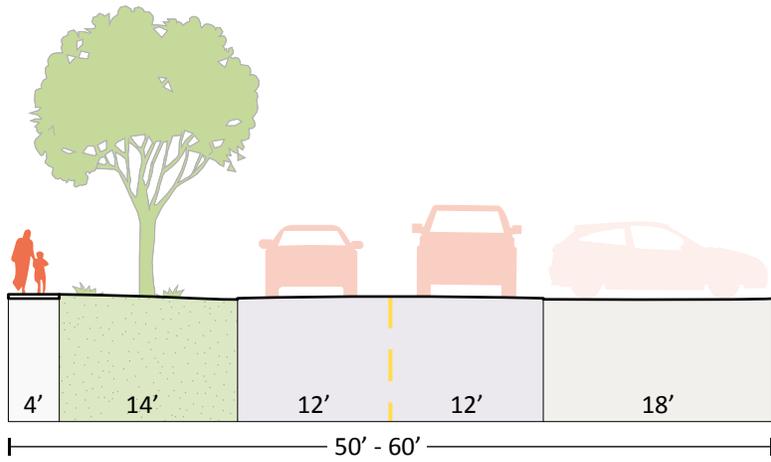
Figure 45 - View North Down Main Street (Google)



Figure 46 - View North Down Main Street (Google)



'B' Street (Existing)



'B' Street (Near-Term)

Tactical Interventions

The 'B' Streets should be less of an investment priority. They receive less of the pedestrian and vehicular traffic compared to Bridge Street, Hydraulic Street, and Van Emmon Street. It is important that they play a supporting role, but resource allocation should be minimal.

Ensuring sidewalk continuity and basic maintenance should be the City's top priority. Having overgrown landscape shoulders, missing and poorly maintained portions of the sidewalks, and poor road surfacing are examples of issues that should be addressed. Downtown's 'B' Streets should be eligible for resurfacing and landscape maintenance.

One unique aspect of downtown's 'B' Streets, which primarily run north/south, is how they terminate into the Fox River. Each of these moments where drivers and pedestrians can look down the street and have a visual connection to the river is an opportunity that should be taken advantage of by implementing such measures as prohibiting parking, providing an active or focal point of interest, and trimming back overgrown vegetation to reveal the water.

- 1 Added Crosswalks Towards River
- 2 Roadside Swale Improvements
- 3 Additional Street Trees
- 4 River Access Signage
- 5 Public Art/Sculpture at Main Street Terminus
- 6 Trim Vegetation for View Towards Water



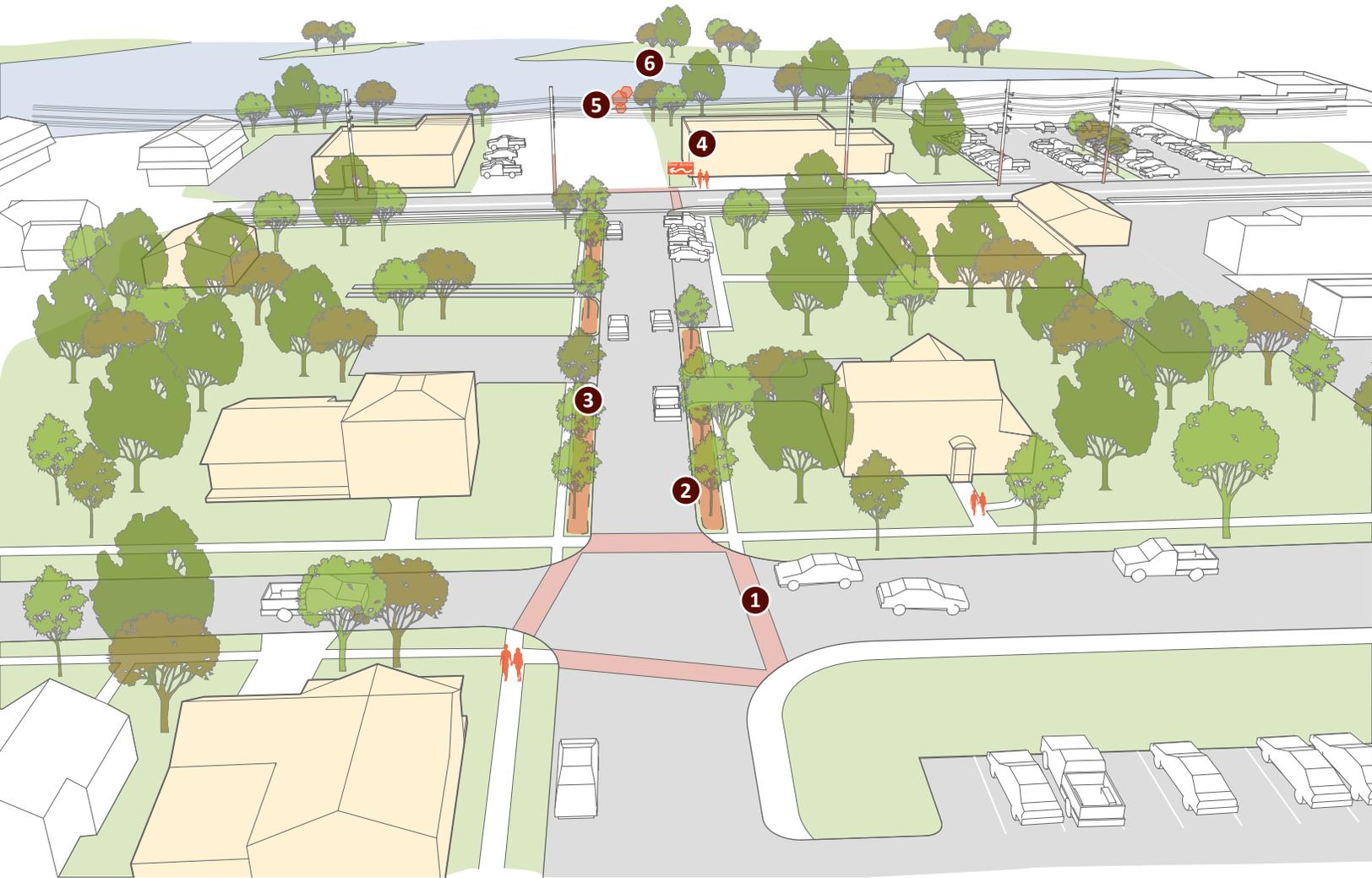
Figure 47 - River Access Signage (Google)



Figure 48 - Road Resurfacing (Decatur Daily)



Figure 49 - View Towards Water (Google)



'B' Street (Near-Term)

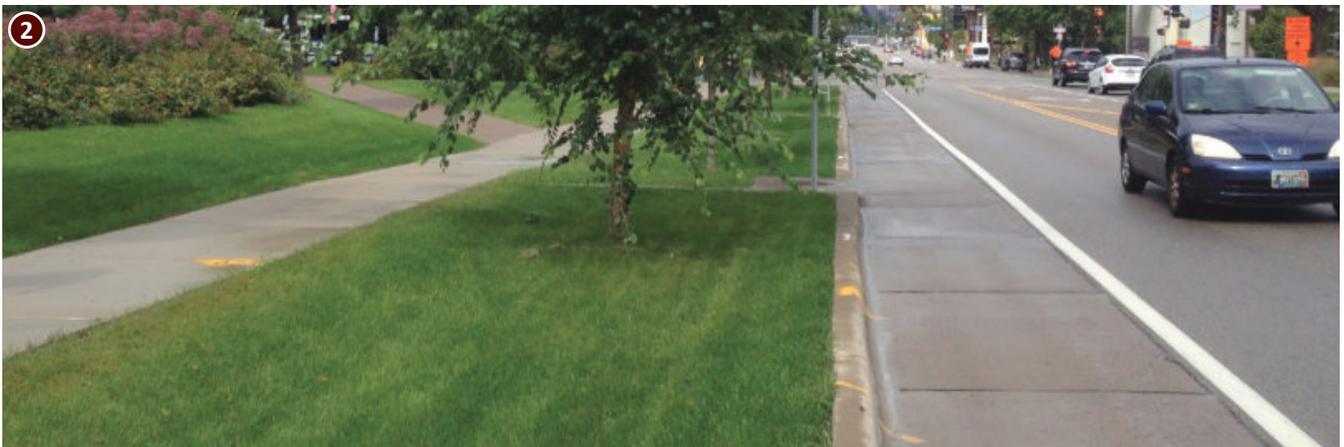


Figure 50 - Basic Roadside Swale (NACTO)

'B' Street (Long-Term)

Redefining Small Town Streets

As streets are rebuilt to support a changing downtown, it will remain important to minimize impact on the pleasant, historic character of the surrounding neighborhood. Careful design consideration to not over-correct the 'B' Streets should be maintained. Community members have expressed how they value the small-town character that brought many of them to Yorkville, or kept them there, in the first place.

Roadside swales can be used to both direct and detain stormwater. Plantings can include a mixture of City contributions and resident contributions. This will allow the property owner to make some customizations to the extension of their front lawn. For example, small wooden bridges might connect the street with the sidewalk to their front door. These small customizations add to the character that already exists.

Small details can make these streets more attractive, such as having a curbless intersection that allows stormwater runoff to enter the roadside swales. The edge of the street pavement can be a gravel transition that delineates between travel lane and shoulder. Trees could be planted irregularly or even provided by the property owners from an approved landscape palette. Sidewalks should be narrow, but continuous to have minimal impact while providing maximum connectivity.



Figure 51 - Roadside Swale (Green Infrastructure Digest)



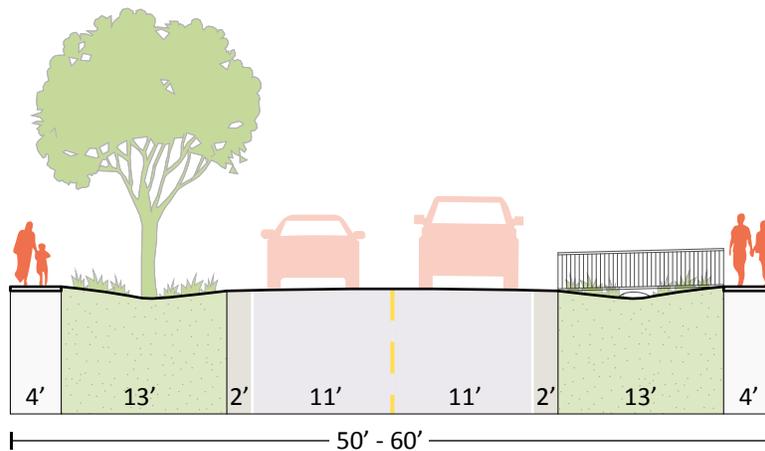
Figure 52 - Roadside Swale (Mithun)



Figure 53 - Roadside Swale (IPFS)



'B' Street (Long-Term)



Concrete Curb Ribbon Apron

Curbless streets often become unkempt with grass, weeds and dirt loosely defining the edge between roadway and shoulder. To maintain visual tidiness, a 18"-24" concrete curb ribbon may be used to transition from roadway to landscape. This straight curb type is often used along roads to prevent vehicles from crossing over into a pedestrian walkway or any outdoor landscaped area and provides a barrier between the roadway and amenity zone.



Figure 54 - Rolled Concrete Apron (Specify Concrete)

Roadside Swales

Fitting with the character, roadside swales are often seen along rural or small-town streets or highways. They can take many forms and be aesthetically pleasing or simply utilitarian. They help detain and direct stormwater runoff from the roadway and provide a layer of buffer between sidewalk and street. Swales can be personalized by adjacent property owners or fully planted and maintained by the City.



Figure 55 - Personalized Swale (City of Seattle)

Over-Swale Pedestrian Bridges

Small pedestrian bridges can connect the street to sidewalk at each property. Each can take on a slightly different design language or be required to adhere to set design guidelines. These bridges could be a unique contribution to a 'B' Street that features minimal design qualities.



Figure 56 - Bridge Over Swale (Chesapeake Dock)

Maintaining Small Town Charm

Each component from street, to swale, to narrow residential sidewalk works together to maintain a small town feel. No improvements should look over-engineered or oversized on Yorkville’s quaint neighborhood streets. These ‘B’ Streets should feel like a place where kids can play in the street and parents can take a quiet nighttime stroll.

- 1 Resurfaced Travel Lane
- 2 Flood Tolerant Street Trees
- 3 Continuous Swale Along N/S Streets
- 4 Bridge Over Swale
- 5 Private Property
- 6 Concrete Curb Ribbon Apron

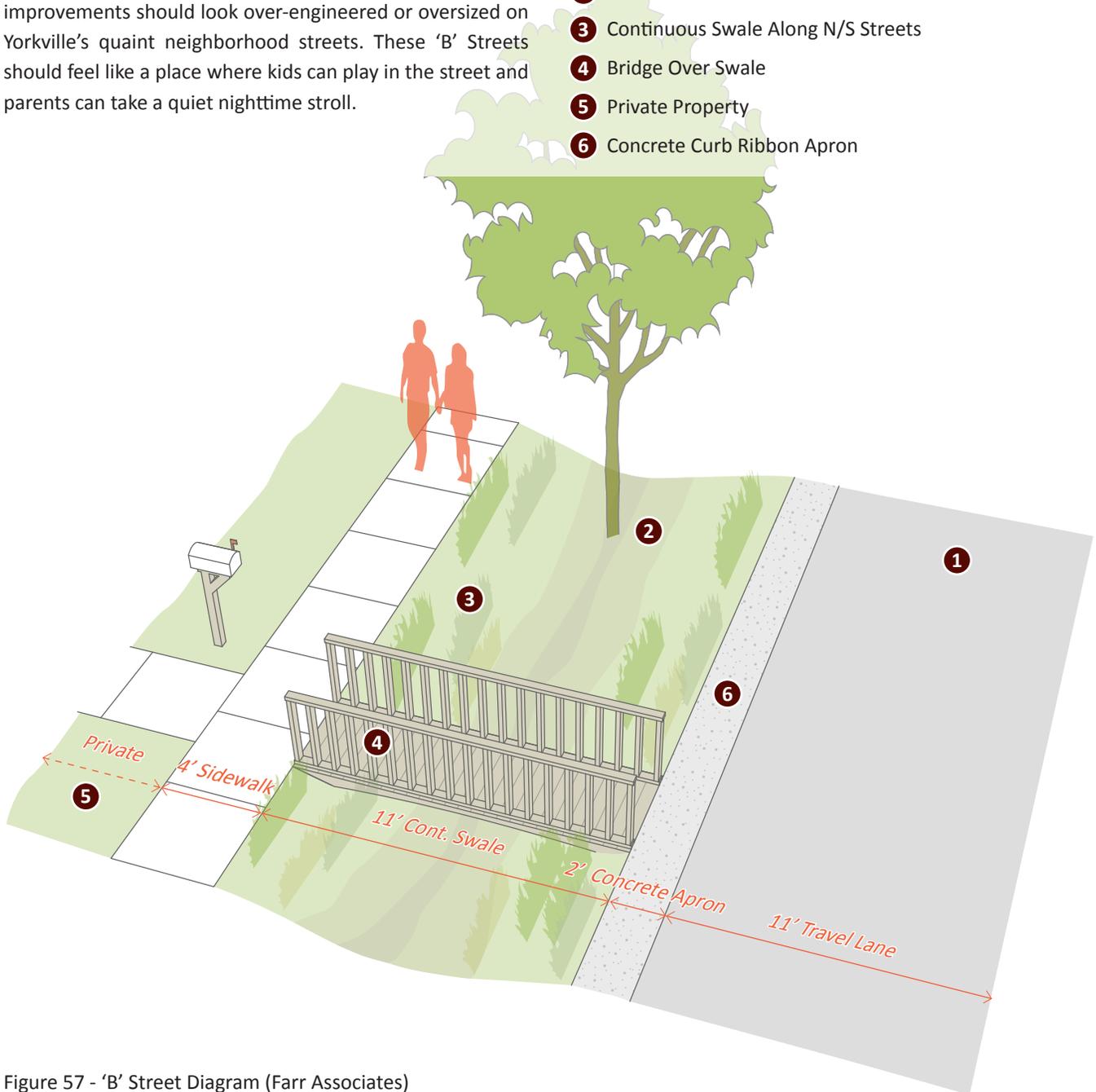


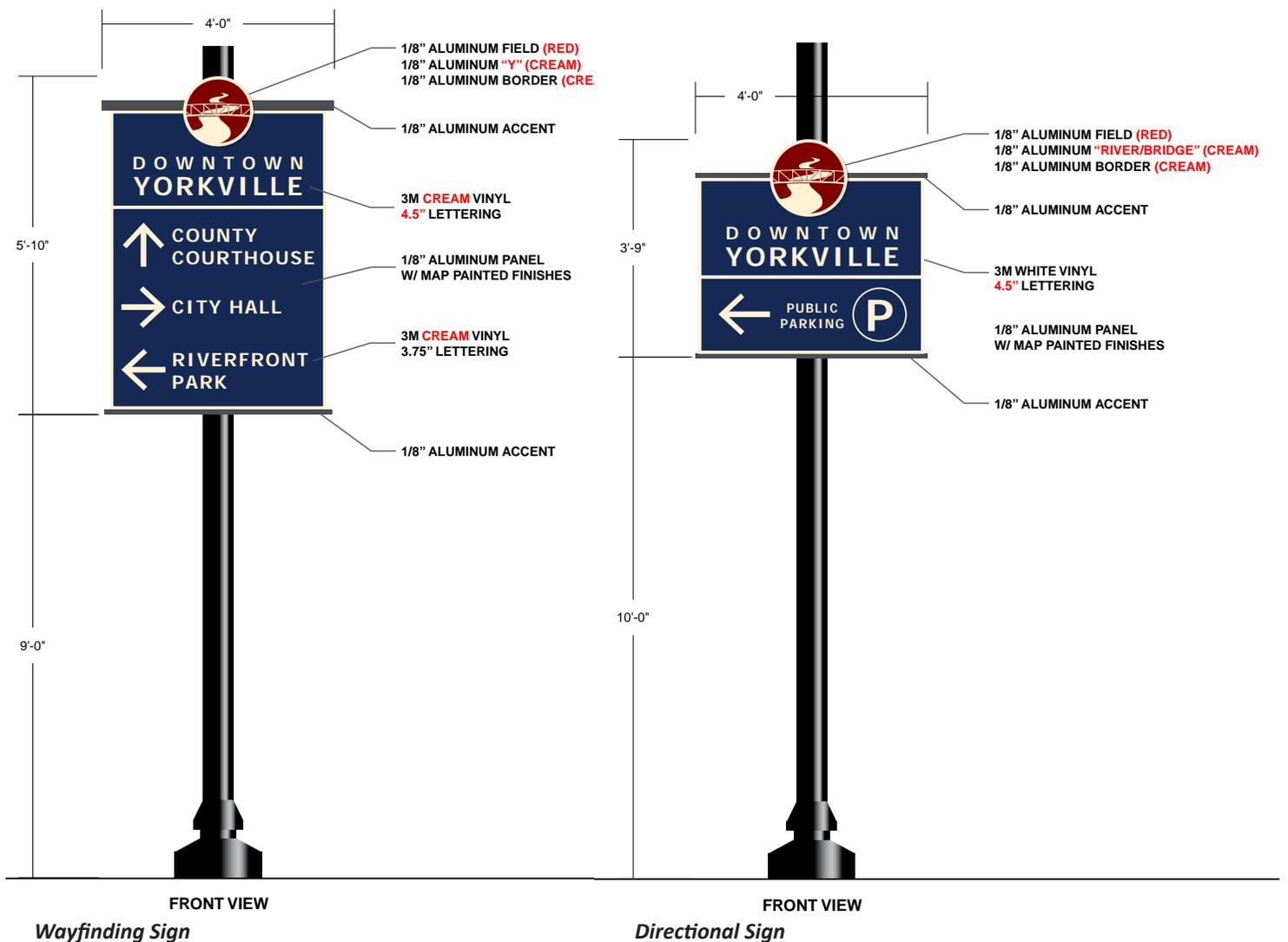
Figure 57 - ‘B’ Street Diagram (Farr Associates)

Downtown Signage and Wayfinding

Cohesive Downtown Signage

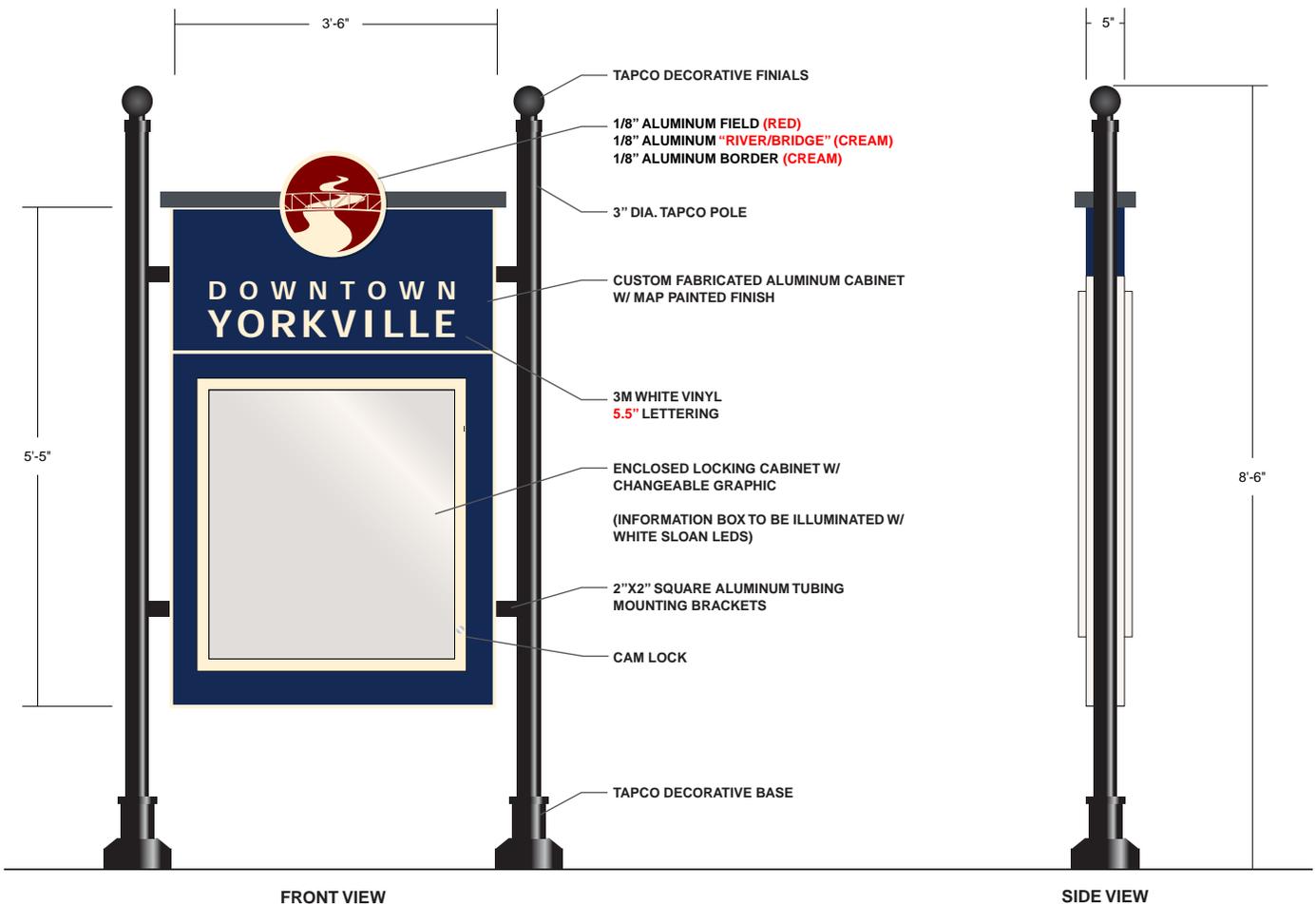
Effectively navigating downtown is one of the major elements to be improved. For example, there is currently sufficient parking to accommodate patrons of the downtown uses; however, that parking is dispersed unevenly throughout downtown and is difficult to locate unless you are familiar with the area. Situations, such as no left turns allowed at the Bridge Street and Hydraulic Street intersection, make it difficult to navigate if a turn is missed and an opportunity to redirect is not obvious.

One way to help alleviate this problem is through providing downtown signage. This signage can make a statement and solidify a cohesive brand and image for the area as a whole. From signage to banners or emblems, consistent, beautiful signage shows visitors that the city cares about providing an inviting user experience.





Seasonal Banners



Information Kiosk

Downtown Wayfinding

Starting with three different wayfinding sign types: (1) an informational kiosk; (2) pole mounted directional signage; and (3) branded wayfinding signage; Yorkville can make a near-term impact on how users navigate the area.

Informational kiosks can house either rotating or stationary content, such as maps, historic markers, or recreational programming. Maps can help pedestrians identify where they are in relation to other nearby destinations and help them discover new places to explore. These kiosks will be sidewalk mounted and cater to the pedestrian over vehicles.

Directional signage is most effectively mounted on the existing light poles in locations that help drivers know when to turn for parking or riverfront access. The directional signage should have a specific purpose of getting someone from point A to point B. Though these signs should be pole mounted and high enough to be visible for passing cars, they should also be easily visible to pedestrians.

As with each type of sign, wayfinding signage should be both branded and informative, letting users know that they are located in the proper area or neighborhood and provide markers for points of interest. Points of interest within downtown Yorkville may include specific restaurants, the historic Kendall County Courthouse, County offices, Bicentennial Riverfront Park, and public parking lots.



Figure 58 - Informational Kiosk (Trans Associates)



Figure 59 - Directional Signage (Flickr)



Figure 60 - Branded Wayfinding Signage (Google)



- Key**
- i Kiosk
 - P Parking
 - + Wayfinding

Downtown Signage



Lighting Strategies

Implement Lighting Best Practices

Lighting is a key component of walkability, as it lends itself to creating a more safe environment for pedestrians. There are several streets where street lighting exists, but the lighting is inconsistent and is not always human-scaled. Lighting in the downtown can serve multiple functions, including branding the area, creating more vibrancy, and increasing safe conditions for residents and visitors. The increase and consistency of lighting gives people a sense of street character and trust of what is to come, and encourages walking and biking.

Nancy Clanton of Clanton & Associates has provided lighting best-practices for municipalities by creating some “dos and don’ts” for street and facade lighting. In general, well-designed lighting strategies should not only light the area, but take into account all view angles while creating a beautiful ambiance without glare and annoyance. The list on page 45 expands on Nancy Clanton’s strategies.

High color temperature (CCT) light sources have the highest concentration of blue light. Many municipalities are limiting the CCT of their street and pedestrian lighting to 3000K or less which is similar to the color of the setting sun. Dimming or turning off lighting is another strategy to reduce over-lighting an area.

Ideally, light sources should change color over the course of the evening and into the late night. Blue light will enhance visibility during the rush hour, but as vehicular and pedestrian traffic decreases at night, the spectrum can minimize blue light and switch to the red range.

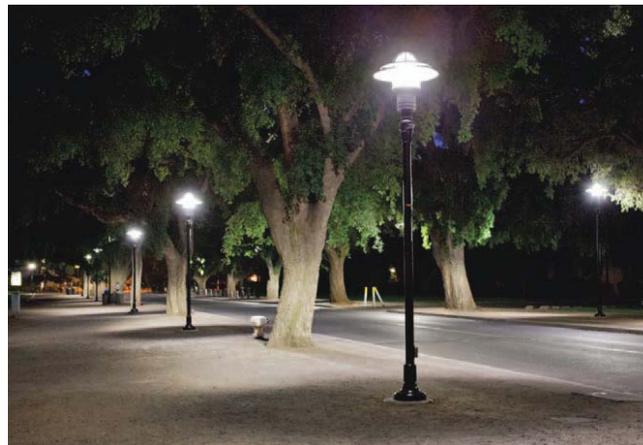


Figure 61 - Street Lighting “Do” (American City and County)



Figure 62 - Building Lighting “Do” (Houzz)



Figure 63 - Public Space Lighting “Do” (Google)



Figure 64 - Street Lighting “Don’t” (Farr Associates)



Figure 65 - Building Lighting “Don’t” (Farr Associates)



Figure 66 - Public Space Lighting “Don’t” (Google)

Lighting “Dos”

Street Lighting

- Light sidewalks and streets appropriately for the neighborhood and explicitly where needed
- Use low-glare streetlights
- Minimize uplight of all kinds (cobra lights, etc.)

Building Lighting

- Mount light at the top of facades and aim inward
- Emphasize architectural features such as columns and arches using beam distributions
- Use warm-colored, dim light

Public Space Lighting

- Define the space with lighting of a consistent type and brightness
- Dimly light the features where people gather: seating areas, outdoor dining, public parks and plazas

Lighting “Don’ts”

Street Lighting

- Use streetlights to light yards and private property
- Overlight
- Use high-glare streetlights

Building Lighting

- Aim across a visual path with a floodlight
- Aim up a façade
- Overlight
- Select glaring luminaries

Public Space Lighting

- Light individual elements unless they are features
- Flood the public space with heavy lighting
- Appear messy and unorganized
- Include multiple types of lighting accomplishing the same goal

Stormwater Management

Green Infrastructure

It is widely understood that stormwater runoff from roofs, pavement, and other urban surfaces are contributing to water body degradation and flooding. Minimizing this impact, particularly in urban contexts, is often difficult because of the amount of land dedicated to dense buildings, supportive surface parking lots, and wide streets with generous sidewalks; however, there are strategies to accommodate both the urban and green infrastructure solutions.

Yorkville has been a settlement for a long time - even longer than many other cities and villages in the greater Chicago area. The Fox River was the main reason for locating Yorkville where it is today, so taking measures to preserve the water quality, beauty, and function of the river should be a priority. The City has old stormwater infrastructure, so removing some of the burden would help to lengthen its useful life. As capital improvements occur, there is an opportunity to do two things: 1) updated the stormwater infrastructure and 2) construct green infrastructure in the public rights-of-way to reduce potential negative impacts of storm surges and surface runoff degradation the Fox River. Strategies such as rain gardens, bioswales on the sloped streets, or even green roofs could all contribute to beauty and utility.

Signage Encouraging Awareness

Green infrastructure is not familiar to everyone, so these can be excellent educational opportunities for the public. Signage and story-telling of why the City is placing rain gardens next to sidewalks and bioswales cascading down slopes could encourage residents to invest in doing the same on their properties. Additionally, the City could incentivize rain gardens or rain water cisterns for private residences to minimize negative impacts on natural resources and reduce flooding.



Figure 67 - Residential Rain Garden (Metro Blooms)



Figure 68 - Rain Garden Off Curbless Street (Prairie Rivers)



Figure 69 - Rain Garden Signage (City of Springfield, MO)



Figure 70 - Pervious Pavers (Techniseal)



Figure 71 - Pervious Pavers (MWMO)



Figure 72 - Paver Laying Machine (Detroit News)

Pervious Pavement

Permeable paving products, such as porous asphalt and concrete, as well as permeable pavers allow water to pass through the surface and into a stone storage layer below.

The water stored in the stone layer either infiltrates into the soil below or is slowly released to a sewer or other drainage system to reduce stormwater runoff volumes and rates. Sediment, metals, and organic compounds are filtered and/or biologically treated as the runoff moves through and is stored in the system.

Properly designed permeable paving systems are applicable to both pedestrian and vehicular areas. Permeable paving should be avoided in the through lanes of high traffic areas (such as County and State highway routes) and areas of high sediment or other pollutant loading that could clog the system or overwhelm the system's ability to treat typical urban runoff pollutants.

Ideal locations for pervious pavement might be Hydraulic Street, parallel parking lanes, mid-block alleys, and surface parking lots.

Installing Pervious Pavers

Pavers appear laborious to lay down in large quantities, such as a street or sidewalk; however, technology advancements have bred paver laying machines that lay down entire swaths of pavers in one motion. This drastically minimizes installation times, but still provides that hand-laid, classic look that is often loved for its character. Pavers can be cost competitive to concrete, because of the external impact it has on sizing stormwater infrastructure systems.

Public Art and Sculpture

The Role of Public Art

Yorkville's current downtown does not compete well with some of the loved downtowns nearby, such as Oswego and Plainfield; however, it should not need to directly compete. Yorkville can distinguish itself with its own identity and unique character as a community. There are few better ways to do this than through public art.

First and foremost, public art is free. Anyone can experience and enjoy it. It also adds a layer of uniqueness when so many downtowns attempt to emulate each other and therefore lose some of their authenticity. There are plenty of opportunities for public art, from blank walls on the sides of buildings, to vacant parcels and parking lots, and even the grain elevator. Each can become a canvas for community expression.

The City should seek to commission works in addition to allowing community members to contribute pieces to the collection through interactive events for residents of all ages. When everyone can get involved, there is more of a sense of ownership over the end product. Public art is a medium to show visitors what Yorkville is all about.

Make it Interactive!

Not only can art be something to experience visually, but it can also be interactive. This can be done through sculptures that encourage climbing or provide a backdrop for a photograph. Art can cater to children and adults alike and should remain informal enough to fit into Yorkville's beloved small-town character.



Figure 73 - Wall Mural on Blank Wall (City of Missouri City)



Figure 74 - Interactive Sculpture (ISU College of Design)



Figure 75 - Temporary Installation (Playscapes)



Key

- Wall Mural on Blank Wall
- ⬡ Interactive Sculpture
- ▲ Temporary Installation

Potential Public Art Locations



(Page intentionally left blank)

Appendix

Supplemental Content

Context Scale Analysis Mapping	p. 52
Site Scale Analysis Mapping	p. 60
Website Survey Results	p. 68

Downtown Overlay Context Scale

Downtown Overlay District

The Downtown Overlay District context scale includes both the defined downtown from the 2016 Comprehensive Plan and the immediate surrounding areas, which includes the north banks of the Fox River. The following series of analysis maps reveals that downtown Yorkville has a variety of commercial, industrial, and public land uses surrounded by primarily lower density single-family housing. The underlying zoning allows for a more intense development pattern than currently exists; therefore, downtown has potential to densify and redevelop to more intense land uses. With relatively high traffic counts along Bridge Street, downtown experiences crosstown traffic that makes it visible and accessible by pedestrians and vehicles, and it could take advantage of higher traffic through non-residential uses.

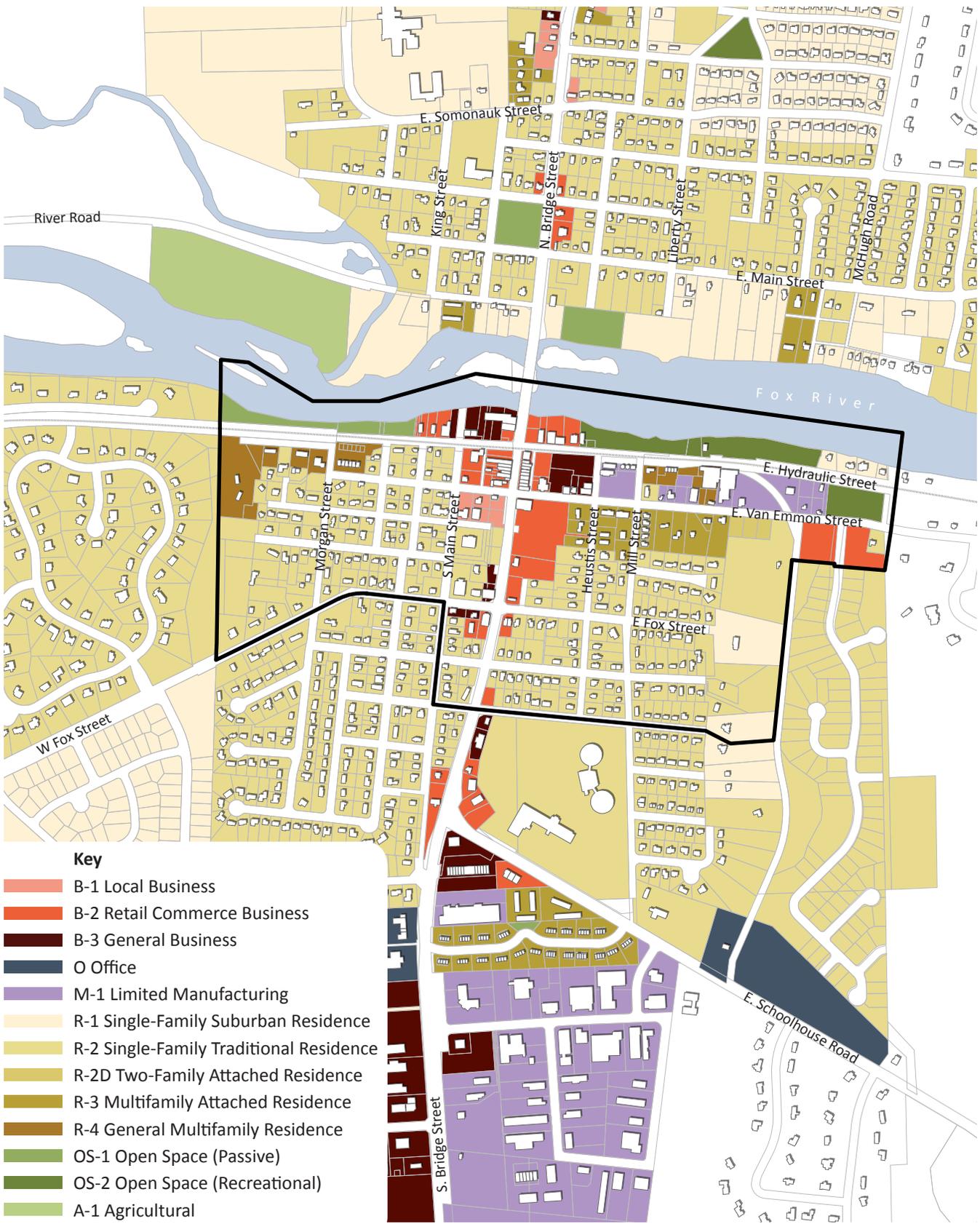
A TIF 1 and 2 are current and future strategies that seek to encourage reinvestment in the downtown. Though the TIF boundaries are not consistent with the downtown boundary defined in the map, it does include much of the downtown land and parcels along the Fox River. Parcels along the Fox River may be the most attractive to investors because of the views towards the river and access to recreation.

Investment in public space along the river has helped provide a destination for visitors and residents alike. Additionally, recreational bicycle facilities, trails, and connections to the greater trail system offer opportunities to improve quality of life and may become a catalyst for future real estate investment.



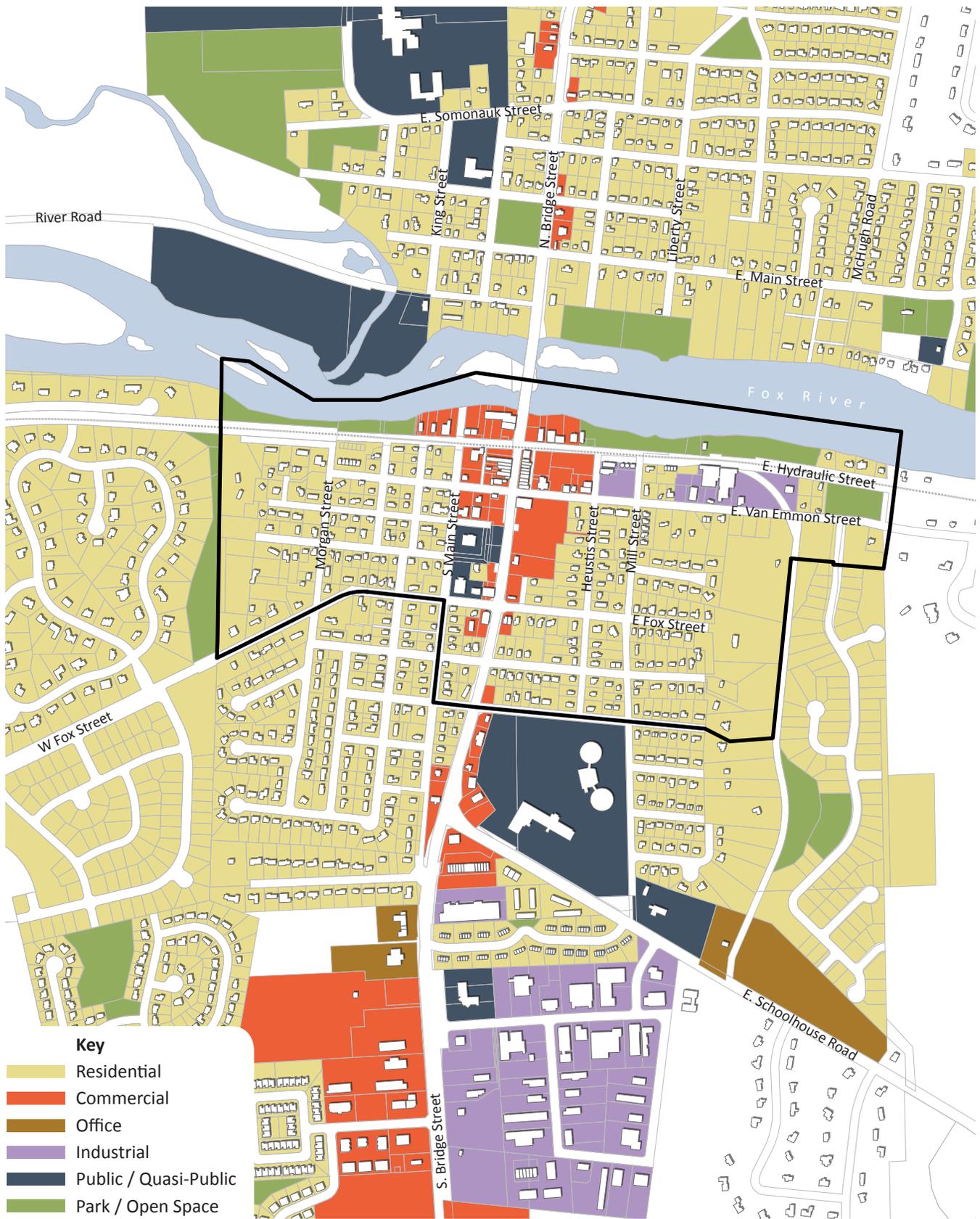
Area Scale Map (Aerial)





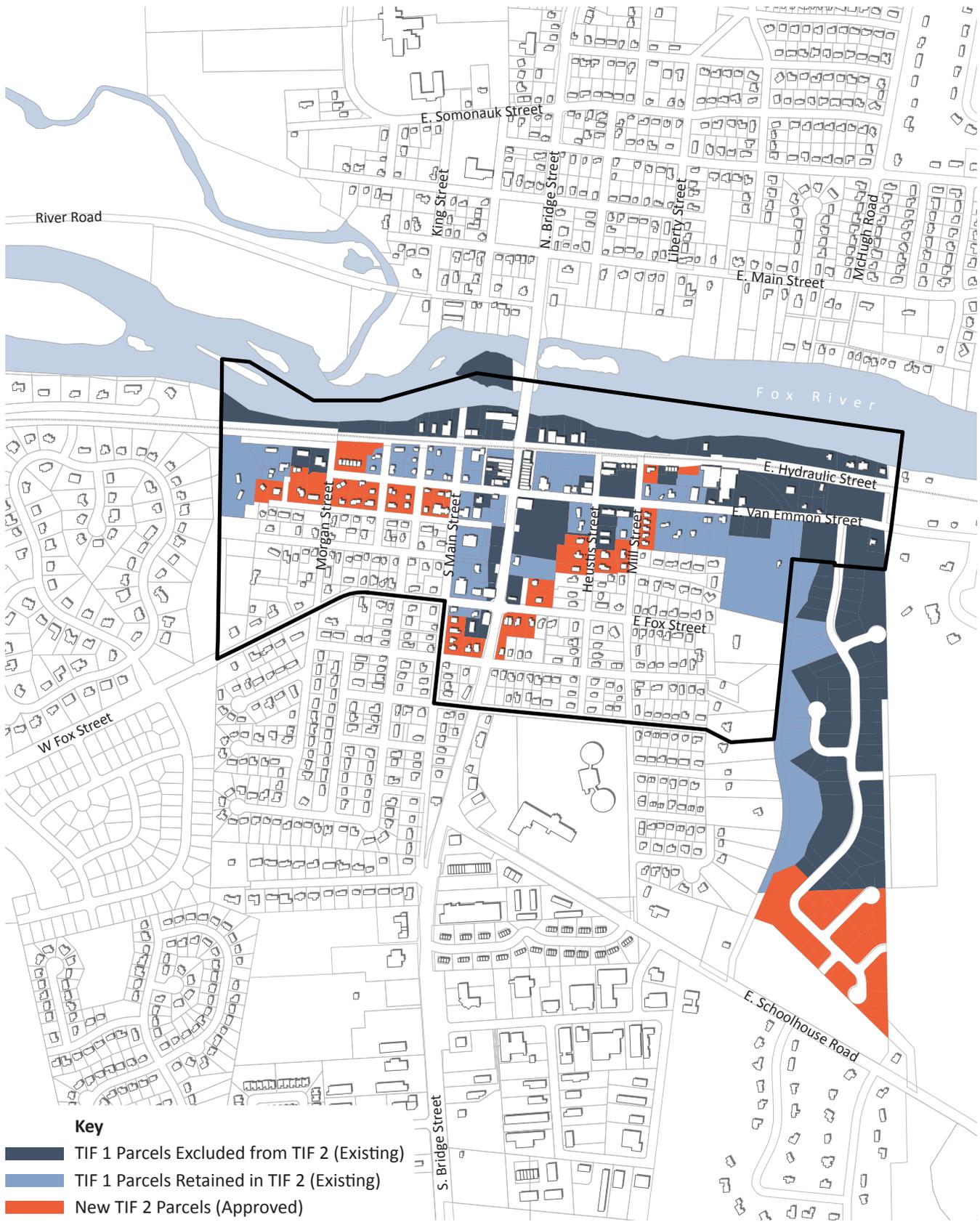
Existing Zoning Classifications





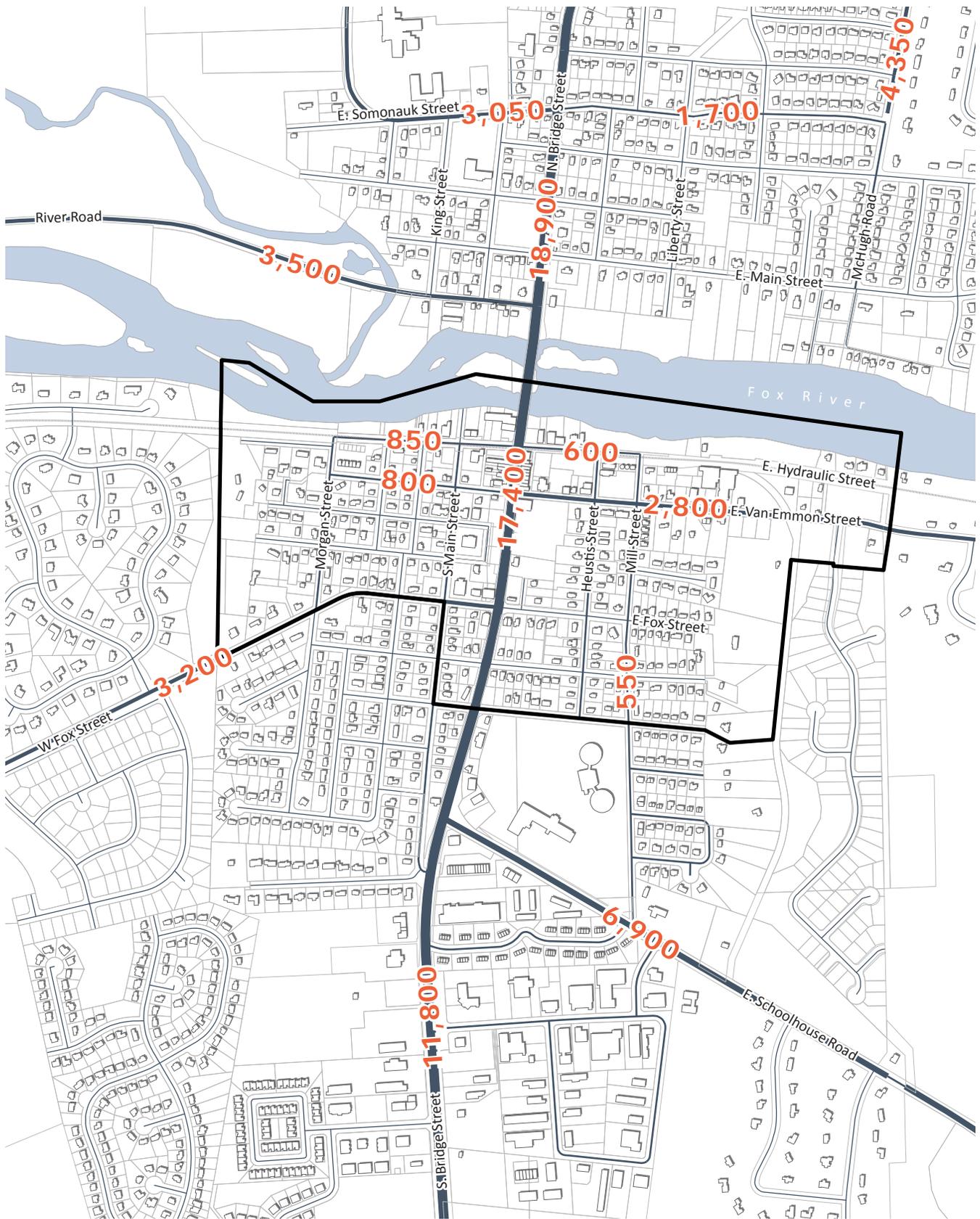
Existing Land Uses





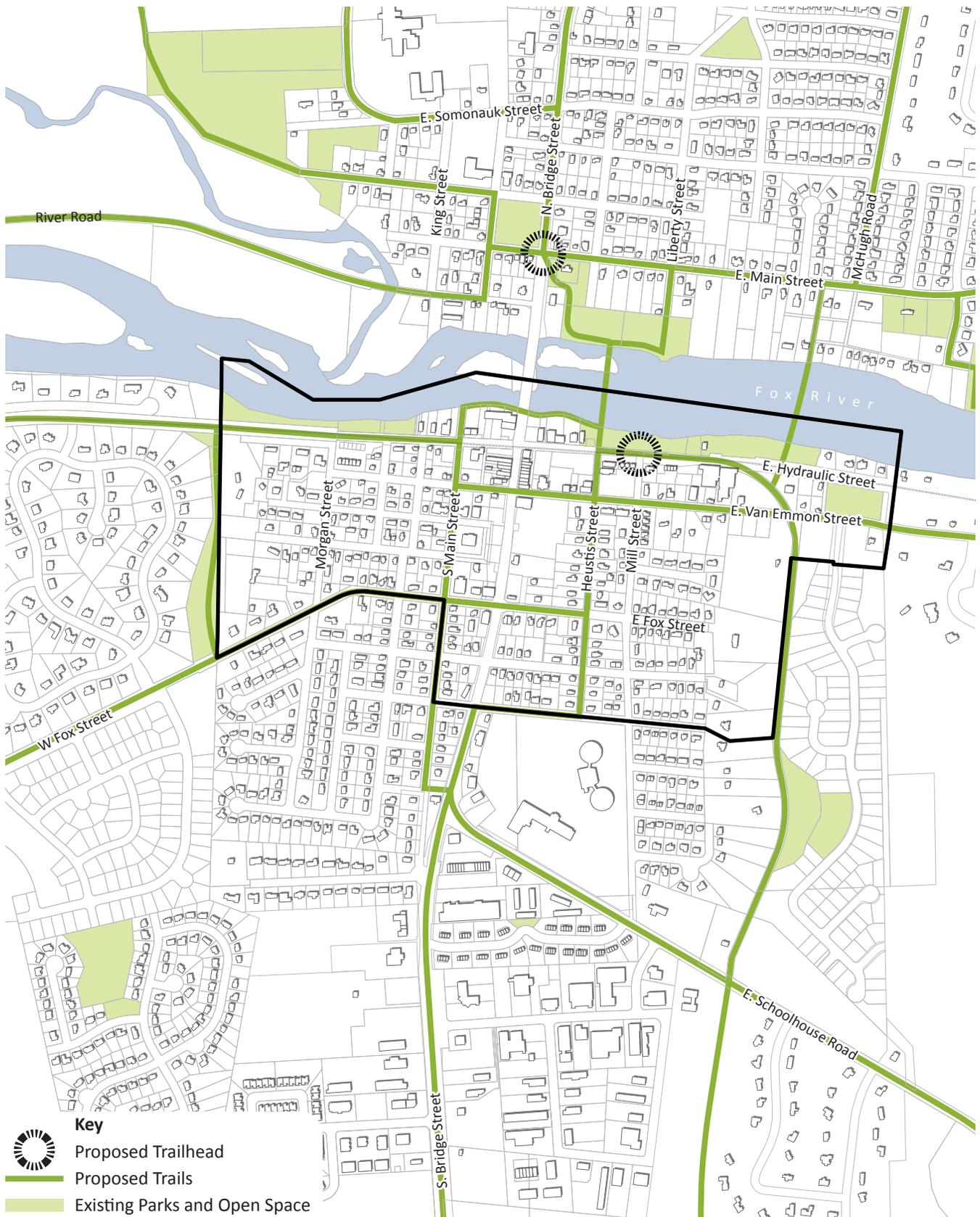
Existing TIF Boundaries



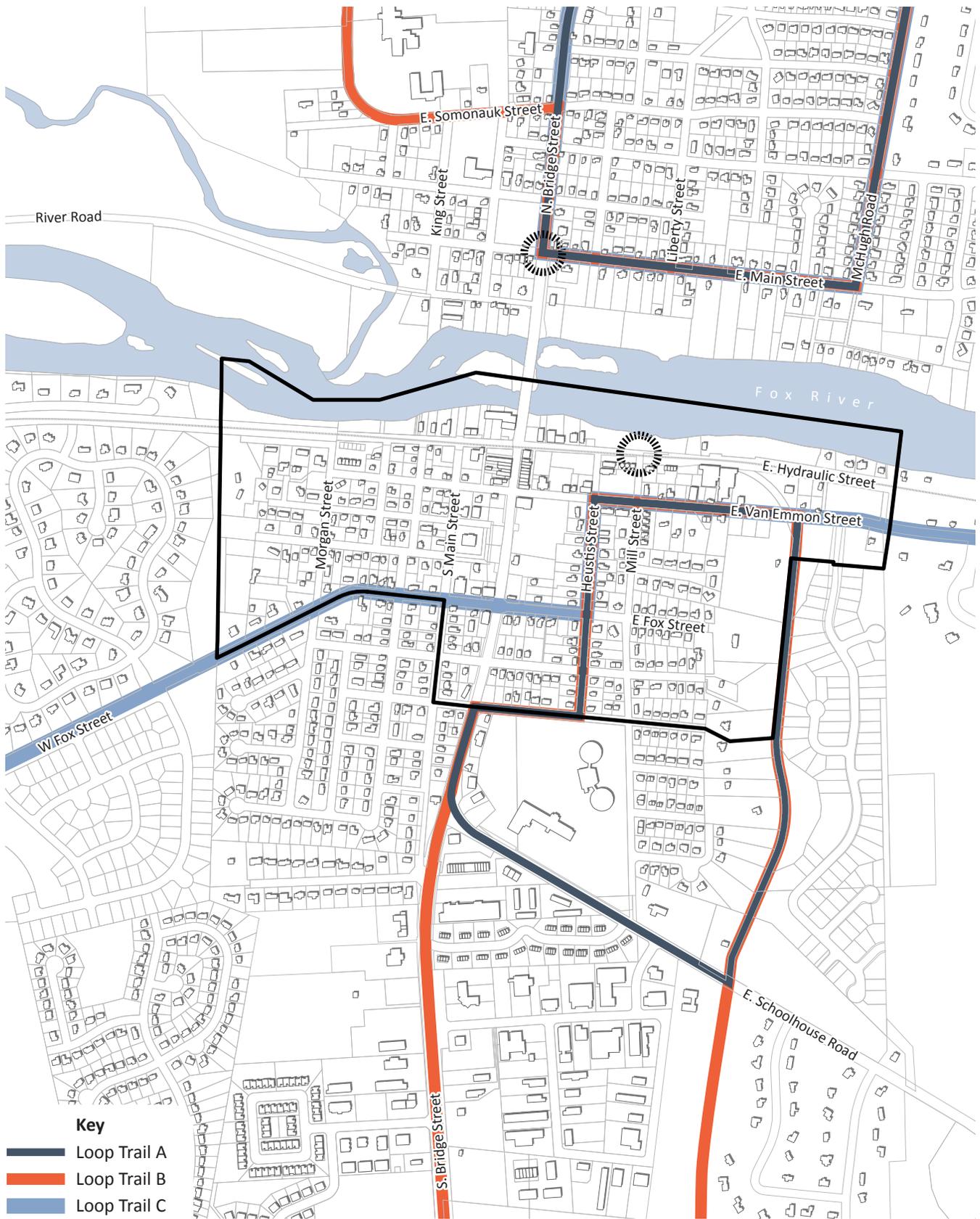


Average Annual Daily Traffic (AADT)





Parks and Proposed Trail Network



Proposed Loop Trail System

Downtown Overlay Site Scale

Downtown Overlay District

The Downtown Overlay District site scale includes most of the downtown as defined in the 2016 Comprehensive Plan, as well as the immediate areas to the north and south. The following series of analysis maps illustrates that downtown Yorkville features a walkable grid; however, certain factors are minimizing its effectiveness. Notable contributing factors include segments of non-continuous sidewalks, primary building entries being oriented towards parking lots, and blocks that feature highly visible surface parking.

The primary stretch of downtown is defined by buildings that are built to the sidewalk in a continuous row along Bridge Street; however, Bridge Street's priority as a truck route has added to an anti-pedestrian feel. With only a small stretch of downtown being defined by urban building types, walkability is limited.

There are few buildings taller than two-stories, which reduces visual cues that downtown Yorkville is in fact a downtown. Some exceptions to this include the historic courthouse and grain elevator, which provide a unique sense of place. Large, vacant, or underutilized parcels are ripe for redevelopment and, with generous underlying zoning, should be attractive investments with modified guidelines.



Area Scale Map (Aerial)





Key

- State-owned
- County-owned
- City-owned
- Private-owned

Parcel Ownership





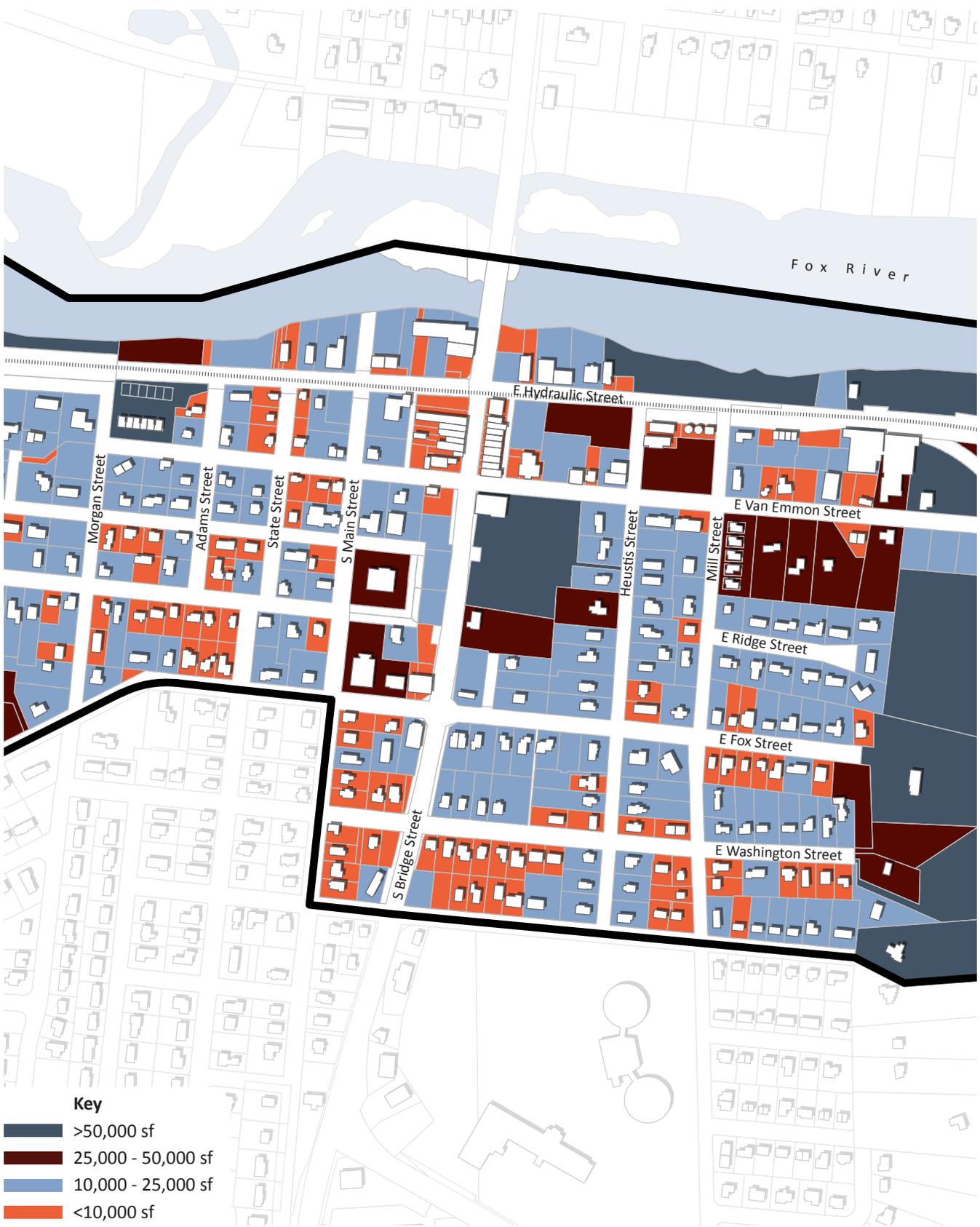
Commercial Buildings & Frontages







ROW Width and Ownership



Parcel Area





Building Heights

Website Survey Results

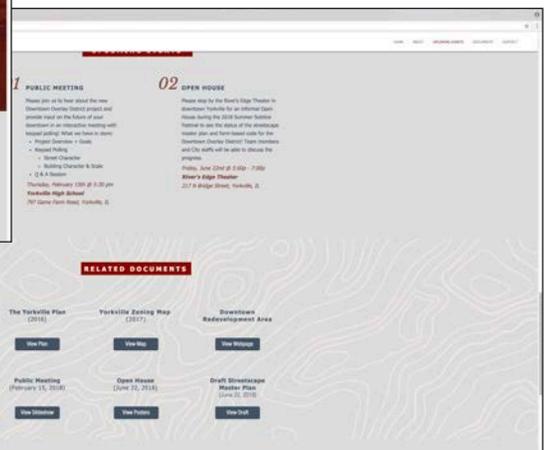
www.downtownyorkville.com/

The Yorkville Downtown Overlay District website survey was posted between February 19, 2018, and March 12, 2018. The survey gathered **473 responses**. The survey participants were concentrated in the 18 - 49 years old range. Key takeaways from the online survey include:

- A faster actual driving speed on Bridge Street is revealed
- People seem to feel safer than expected walking along Bridge Street; however, online comments conflict
- There is a perceived or real lack of parking
- Bridge Street is clearly important from a downtown image standpoint
- All gateway elements seem popular; however, the overhead lights are leading
- Bridge Street, Hydraulic Street, and Van Emmon Street are the focus
- The form-based code should not regulate architectural

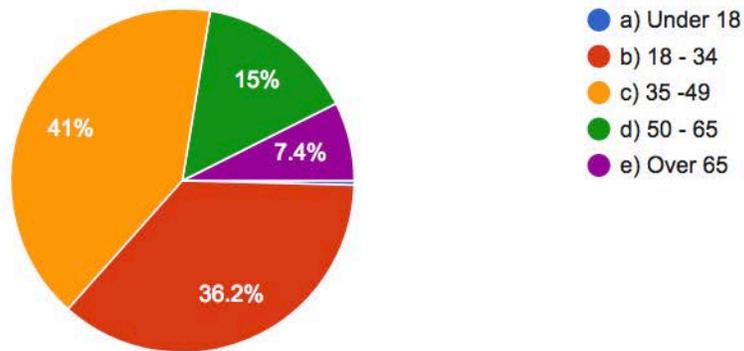
style to leave flexibility

- The “step back” question was difficult to comprehend in the survey format
- The front of Bridge Street needs a makeover



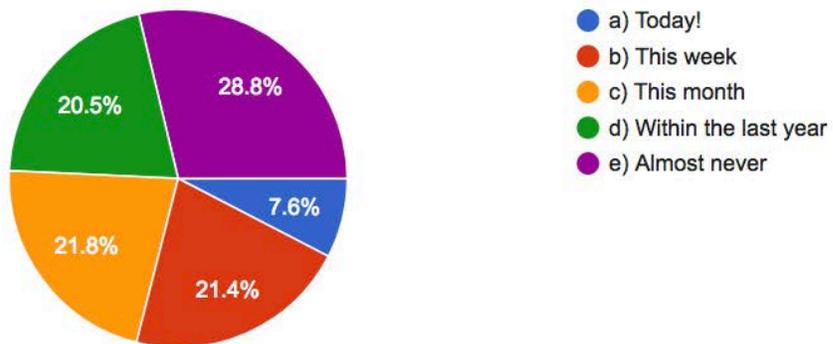
Question 1

Which is our age group?



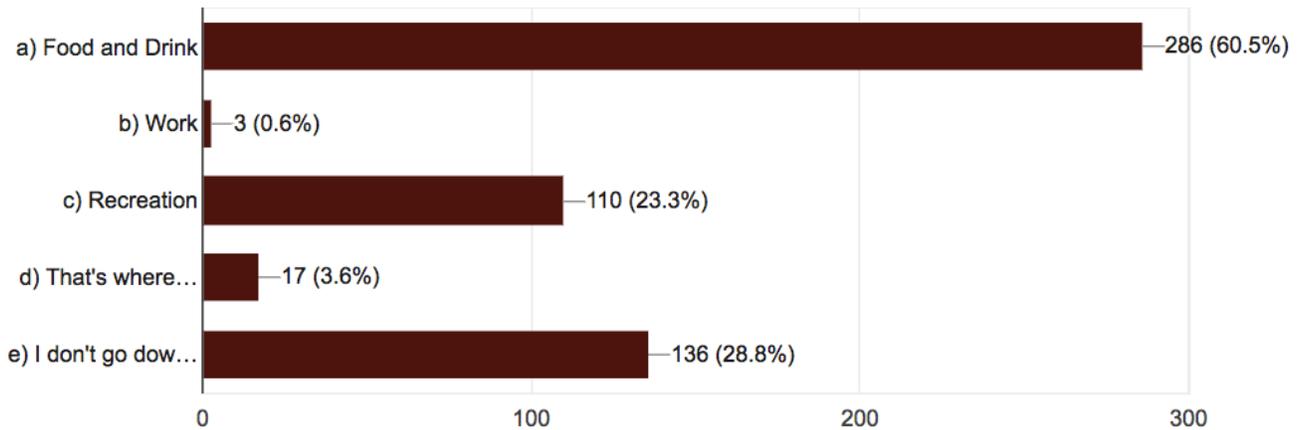
Question 2

When was the last time you visited downtown?



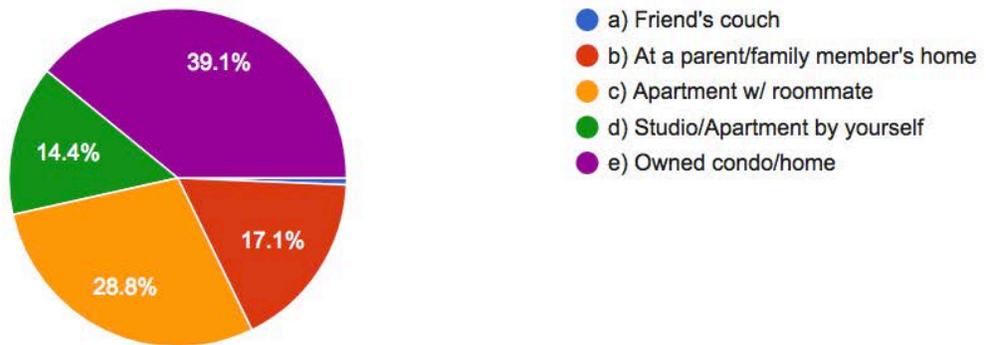
Question 3

What is the primary reason you come to downtown? (Check multiple)



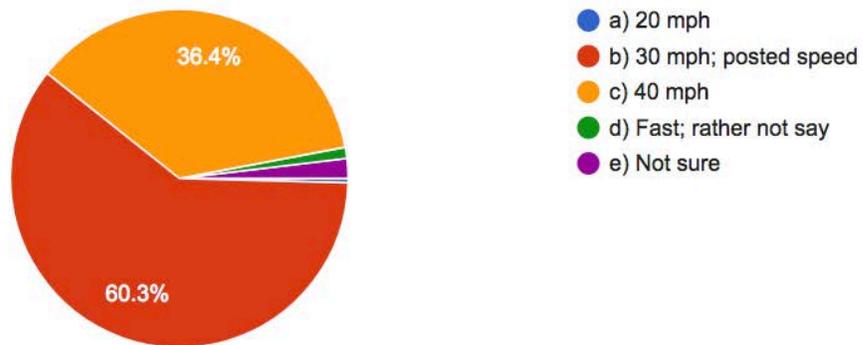
Question 4

Where was the first place you lived as an adult?



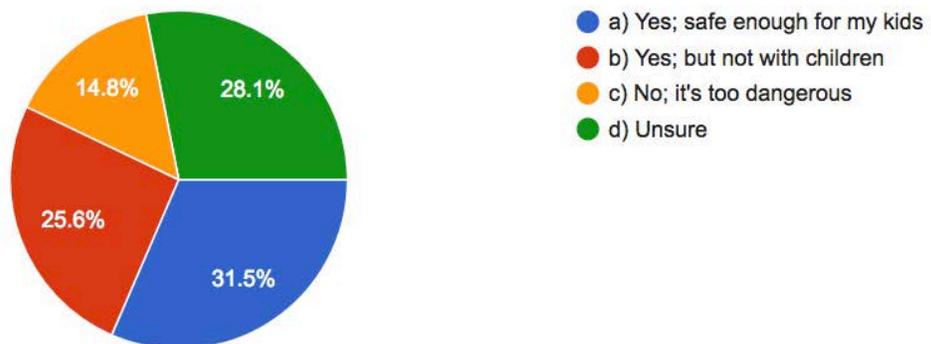
Question 5

How fast do you drive through downtown on Bridge Street?



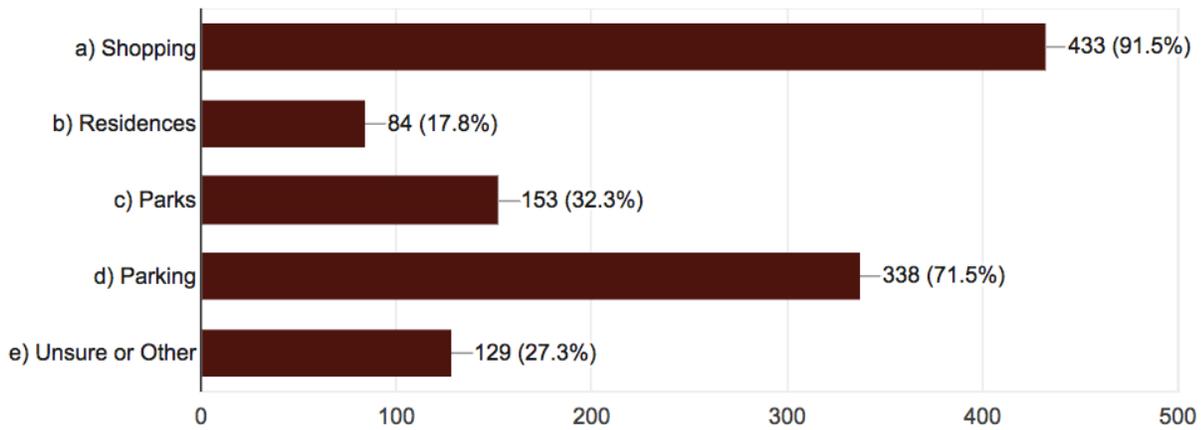
Question 6

Would you feel safe walking along Bridge Street?



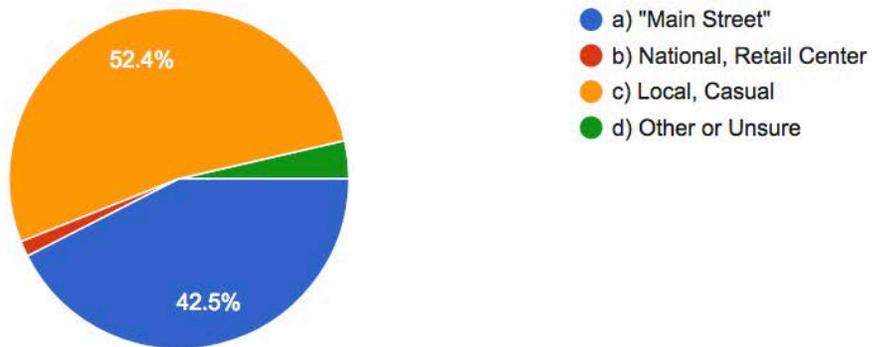
Question 7

What is missing most from downtown? (Check multiple)



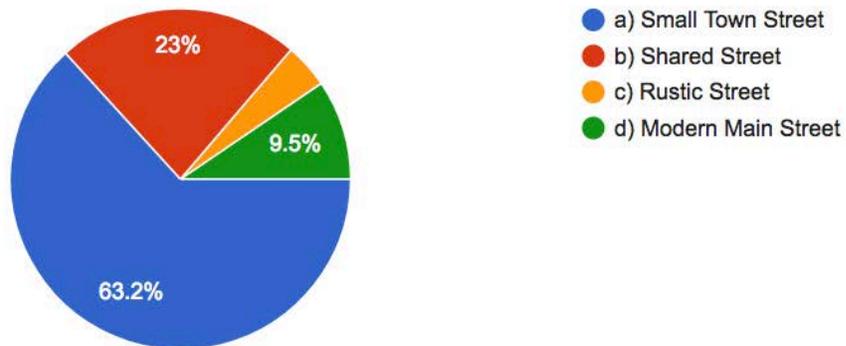
Question 8

Which image most represents your vision of downtown?



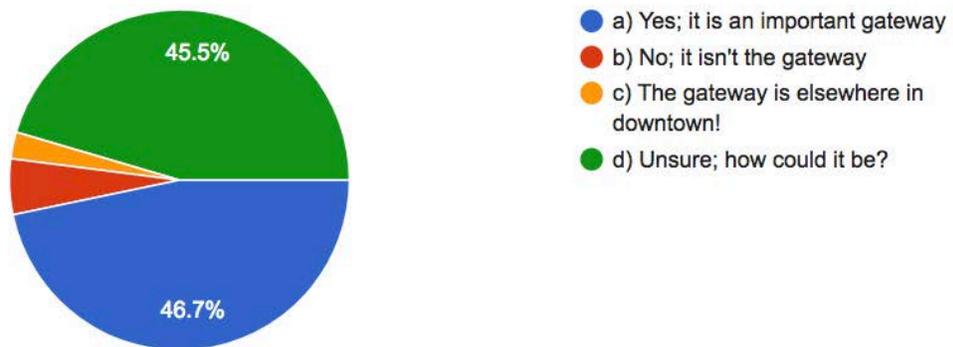
Question 9

Which street character do you prefer?



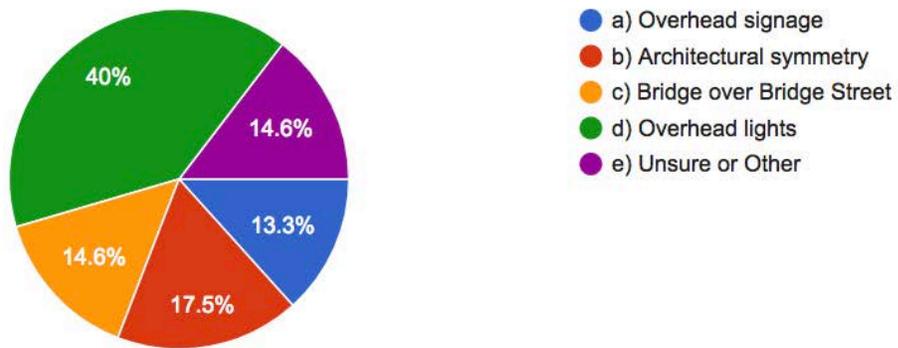
Question 10

Should Bridge Street be a gateway to downtown?



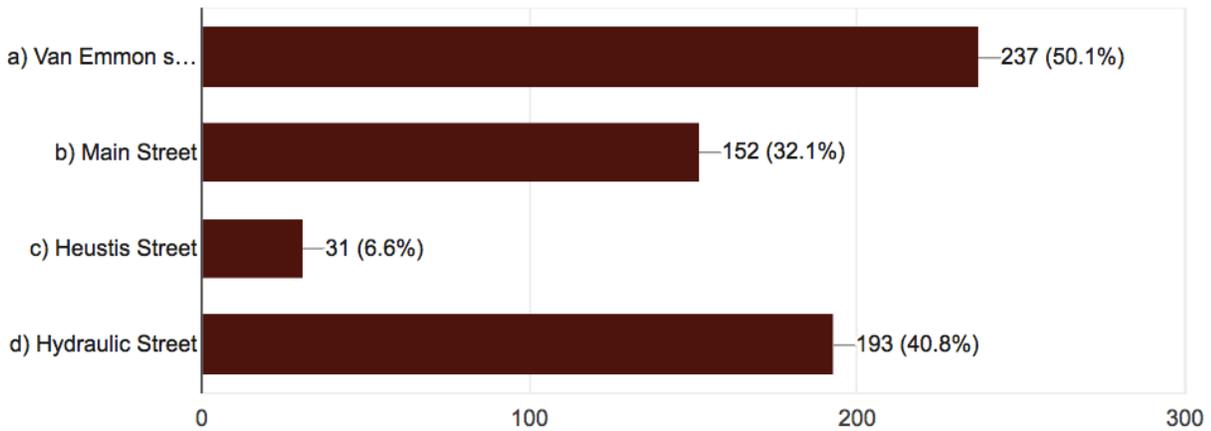
Question 11

Which is a preferred gateway element into downtown?



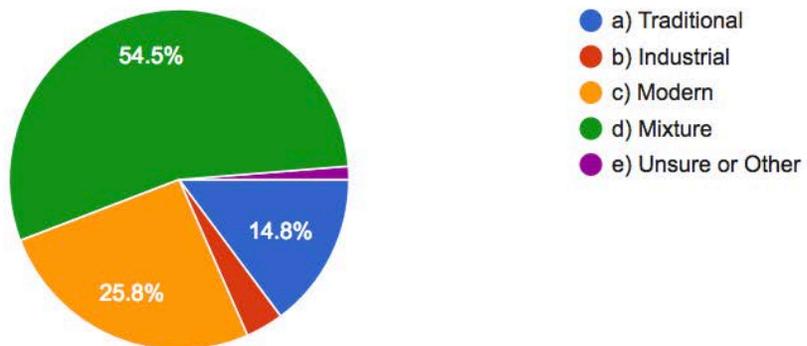
Question 12

Which street do you most consider an 'A' Street? (Check multiple)



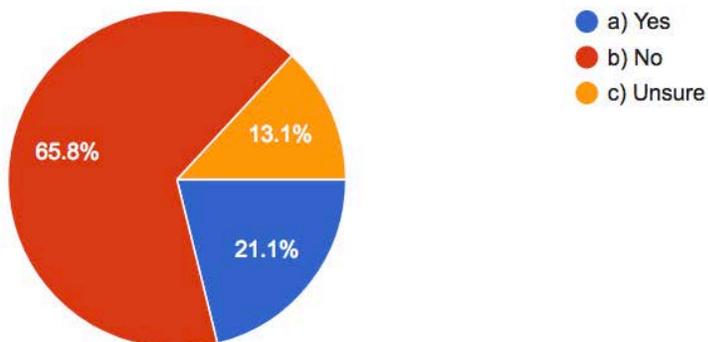
Question 13

Which architectural character do you prefer?



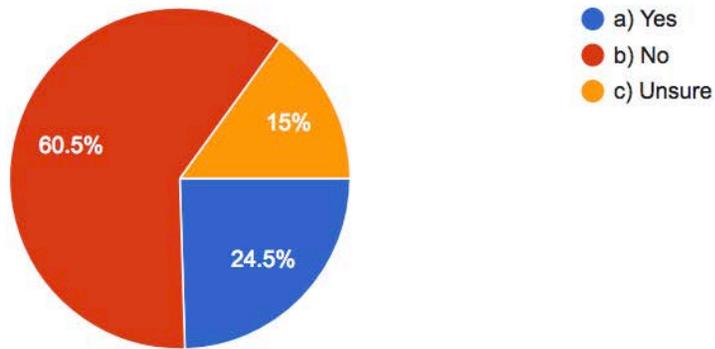
Question 14

Would attached housing be a good fit downtown?



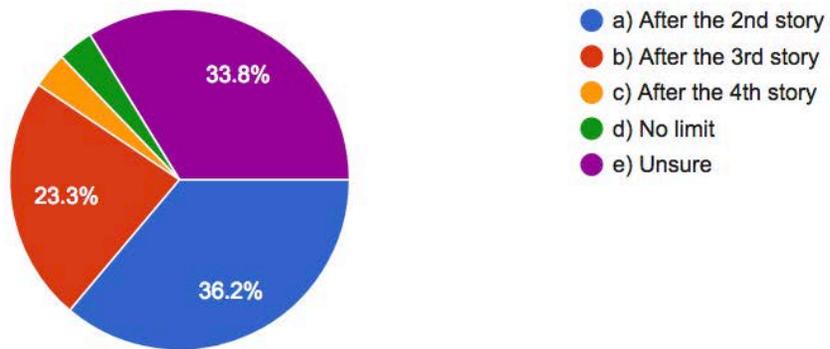
Question 15

Would multi-family housing be a good fit downtown?



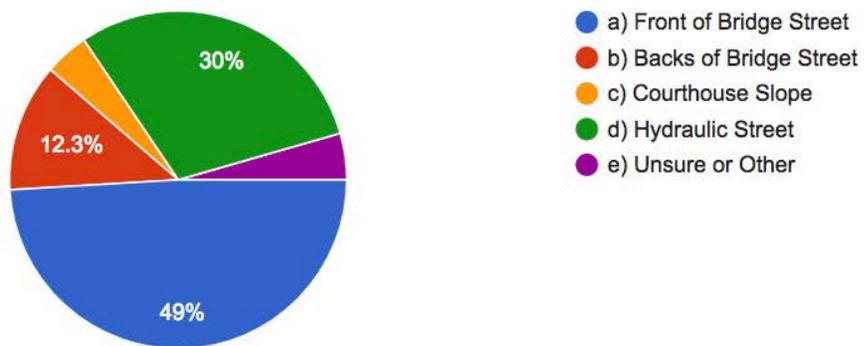
Question 16

After how many stories should upper levels "step back" from the facade?



Question 17

Which place would you invest in first?



(Page intentionally left blank)



RIVERS
EDGE
COMING SOON-ISH
DRINK LAB
AND
PROVISIORS
THEATRE
★★★★

RIVERS EDGE THEATRE

RIVERS EDGE THEATRE

219
RIVERS
EDGE



YORKVILLE

Downtown Overlay District

Form-Based Code



United City of
Yorkville^{ILLINOIS}

FARR ASSOCIATES

Acknowledgements

United City of Yorkville
Farr Associates
Former Mayor Gary J. Golinski
Bart Olson, *City Administrator*
Erin Willrett, *Assistant City Administrator*
Krysti Barksdale-Noble, *Community Development Director*
Jason Engberg, *Senior Planner*
Eric Dhuse, *Public Works Director*
Lisa Pickering, *City Clerk*
Richard T. Hart, *Chief of Police*
Tim Evans, *Director of Parks and Recreation*
Brad Sanderson, EEI, *Engineering Consultant*

City Council

Mayor John Purcell
Joel Frieders, *Alderman*
Chris Funkhouser, *Alderman*
Ken Koch, *Alderman*
Jacquelyn Milschewski, *Alderman*
Jason Peterson, *Alderman*
Arden Joe Plocher, *Alderman*
Seaver Tarulis, *Alderman*
Daniel Transier, *Alderman*

Planning and Zoning Commissioners

Randy Harker, *Chairman*
Reagan Goins, *Commissioner*
Debra Horaz, *Commissioner*
Don Marcum, *Commissioner*
Jeff Olson, *Commissioner*
Richard Vinyard, *Commissioner*
Daniel Williams, *Commissioner*



United City of
Yorkville ILLINOIS



Table of Contents

10-21-1 Introduction

- 1.A. Title
- 1.B. Intent
- 1.C. Overview of the Code
- 1.D. Applicability
- 1.E. How to Use The Code
- 1.F. Development Approval Process
- 1.G. Nonconforming Structures
- 1.H. Definitions

10-21-2 Districts

- 2.A. S1: Bridge Street District
- 2.B. S2: Hydraulic Street District
- 2.C. S3: Van Emmon Street District
- 2.D. S4: 'B' Street District
- 2.E. S5: 'B' Street - Residential District
- 2.F. Hierarchy of Districts
- 2.G. Primary Streets

10-21-3 Uses

- 3.A. General Provisions
- 3.B. Permitted Use Table
- 3.C. Use Requirements

10-21-4 Building Types

- 4.A. Introduction to Building Type Standards
- 4.B. Explanation of Building Type Table Standards
- 4.C. Downtown Commercial Building Type
 - Siting
 - Height & Use
 - Facade & Roof
- 4.D. Downtown Living Building Type
- 4.E. Cottage Commercial Building Type
- 4.F. Civic Building Type
- 4.G. Attached Building Type
- 4.H. Yard Building Type
- 4.I. Entrance Types
- 4.J. Roof Types
- 4.K. Additional Design Requirements
 - Materials and Color
 - Windows, Awnings, and Shutters
 - Rear Parking Facade Design
 - Balconies

10-21-5 Site Development Standards

- 5.A. Signage
- 5.B. Parking
 - Applicability
 - Requirements (#)
 - Multiple Use Reductions
 - Credits
 - Bicycle Parking
 - Access
- 5.C. Landscape
- 5.D. Street Guidelines
- 5.E. Temporary Structures

DRAFT

10-21-1 Introduction

A. Title

This Article 10-21 shall be known, cited, and referred to as the Downtown Overlay District code. Any reference to this Article following its effective date shall mean this entire Article as it may hereafter be amended.

B. Intent

It is the intent of this Article to provide development standards to the United City of Yorkville for downtown and adjacent redevelopment areas that promote public health, safety, and general welfare of the community, including, but not limited to the specific purposes set forth below.

1. To guide the development of a mix of uses and a pedestrian-oriented environment as established in the United City of Yorkville 2016 Comprehensive Plan, adopted September 2016, and the 2019 Yorkville Downtown Overlay District Streetscape Master Plan.
2. To provide for a mix of housing types within the Downtown Overlay District and adjacent areas for people of all ages and lifestyles.
3. To achieve development that is appropriate in scale and intensity for the Downtown Overlay District and adjacent neighborhoods.

C. Overview of the Code

1. **Section 10-21-2: Districts.** These regulations are organized within street types for adoption into the City's existing code. These zoning districts shall be mapped on the City's Zoning Map; however, the Primary Streets designation shall be referenced from Figure 10-21-2G (4). The following Districts are established for mixed use, commercial, and residential development within downtown and adjacent redevelopment areas. Figure 10-21-1B (1) illustrates the locations for the districts.

- S1: Bridge Street District
- S2: Hydraulic Street District
- S3: Van Emmon Street District
- S4: 'B' Street District
- S5: 'B' Street - Residential District

3. **Section 10-21-3: Uses.** Use requirements are defined in Section 10-21-3 for each of the Street Districts. Uses may also be further limited by the Building Types. Refer to Section 10-21-2 Building Types and the "Uses" section in the tables per building type.
4. **Section 10-21-4: Building Types.** Six (6) Building Types are defined for use in the Street Districts. A mix of building types are typically permitted per district. These Building Types outline the desired building forms for the new construction and renovation of structures and contain regulations that determine physical building elements such as build-to-zones, transparency levels, entrance location, and parking location. Refer to Figure 10-21-4A (1) for a typical Building Type page layout.
5. **Section 10-21-5: Site Development Standards.** The site development standards provide references to other City ordinances or parts of the zoning ordinance and may include additional information or revision to those ordinances applicable only to the Street Districts. These include signage, parking, and landscape.

D. Applicability

These regulations apply to the downtown and adjacent redevelopment areas within the City as mapped on the City's zoning map. Refer to Figure 10-21-1B (1) for affected parcels.

E. How to Use the Code.

Refer to Figure 10-21-1D (1) for a step by step illustration about applying the code to a parcel. Throughout this section, call out boxes titled "How to Use the Code" appear with code application instructions.

F. Development Approval Process

1. **Site Plan Review.** An approved site plan is required for the development or redevelopment of all parcels in any District (refer to 10-4 of the Zoning Ordinance) with the following revisions:
 - (a) The Community Development Director may approve a major site plan if the site plan complies with all requirements of the city's zoning ordinance. If the Community Development Director denies approval of a major site plan, including the provision of written comments as to the reason for such denial, the denial may be appealed by the applicant to the Planning and Zoning Commission for review. The Planning and Zoning Commission shall then recommend approval to City Council the major site plan, recommend approval to City Council of the major site plan with conditions, or recommend denial of the site plan to City Council.
2. **Deviations.** The Applicant shall submit requested deviations to the Community Development Director with the Site Plan application. The Community Development Director may approve deviations to a site plan for the following:
 - (a) **Minor Deviations.** The Community Development Director may approve minor deviations to any dimension or percentage as follows:
 - i. The location of the building within up to one (1) foot from any minimum yard requirement or build-to zone width/location.
 - ii. Up to five percent (5%) increase in total impervious coverage, not to exceed the total amount of permitted impervious plus semi-pervious coverage.
 - iii. Up to five percent (5%) decrease in Front Property Line coverage.
 - iv. Additional height of any story up to two (2) feet, as long as the overall building height does not exceed the allowable height of all floors at their maximum permitted height.
 - (b) **Design Deviations.** The Community Development Director shall review and make a recommendation for the following deviations:
 - i. **Alternative Building Materials.** The Director may approve alternative building materials from the requirements of Section 10-21-5, with the exception of the prohibited materials. For approval, the Applicant shall submit samples and local examples of the material a minimum of four weeks prior to the review, to allow site visits to the location.

- ii. Facade Variety Alternative. The Director may approve a reprieve from the facade variety requirements in Section 10-21-4. The Applicant shall submit fully rendered elevations and three (3) dimensional drawings of all street facades with materials samples for all surfaces to prove a higher quality building design with variation and relief from monotony.
- (c) Existing Building Deviations. The Planning and Zoning Commission shall review and make a recommendation to City Council for the following deviations, when applied to the renovation of an existing building(s):
 - i. For renovation of existing buildings, the maximum front property line coverage may be waived with an existing coverage of sixty percent (60%); however, any expansion on the ground story shall contribute to the extension of the front property line coverage.
 - ii. For renovation of existing buildings, the location of the building within up to five (5) feet from any minimum yard requirement or build-to zone width/location.
 - iii. For renovation of existing buildings, the minimum height of the ground story and upper story may be increased or decreased by up to two (2) feet for existing stories.
 - iv. For renovation of existing buildings, other required dimensions may be modified up to five (5) feet or ten percent (10%), whichever is less, unless otherwise modified by this section.

G. Nonconforming Structures

1. **Nonconforming Uses.** Refer to Section 10-15 of this Article for Non-Conforming Uses requirements with the following exception and addition.
2. **Nonconforming Structures.** The following regulations allow for the continuation of occupation of a structure that was legally constructed prior to the adoption or amendment to this code, but that could not occur under the provisions of this code.
 - (a) All Building Type standards apply to all new construction and renovation of existing structures, where the renovation includes an addition of more than sixty percent (60%) in gross building square footage.
 - (b) When the existing front or corner facade is located within the build-to zone and a renovation of the front facade occurs with or without any added building square footage, the Street Facade Requirements and Entrance Type Requirements of any permitted Building Type shall be met when the renovation includes any of the following:
 - i. Installation of additional doors or a change in location of a door;
 - ii. Expansion or change in location of thirty percent (30%) of windows on any street façade; or
 - iii. Replacement of thirty percent (30%) or more of facade materials on any street facade with a different facade material.
 - (c) When the existing building front or corner facade is located within the build-to zone and a renovation of the shape or style of the roof occurs with or without added building square

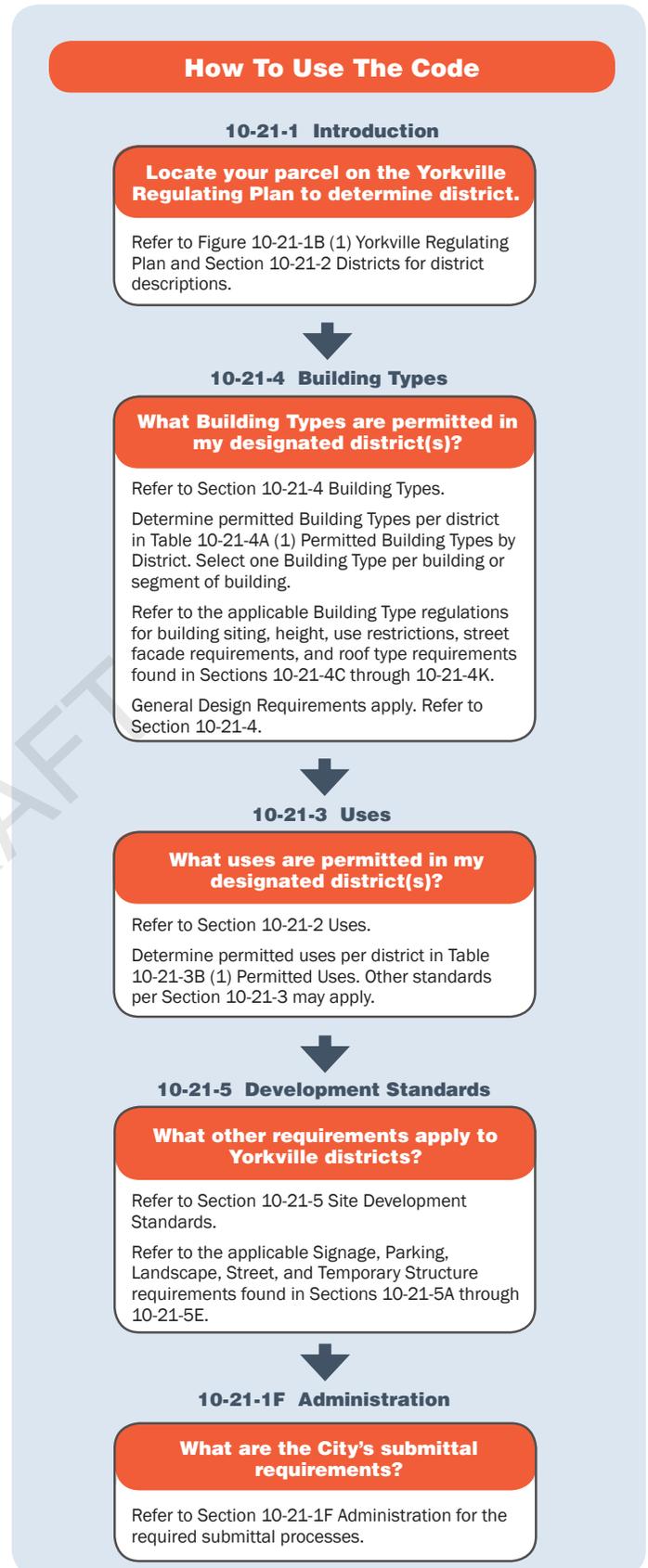


Figure 10-21-1D (1). Yorkville Code Flow Chart.

10-21-1 Introduction



Figure 10-21-1B (1). Yorkville Form-Based Districts Regulating Plan.

footage, the Roof Type Requirements of any permitted Building Type shall be met.

- (d) Under all circumstances, no portion of the Building Type standards must be met in the case of normal repairs required for safety and continued use of the structure, such as replacement of window or door glass.

H. Definitions

For the purposes of this document, the following terms shall have the following meanings:

Applicant. The Owner of a subject property or the authorized representative of the Owner on which a land development application is being made.

Block. Refer to Section 10-2-3 for definition.

Block Depth. A Block measurement that is the horizontal distance between the Front Lot Line on a Block Face and the Front Lot Line of the parallel or approximately parallel Block Face.

Block Ends. The Lots located on the end of a Block; these Lots are often larger than the Lots in the interior of the Block or those at the opposite end of the Block and can be located on a more intense Street Type. They are typically more suitable for more intensive development, such as multiple family or mixed Use development.

Block Face. The aggregate of all the building Facades on one side of a Block.

Block Length. A Block measurement that is the horizontal distance along the Front Lot Lines of the Lots comprising the Block.

Building Type. The Facade of a structure defined by the combination of configuration, form, and function as it relates to the adjacent street. Refer to 10-21-4B Building Type Standards for more information and the list of permitted Building Types.

Build-to Zone. An area in which the front or corner side facade of a building shall be placed; it may or may not be located directly adjacent to a lot line. The zone dictates the minimum and maximum distance a structure may be placed from a lot line. Refer to Figure 10-21-1H (3).

Courtyard. An outdoor area enclosed by a building on at least three (3) sides and is open to the sky.

Coverage, Building. The percentage of a Lot developed with a Principal or Accessory Structure.

Coverage, Impervious Site. The percentage of a Lot developed with Principal or Accessory Structures and Impervious Surfaces, such as driveways, sidewalks, and patios. Refer to "Lot Coverage" in Section 10-2-3.

Eave. The edge of a pitched roof, typically overhangs beyond the side of a building.

Entrance Type. The permitted treatment types of the Ground Floor Facade of a Building Type. Refer to Section 10-21-4I for more information and a list of permitted Entrance Types.

Expression Line. An architectural feature consisting of a decorative, three (3) dimensional, linear element, horizontal or vertical,

protruding or indented at least two (2) inches from the exterior facade of a building typically utilized to delineate the top or bottom of floors or stories of a building.

Facade. The exterior face of a building, including but not limited to the wall, windows, windowsills, doorways, and design elements such as Expression Lines. The front facade is any building face adjacent to the Front Lot Line.

Frontage District. A type of zoning district specific to this Article, where the location, height and bulk of structures is defined by Building Types. Refer to Section 10-21-4.

Landscape Area. Area on a Lot not dedicated to a structure, parking or loading facility, frontage buffer, side and rear buffer, or interior parking lot landscaping. Landscape Areas may include landscape, sidewalks, patios, or other pedestrian amenities.

Lot, Flag. Refer to Section 10-2-3 for definition. Refer to Figure 10-21-1H (2).

Lot, Interior. For the purposes of this Chapter, a parcel of land abutting a vehicular Right-of-Way, excluding an Alley, along one Lot Line; surrounded by Lots along the remaining Lot Lines.

Lot, Through. Refer to Section 10-2-3 for definition. Refer to Figure 10-21-1H (2).

Lot Area. Refer to Section 10-2-3 for definition; it is typically denoted in square feet or acres.

Lot Depth. For the purposes of this Chapter, the smallest horizontal distance between the Front and Rear Lot Lines measured approximately parallel to the Corner and/or Side Lot Line. Refer to Figure 10-21-1H (2).

Lot Frontage. Refer to Section 10-2-3 for definition.

Lot Line, Corner. For the purposes of this Chapter, a boundary of a Lot that is approximately perpendicular to the Front Lot Line and is directly adjacent to a public Right-of-Way, other than an Alley or railroad. Refer to Figure 10-21-1H (2).

Lot Line, Front. For the purposes of this Chapter, the boundary abutting a Right-of-Way, other than an Alley, from which the required Setback or Build-to Zone is measured, with the following exceptions.

- (a) Corner and through Lots that abut a Primary Street (refer to Figure 10-21-2G (4)) shall have the Front Lot Line on that Primary Street.
- (b) Corner and Through Lots that abut two (2) Primary Streets or do not abut a Primary Street shall utilize the orientation of the two (2) directly adjacent lots, or shall have the Front Lot Line determined by the Director.
- (c) Lot Line, Rear. Refer to Section 10-2-3 for definition. Refer to Figure 10-21-1H (2).

Occupied Space. Interior building space regularly occupied by the building users. It does not include storage areas, utility space, or parking.

Open Space. Refer to Section 10-2-3 (Public Open Space and Usable Open Space) for definitions. Open space may also be utilized to host temporary private or community events, such as a farmer's

10-21-1 Introduction

market or art fair.

Pedestrianway. A pathway designed for use by pedestrians; it can be located mid-block allowing pedestrian movement from one street to another without traveling along the block's perimeter.

Pervious Surface. Also referred to as pervious material. A material or surface that allows for the absorption of water into the ground or plant material, such as permeable pavers or a vegetated roof.

Primary Street. A street that receives priority over other streets in terms of setting front lot lines and locating building entrances. Refer to Figure 10-21-2G (4) for mapped Primary Streets.

Roof Type. The detail at the top of a building that finishes a Facade, including a pitch roof with various permitted slopes and a parapet. Refer to 10-21-4I for more information and a list of the permitted Roof Types.

Scale. The relative size of a building, street, sign, or other element of the built environment.

Semi-Pervious Surface. Also referred to as semi-pervious material. A material that allows for at least forty percent (40%) absorption of water into the ground or plant material, such as pervious pavers, permeable asphalt and concrete, or gravel.

Setback. For the purposes of this Chapter, the horizontal distance from a Lot Line inward, beyond which a structure may be placed. For the purposes of this Chapter, structures and parking lots are not permitted within a Setback, unless specifically stated otherwise in this Chapter. Refer to Figure 10.21.1H (1).

Solar Reflectance Index (SRI). A measure of a constructed surface's ability to reflect solar heat, as shown by a small temperature rise. The measure utilizes a scale from zero (0) to one hundred (100) and is defined so that a standard black surface is zero (0) and a standard white surface is one hundred (100). To calculate for a given material, obtain the reflectance value and emittance value for the material; calculate the SRI according to ASTM E 1980-01 or the latest version.

Story. For the purposes of this Chapter, a habitable level within a building measured from finished floor to finished floor. Refer to Section 10-21-4 for dimensions.

Story, Ground. Also referred to as ground floor. The first floor of a building that is level to or elevated above the finished Grade on the Front and Corner Facades, excluding basements or cellars.

Story, Half. For the purposes of this Chapter, a story either in the base of the building, partially below grade and partially above grade, or a story fully within the roof structure with transparency facing the street.

Story, Upper. Also referred to as upper floor. The floors located above the Ground Story of a building.

Street Face. The Facade of a building that faces a public Right-of-Way.

Street Frontage. Also refer to Lot Frontage. The portion of a building or Lot directly adjacent to a vehicular Right-of-Way.

Street Termini. At a three (3)-way or "T" intersection, it is the location where one street terminates at the other street.

Streetwall. The vertical plane created by building Facades along a

street. A continuous Streetwall occurs when buildings are located in a row next to the sidewalk without vacant Lots or significant Setbacks.

Transparency. The measurement of the percentage of a facade that has highly transparent, low reflectance windows. Mirrored glass is not permitted.

Yard. Refer to Section 10-2-3 for definition. Refer to Figure 10-21-1H (3) Illustration of Yards. Note that the Rear Yard is fully screened from the street by the Structure.

- (a) **Yard, Corner Side.** A Yard extending from the corner side building Facade along a Corner Side Property Line between the Front Yard and Rear Property Line.
- (b) **Yard, Front.** Refer to Section 10-2-3 for definition.
- (c) **Yard, Rear.** Refer to Section 10-2-3 for definition.
- (d) **Yard, Side.** Refer to Section 10-2-3 for definition.

Visible Basement. A half story partially below grade and partially exposed above with required transparency on the street facade.

10-21-1 Introduction



Build-to Zone vs Setback Line

A setback line indicates the closest a building may be placed to a property line, but is silent on where behind that line a building may be placed. A build-to zone indicates a zone or area in which the Facade of a building must be located. The use of a build-to zone allows some control over building placement, while the range provides some flexibility. This method also provides an element of predictability that is absent when the only requirement is to locate a building beyond a certain line.

Figure 10.21.1H (1). Build-to Zone vs. Setback Line.

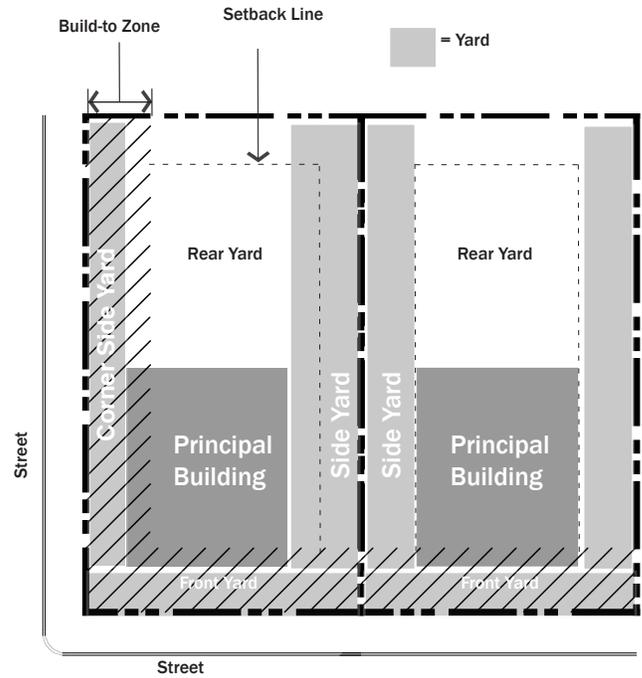


Figure 10-21-1H (3). Illustration of Yards.

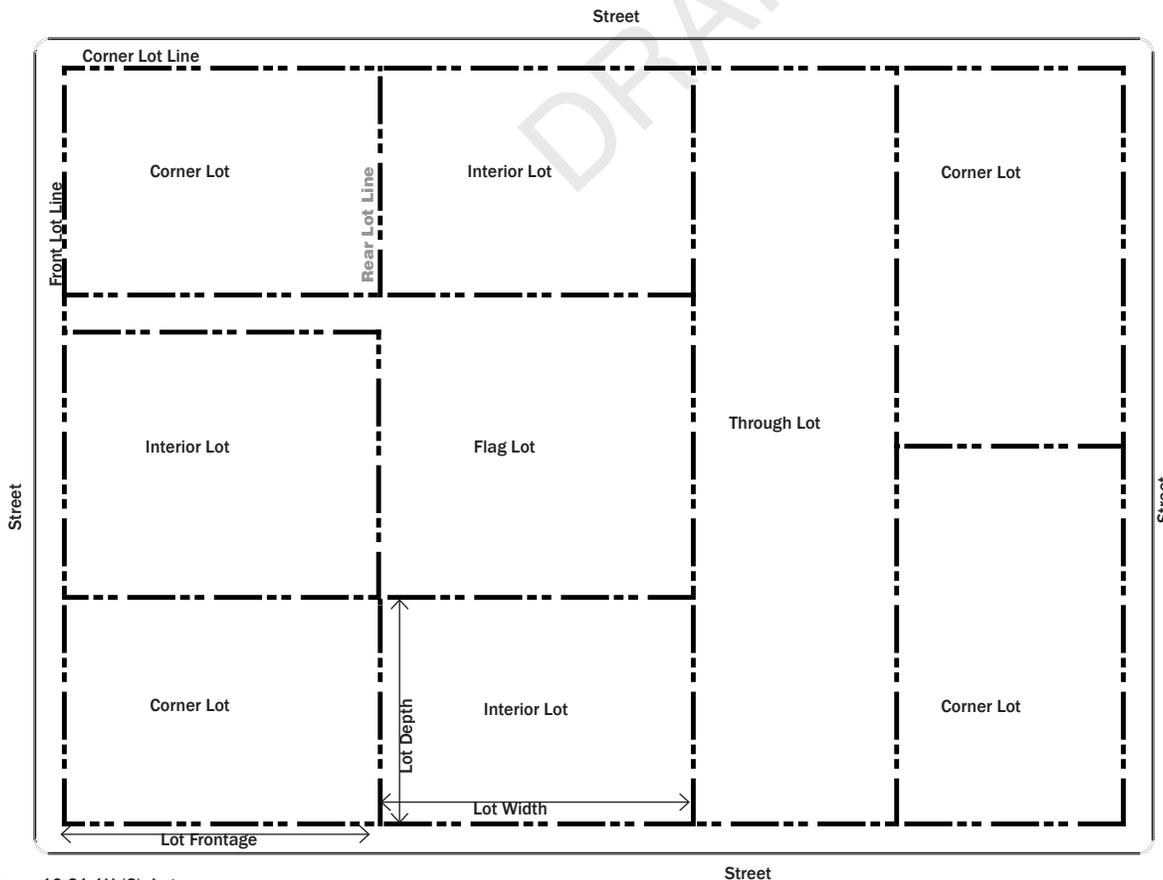


Figure 10-21-1H (2). Lots.

10-21-2 Districts

The following Districts are established for mixed use, commercial, and residential development within downtown and adjacent redevelopment areas. Figure 10-21-1B (1) illustrates the locations for the districts.

- S 1: Bridge Street District
- S 2: Hydraulic Street District
- S 3: Van Emmon Street District
- S 4: 'B' Street District
- S 5: 'B' Street - Residential District

DRAFT

A. S1: Bridge Street District

The Bridge Street District may be considered the gateway into downtown Yorkville and retains potential to become the iconic stretch that helps draw people into local businesses and displays an attractive image that represents the people of Yorkville. The form of this retail and service-centered area establishes a street wall of storefront style-building facades with shallow build-to-zones along the sidewalk and parking in the rear or off-site. It focuses pedestrian-friendly retail and service uses on the ground story with office uses in upper stories.

Bridge Street	
Building Placement	
Build-to Zone	0' to 10'
Space Between Buildings	
Attached	0'
Detached	5' to 10'
Building Volume	
Maximum Building Height	80'
Maximum Stories	6
Minimum Ground Floor Height	14'
Typical Street Attributes	
Typical ROW Width	72'
Number of Travel Lanes	4
Lane Width	10' to 13'
Dedicated Turn Lanes	1
Parking Lanes	none
Pavement Width	60'
Dedicated Bicycle Facilities	none
Pedestrian Realm	
Pedestrian Facilities	6' wide sidewalk
Street Buffer	concrete barrier

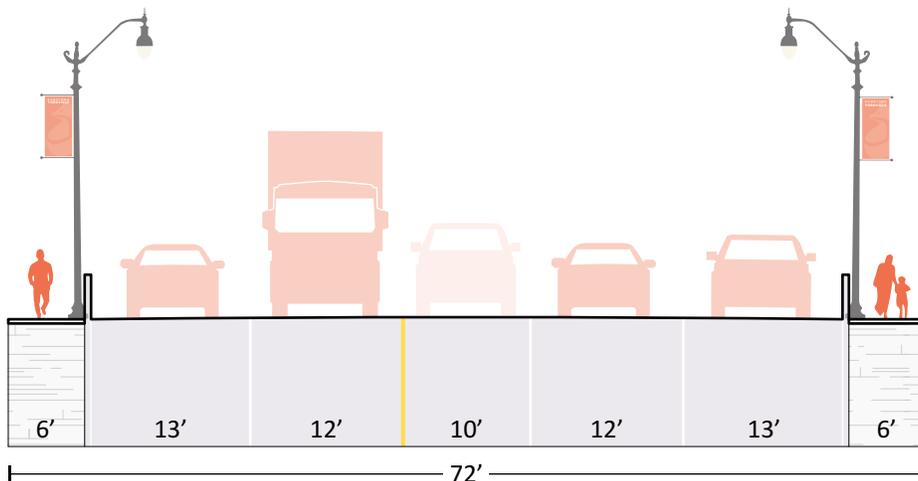


Figure 10-21-2A (1). Bridge Street (Long-term).

10-21-2 Districts

B. S2: Hydraulic Street District

The Hydraulic Street District includes complex conditions, including a tapering right-of-way that narrows from west to east and an active freight rail line that runs parallel to the street surface within the right-of-way. Hydraulic Street features short, utilitarian buildings, as well as an inoperable grain elevator, that create an eclectic mix of land uses and character. The form of this area remains pedestrian-centered but storefront-style buildings focus a broader spectrum of retail and service uses on the ground story with residential and/or office uses in upper stories.

Hydraulic Street	
Building Placement	
Build-to Zone	0' to 15'
Space Between Buildings	
Attached	0'
Detached	10'
Building Volume	
Maximum Building Height	80'
Maximum Stories	5
Minimum Ground Floor Height	14'
Typical Street Attributes¹	
Typical ROW Width	48' to 66'
Number of Travel Lanes	2
Lane Width	10' to 12'
Dedicated Turn Lanes	none
Parking Lanes	1 lane of parallel parking on south side of street
Pavement Width	22'
Dedicated Bicycle Facilities	Multi-purpose lane on north side of street
Pedestrian Realm	
Pedestrian Facilities	6' to 8' wide sidewalk
Street Buffer	1 lane of parallel parking on south side of street; 4' planter on north side of multi-purpose street

Notes:

¹ The Streetscape Master Plan includes a proposed slip lane on the south side of Hydraulic Street between Bridge Street and Heustis Street. This table includes dimensions for the public right-of-way section of the street.

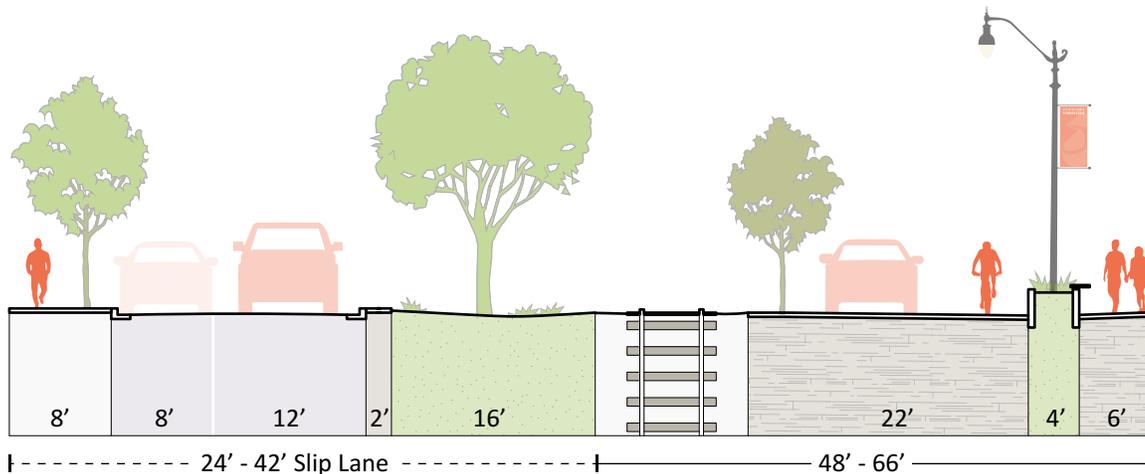


Figure 10-21-2B (1). Hydraulic Street (Long-term).

C. S3: Van Emmon Street District

The Van Emmon Street District is a lower scale district. Commercial, residential, and mixed use buildings make up this district in variety of building types. A broader spectrum of retail, service, and office uses support the Bridge Street and Hydraulic Street Districts and ground floor residential is permitted. The form of this area is pedestrian-centered, but allows for more vehicular access and off-street parking options.

Van Emmon Street	
Building Placement	
Build-to Zone	0' to 20'
Space Between Buildings	
Attached	0'
Detached	10'
Building Volume	
Maximum Building Height	80'
Maximum Stories	5
Minimum Ground Floor Height	12'
Typical Street Attributes	
Typical ROW Width	60'
Number of Travel Lanes	2
Lane Width	12'
Dedicated Turn Lanes	none
Parking Lanes	1 lane of parallel parking on each side of street; permeable paving 1 lane of parallel parking each side of street (intermittent planting on south side to accommodate parking)
Pavement Width	24' to 40'
Dedicated Bicycle Facilities	none
Pedestrian Realm	
Pedestrian Facilities	8' to 12' wide sidewalk
Street Buffer	1 lane of parallel parking on north side of street; street trees or 1 lane of parallel parking on south side of street

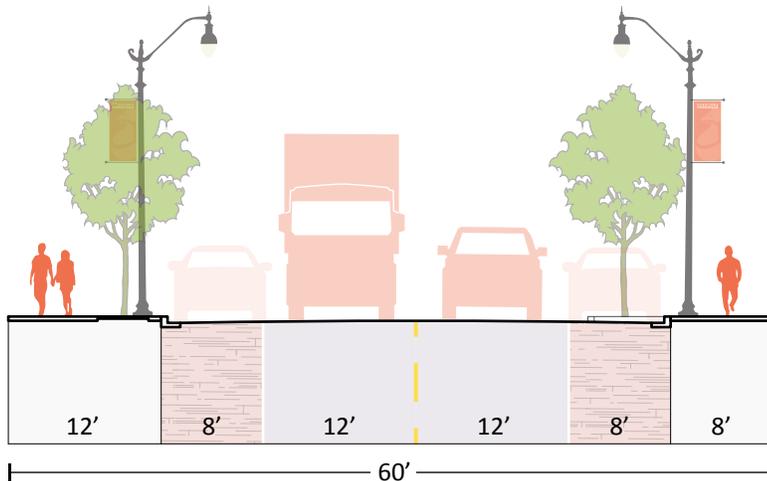


Figure 10-21-2C (1). Van Emmon Street (Long-term).

2.0 Districts

D. S4: 'B' Street District

The 'B' Street District is a lower scale commercial district designed to serve adjacent neighborhoods and the greater community. Flanked by the Hydraulic and Van Emmon Districts, a wider range of residential uses is permitted. As one of the main street types between primary thoroughfares, pedestrian access and safety remains a priority.

'B' Street	
Building Placement	
Build-to-line Location	0' to 20'
Space Between Buildings	
Attached	0'
Detached	10'
Building Volume	
Maximum Building Height	80'
Maximum Stories	5
Minimum Ground Floor Height	9'
Typical Street Attributes	
Typical ROW Width	50'-60'
Number of Travel Lanes	2
Lane Width	11'
Dedicated Turn Lanes	none
Parking Lanes	1 lane of parallel parking each side of street (reduce planting where appropriate to accommodate parking)
Pavement Width	26'
Dedicated Bicycle Facilities	none
Pedestrian Realm	
Pedestrian Facilities	5' wide sidewalk
Street Buffer	13' swale and/or stormwater buffer; optional parking

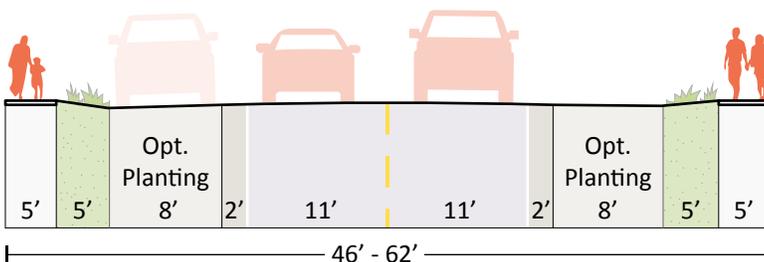


Figure 10-21-2D (1). 'B' Street (Long-term).

E. S5: 'B' Street - Residential District

The 'B' Street Residential District is a lower-scale residential district with a mix of townhouses and yard buildings. With a maximum height of six (6) stories under the general provision of the zoning code, this area provides for a mix of housing types adjacent to downtown for people of all ages and lifestyles.

'B' Street - Residential	
Building Placement	
Build-to-line Location	0' to 15'
Space Between Buildings	
Attached	0'
Detached	10'
Building Volume	
Maximum Building Height	80'
Maximum Stories	3.5
Minimum Ground Floor Height	9'
Typical Street Attributes	
Typical ROW Width	50' to 60'
Number of Travel Lanes	2
Lane Width	11'
Dedicated Turn Lanes	none
Parking Lanes	1 lane of parallel parking each side of street (reduce planting where appropriate to accommodate parking)
Pavement Width	26'
Dedicated Bicycle Facilities	none
Pedestrian Realm	
Pedestrian Facilities	4' wide sidewalk
Street Buffer	13' swale and/or stormwater buffer; optional parking

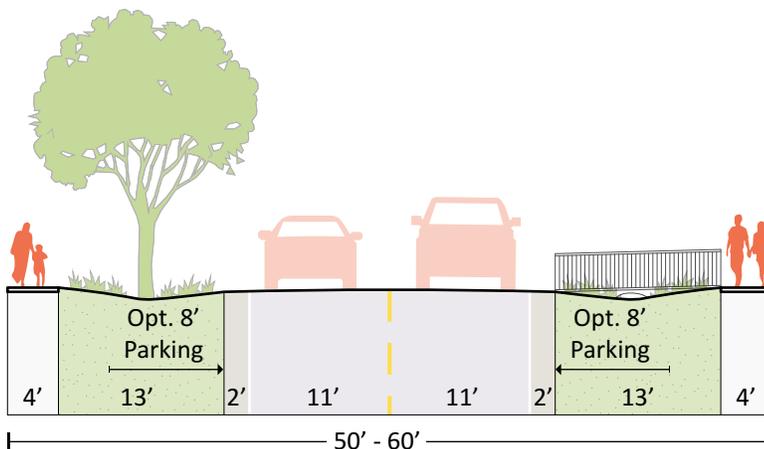


Figure 10-21-2E (1). 'B' Street - Residential (Long-term).

10-21-2 Districts

F. Hierarchy of Districts

Development under this code is regulated by street type. The various street types are related to each other in a hierarchical manner. When these streets intersect, the primary street frontage is determined by its higher order in the hierarchy. The front of a building and its main entrance must face the primary street. Figure 10.21.2G (1) illustrates the hierarchy of street types.

G. Primary Streets

The Primary Streets Map designates Primary, Secondary, and Tertiary Streets within the Downtown Overlay District. These designations prioritize the street frontages for locating the Front Lot Line for required amounts building frontage and allowable amounts of parking frontage. Refer to Figure 10.21.2G (4).

1. **Vehicular Access.** Vehicular access is also managed through this street prioritization within the Building Type requirements (refer to Section 10-21-4). See Figure 10.21.2G (2) and Figure 10.21.2G (3) for illustration of permitted vehicular access locations.
 - (a) The order of access is typically as follows, unless otherwise permitted by Building Type:
 - i. Lane (Tertiary Street) or Alley (un-mapped) permits unlimited access.
 - ii. If no Lane or Alley exists, one (1) driveway per secondary street frontage is permitted.
 - iii. If no tertiary or secondary street abuts the parcel, one driveway is permitted off the Primary Street and shared access with adjacent properties is encouraged.
 - (b) Refer to the Subdivision Control Ordinance for additional requirements of vehicular access off adjacent streets. Interparcel Access requirements shall be met.
2. **Primary, Secondary, and Tertiary Street Designations.**
 - (a) **Primary Streets.** Primary streets require the most intact and continuous streetwall with few, if any, driveway access. The Front Lot Line shall always be located on the Primary Street. When a parcel is fronted by more than two (2) primary streets, the Director shall determine which is the appropriate street for the Front Lot Line. The determination should be based on locations of existing and other proposed development Front Lot Lines.
 - i. Vehicular access should not be located off a primary street, unless no other options exists. Refer to Section 10-21-4 Building Type requirements for allowances of vehicular access, if applicable.
 - (b) **Secondary Streets.** Secondary streets are intended to have a fairly continuous streetwall, but will likely have a driveway for each parcel. If no tertiary street exists, access will likely be off the secondary street.
 - (c) **Tertiary Streets.** Tertiary streets typically have no streetwall requirements and could be fronted by parking structures and parking lots with multiple drives and/or vehicular access points.

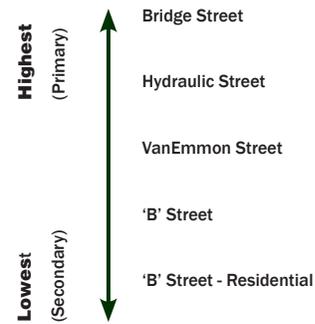


Figure 10-21-2G (1). Hierarchy of Street Types.

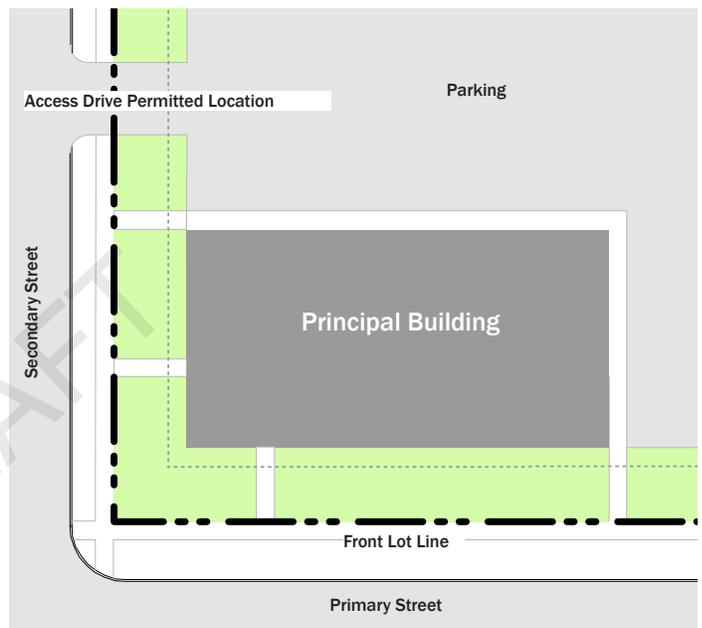


Figure 10-21-2G (2). Secondary Street Frontage: Access Drive Permitted Location

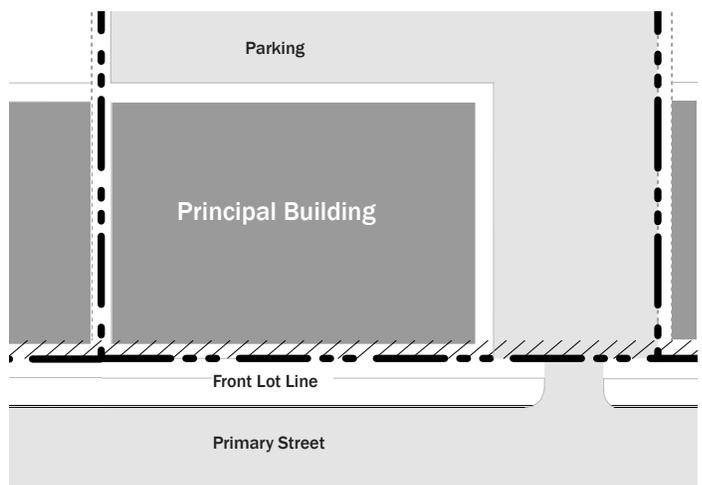


Figure 10-21-2G (3). Primary Street Frontage Only: Access Drive Permitted Location

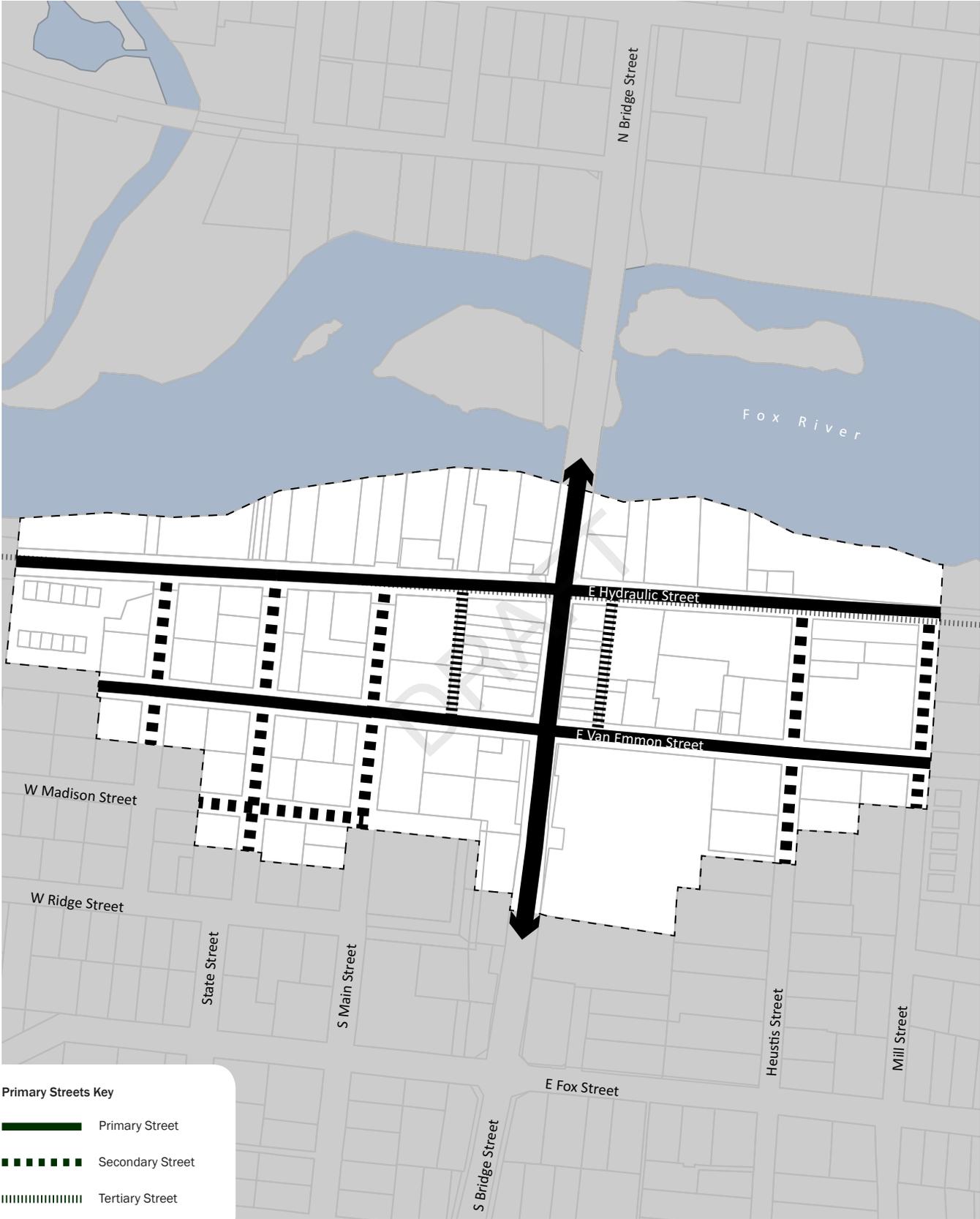


Figure 10-21-2G (4). Primary Streets Map.

10-21-3 Uses

A. General Provisions

The following establishes a simplified set of uses permitted within the Building Forms of each downtown form-based zoning district.

- Uses by District.** Permitted and special uses available in each form-based district are shown in Table 10-21-3B (1).
- Use Definitions.** Uses listed in Tables 10-21-3C (1) - 10-21-3C (4) are defined in Section 10-2-3 of the Zoning Ordinance. Further definitions applicable only to the Yorkville Form-Based Districts are located in Section 10-21-3C.
- Permitted Use (“●”).** These uses are permitted by-right in the form districts in which they are listed.
- Permitted in Upper Stories Only (“◐”).** These uses are permitted by-right in the districts in which they are listed, provided that the uses are located in the upper stories of a structure. These uses may also be located in the ground story provided that they are located beyond a depth of at least twenty feet from the front primary facade.
- Requires a Special Use Permit (“○”).** These uses require a Special Use Permit (refer to Section 10-6 Special Use Regulations) in order to occur in the districts in which they are listed and must follow any applicable development standards associated with the use as well as meet the requirements of the special use.
- Permitted with Development Standards (“◑”).** These uses are permitted in the districts in which they are listed, provided that they are developed utilizing the listed development standards. These standards, combined with the Building Type requirements, are intended to alleviate any negative impacts associated with the use, making it appropriate in a district where it otherwise might not have been appropriate.
- Prohibited Use.** A blank cell or an unlisted use in the use table indicates that the land use is prohibited in that district.
- City Determination.** When a proposed land use is not explicitly listed in Table 10-21-3B (1), the Director shall determine whether or not it is included in the definition of a listed use or is so consistent with the size, scale, operating characteristics and external impacts of a listed use that it should be treated as the same use. Any such determination may be appealed to the Planning and Zoning Commission.
- Temporary Accessory Uses.** Temporary accessory uses are permitted in all Downtown Overlay Districts, including outdoor displays, seasonal displays and sales, festivals, and vendors. The following requirements apply:
 - Outdoor displays are permitted up to eight (8) hours per day, no more than three (3) consecutive days in any one (1) month.
 - Build-to Zone. Temporary outdoor displays of merchandise are permitted in the Build-to Zone without a permit, subject to all other requirements of Section 10-21-3.
 - Right-of-way. Any temporary outdoor display or occupation in the right-of-way requires a review approval by the Community Development Director.

How To Use The Code

Permitted Use Table

The Permitted Use Table contains all the uses permitted in the Downtown Overlay District. It is organized by district, so only the information in a parcel’s district is applicable to that parcel. The other columns do not apply.

For example, a parcel in CC1 only applies the standards from that column, illustrated below.

	Districts				
	CC1: Core	CC2: Shopping	CC3: Core Support	CC4: Residential / Office	CC5: Gateway
Residential & Lodging Uses					
Residential	◐	◐	●	●	◐
Bed & Breakfast				○	
Hotel/Motel	◐	●	●	●	●
Care Facilities	◐	◐	●	○	●
Senior Housing	◐	◐	◐	◐	◐
Civic / Institutional Uses					
Assembly Uses			●	●	●
Civic Offices & Services	●	●	●	●	
Library/Museum	●	●	●	●	●
School: Elementary, Secondary			●	●	●
School: High School				●	●
School, Private or Special				○	○
Retail Uses					
Neighborhood Retail	●	●	●	◐	●
General Retail		●			●
Adult Establishments		◐	◐		◐
Service Uses					
Neighborhood Service	●	●	●	◐	●
General Service		●			●
Automotive Specialty Shop					◐
Service Station					◐
Commercial Amusement, Outdoor					○
Daycare Facility			●	●	●
Hospital			●		●
Veterinary Clinic/Hospital or Kennel (Enclosed)		◐	◐		◐

Figure 10-21-3A (1). Yorkville Code Permitted Uses How-To.

10-21-3 Uses

	Districts				
	S 1: Bridge Street	S 2: Hydraulic Street	S 3: Van Emmon Street	S 4: 'B' Street	S 5: 'B' Street - Residential
Residential & Lodging Uses Category					
Residential Dwelling	●	●	●	●	●
Hotel/Motel	●	●	●	○	○
Bed & Breakfast Inn	●	●	○	○	○
Residential Care Facility				○	○
Senior Housing		●	●	●	○
Civic & Institutional Uses Category					
Civic Offices & Services	●	●	●	●	●
Library/Museum	●	●	●	○	○
Police & Fire	●	●	●		
Post Office (no distribution)	●	●	●		
Religious & Organization Assembly	●	●	●	●	●
School: Public or Private	○	○	○	○	○
School: College	○	○	○	○	○
Retail & Service Uses Category					
Neighborhood Retail (<8,000 sf)	●	●	●	●	○
General Retail	●	●	●		
Neighborhood Service	●	●	●	○	○
General Service	●	●	●		
Recreation/Entertainment	●	●	●	○	
Automotive Services		○	○	○	
Food Services & Drinking Places	●	●	●	●	○
Office & Industrial Uses Category					
Office	●	●	●	●	
Small-Scale Industry	●	●	●	●	
Infrastructure Uses Category					
Communication Antennae & Towers	○	○	○	○	○
Parking Lot	●	●	●	●	●
Parking Structure	●	●	●	●	●
Public Utility & Infrastructure	●	●	●	●	●
Open Space Uses Category					
Open Space	●	●	●	●	●

	Districts				
	S 1: Bridge Street	S 2: Hydraulic Street	S 3: Van Emmon Street	S 4: 'B' Street	S 5: 'B' Street - Residential
Accessory Uses					
Alternative Energy Generation	●	●	●	●	●
Amphitheater		○		○	
Car Washes, Detail Shops, and/or Service Stations		○	○	○	
Drive Through Facility		○	○	○	
Home Occupation	●	●	●	●	●
Outdoor Storage of Goods		●	●	●	
Outdoor Restaurant Dining	●	●	●	●	
Parking Lot (Accessory)	●	●	●	○	
Parking Structure (Accessory)	●	●	●		
Roadside Produce Stand/Vending		●		●	
Sidewalk Sales	●	●	●	○	○
Temporary Storage Containers		●	●	●	●
Mobile Food Vendors	●	●	●	●	●
Temporary Structures	●	●	●	●	●

Key

- Permitted
- Permitted in Upper Stories Only
- Permitted with Development Standards
- Requires Special Use Permit

Table 10-21-3B (1). Permitted Uses Table.

10-21-3 Uses

B. Permitted Use Table

See Table 10-21-3B (1) for a list of permitted and special uses.

C. Use Requirements

1. **Residential and Lodging Uses.** A category of uses that include several residence types.
 - (a) Refer to 10-2-3 for the following definitions.
 - i. Residential.
 - ii. Bed & Breakfast Inn.
 - (b) Hotel/Motel. Refer to 10-2-3 for definition. In the districts where a hotel/motel requires development standards (“●”), rooms shall be located in the upper stories. Lobbies and eating facilities shall be located on the ground floor.
 - (c) Residential Care Facility. A facility offering temporary or permanent lodging to the public consisting of an unlimited number of sleeping rooms with or without in-room kitchen facilities. Includes such uses as independent and assisted living facilities, nursing homes, residential care homes, and transitional treatment facilities. Assistance with daily activities may be provided for residents. Secondary service uses may also be provided, such as dining and meeting rooms. Rooms shall be accessed from the interior of the building. In the districts where this use requires a special use permit (“○”), refer to Section 10-21-1F for procedure and development standards.
 - (d) Senior Housing. In the districts where Senior Housing requires development standards (“●”), the following shall apply:
 - i. Housing shall be designed, constructed and operated in accordance with the Fair Housing Amendments Act.
 - ii. The Property shall comply with all applicable local, state, and federal regulations and copies of any applicable permits shall be provided to the Department prior to the issuance of a Certificate of Occupancy.
 - (e) Use Permits. When a use is listed as permitted with a Use Permit “○,” refer to Section 10-21-1F for procedure and development standards.
2. **Civic & Institutional Uses.** A category of uses related to fulfilling the needs of day-to-day community life including assembly, public services, educational facilities, and hospitals.
 - (a) Civic Offices & Services. Any use that involves transactions between the City government and residents, property owners, or others doing business with the City or other activities related to the function of the municipality.
 - (b) Library/Museum. A structure open to the general public housing educational, cultural, artistic, or historic information, resources, and exhibits. May also include food service and a gift shop.
 - (c) Police & Fire. A facility providing public safety and emergency services; training facilities, locker rooms, and limited overnight accommodations may also be included. The facilities shall be housed in a permitted building, but shall have the following additional allowances:
 - i. Garage doors are permitted on the front facade.
 - ii. Exempt from maximum driveway widths.
- (d) Post Office. A publicly accessed facility for the selling of supplies and mail related products and the small scale collection and distribution of mail and packages. Large-scale postal sorting and distribution is not permitted.
- (e) Religious & Organization Assembly. Assembly Uses include Assembly Hall; Church, Temple, or Other Place of Worship; and Clubs.
- (f) Use Permits. When a use is listed as permitted with a Special Use Permit “○,” refer to Section 10-21-1F for procedure and development standards.
3. **Retail & Service Uses.** A category of uses that include several retail and service types. Refer to Section 10-2-3 for definitions.
 - (a) Neighborhood Retail. A use in this category occupies a space of less than 8,000 square feet. Neighborhood retail includes such uses as those listed in Table 10-21-3C (1).
 - i. When Neighborhood Retail is permitted with development standards “●”, it is limited to 8,000 square feet.
 - (b) General Retail. A use in this category includes all Neighborhood Retail uses occupying a space of greater than 8,000 square feet and such uses as those listed in Table 10-21-3C (1).
 - (c) Service Uses. A category of uses that provide patrons services and limited retail products related to those services. Visibility and accessibility are important to these uses, as most patrons do not utilize scheduled appointments.
 - (d) Neighborhood Service. A use in this category occupies a space of less than 8,000 square feet. Neighborhood service includes such uses as those listed in Table 10-21-3C (2).
 - i. When Neighborhood Service is permitted with development standards “●”, it is limited to 6,000 square feet.
 - (e) General Service. A use in this category includes all Neighborhood Service uses occupying a space of greater than 8,000 square feet and such uses as those listed in Table 10-21-3C (2).
 - (f) Recreation/Entertainment. Refer to Table 10-21-3C (3).
 - (g) Automotive Services. Refer to Section 10-2-3 for definition. In the districts where this use requires a special use permit (“○”), the following applies:
 - i. The use shall meet all of the requirements of the Building Type, refer to Section 10-21-4.
 - ii. Service doors shall all be located on the rear or interior side facade of the building.
 - iii. The showroom or retail space shall be located in the front storefront of the building and shall occupy a minimum of twenty percent (20%) of the gross floor area.
 - (h) Food Services & Drinking Places. Food Services & Drinking Places include only the following:
 - i. Full Service Restaurants
 - ii. Carry-out Food Service Stores
 - iii. Quick Service Restaurants
 - iv. Banquet Centers & Caterers
 - v. Drinking Places (Alcoholic Beverages)

- vi. Brewpubs. A microbrewery that brews beer for both on and off-premise consumption, and may include a bar and/or restaurant.
- vii. Wine Establishments. A properly licensed business which sells wine only (not liquor or beer) for both on and off-premise consumption, as well as wine accessories and/or merchandise.

4. **Office & Industrial Uses.** A category of uses for businesses that involve the transaction of affairs of a profession, service, industry, or government. Patrons of these businesses usually have set appointments or meeting times; the businesses do not typically rely on walk-in customers. Office uses include those listed in Table 10-21-3C (4), List of Typical Uses in Office Category.

- (a) Office. Office uses include those listed in Table 10-21-3C (4), List of Typical Uses in Office Category.
- (b) Small-Scale Industry. A use involving small scale manufacturing, production, assembly, and/or repair with little to no noxious by-products that includes a showroom or small retail outlet. Small-Scale Industry includes such uses as those found in Table 10-21-3C (5). List of Typical Uses in Cottage Industry Category. The following development standards apply:
 - i. This use may also include associated facilities such as offices and small scale warehousing, but distribution is limited to vans and small trucks. Distribution access shall be from the rear.
 - ii. The maximum overall gross floor area is limited to 10,000 square feet, unless otherwise noted.
 - iii. The showroom or retail outlet shall be located in the front storefront of the building and shall occupy a minimum of twenty five percent (25%) of the gross floor area.

5. **Infrastructure & Open Space Uses.**

- (a) Communication Antennae & Towers. Refer to Section 10-18. Communication Antennae & Towers located within the right-of-way or on utility poles are permitted.
- (b) Parking Lot. A lot that does not contain a permitted building and is solely used for the parking of vehicles. In the districts where a parking lot requires a special use permit ("O"), the following applies:
 - i. Corner Lots. A corner lot shall not be used as a parking lot.
 - ii. Adjacent Parking Lots. Two (2) parking lots cannot be located directly adjacent to one another.
 - iii. Single Family. Parking lot cannot be associated with a single family use.
 - iv. Distance. Parking lot must be within 1,300 feet of the principal entrance to the associated use unless:
 - (i) At least seventy five percent (75%) of the spaces are dedicated for public use.
 - (ii) An approved joint parking agreement is in place.
 - v. Pedestrian Access. Must be connected to associated use by a dedicated, public pedestrian pathway.
 - vi. Commercial Vehicles. Parking lots for commercial

vehicles are not permitted in Downtown Overlay Districts.

- (c) Parking Structure. A parking structure on a lot that does not contain a permitted Building Type and is solely used for the parking of vehicles. In the districts where a parking lot requires a special use permit ("O"), the following applies:
 - i. Corner Lots. A corner lot shall not be used for a parking structure.
 - ii. Adjacent Parking Lots. Two (2) parking facilities (lots or structures) cannot be located directly adjacent to one another.
 - iii. Primary Street. No facade of the Parking Structure shall be located on a Primary Street. Refer to Figure 10-21-2G (4). For Parking Structures located on a Primary Street, the Main Street Building Type shall be utilized, requiring occupation of the building in the front thirty (30) feet of the facade on any Primary Street.
 - iv. Distance. Parking lot must be within 1,300 feet of the principal entrance to the associated use unless:
 - (i) At least seventy five percent (75%) of the spaces are dedicated for public use.
 - (ii) An approved parking agreement is in place (refer to Section 10-16).
 - v. Pedestrian Access. Must be connected to associated use by a dedicated, public pedestrianway.
 - vi. Commercial Vehicles. Parking structures for commercial vehicles are not permitted in these districts.
- (d) Utility and Infrastructure. A lot that is primarily utilized for the City's infrastructure needs. Utility and infrastructure includes such uses as electric or gas services, sewage treatment, water treatment and storage, and energy conversion systems.

6. **Open Space Uses.** A use of land for active or passive, public or private, outdoor space, including such uses as parks, plazas, greens, playgrounds, or community gardens. Open space uses may also be utilized to host temporary private or community events, such as a farmer's market or art fair. In the districts where open space is permitted with development standards ("O"), the following apply:

- i. Stormwater Accommodations. Open space that incorporates stormwater management on a site or district scale is encouraged.
 - (i) Stormwater facilities shall be designed to accommodate additional uses, such as an amphitheater or a sports field.
 - (ii) Stormwater facilities shall be designed not to be fenced and shall not impede public use of the land they occupy.
- ii. This use may involve small scale food and beverage service, no more than two hundred (200) square feet in space, located in a kiosk, with no service access.
- iii. Buildings located directly adjacent to an open space use shall treat facades facing this use with street facade requirements.

7. **Accessory Uses.** A category of uses that are not permitted to serve as the principal use on a zoning lot. These uses are incidental to

10-21-3 Uses

and customary in connection with the principal building or use and located on the same lot with such principal building or use.

(a) Alternative Energy Generation. Refer to Section 10-19.

Alternative energy generation includes structures for solar, wind, and geothermal. The following development standards apply:

- i. Roof or Building Mounted Solar panels are permitted with the following requirements:
 - (i) Panels mounted at the same angle of the roof or flush on the building facade are permitted on any roof or building face.
 - (ii) Panels projecting off the roof or building at a different angle are limited to the rear and side facing roofs, unless solar access is limited in these locations.
 - (iii) Panels shall not extend more than ten (10) feet at maximum pitch/tilt above the surface of the roof to which they are attached. On pitched roofs, panels shall not extend more than three (3) feet.
- ii. Roof Mounted Small Wind Energy Systems are permitted with the following requirements:
 - (i) Maximum rated capacity of five kW is permitted per turbine.
 - (ii) One turbine is permitted for each seven hundred and fifty (750) square feet of roof area.
 - (iii) Maximum height is fifteen (15) feet above the surface of the roof.
- iii. Geothermal Energy is permitted in any yard with the following requirements:
 - (i) Any related above ground structure shall be located in a side or rear yard with a maximum height of ten (10) feet, subject to all requirements of the Building Type.

(b) Amphitheater. Refer to Section 10-2-3 for definition. In the districts where an amphitheater is permitted with a special use permit "O," the following apply:

- i. Location. The amphitheater shall be located within an Open Space District (refer to Section 10-10).
- ii. The hours of operation shall be posted and limited to the hours of 8:00 a.m. to 11:00 p.m.

(c) Car Washes, Detail Shops and/or Service Stations. Refer to Section 10-2-3 for definitions. In the districts where car washes, detail shops and/or service stations are permitted with a special use permit ("O"), the following apply:

- i. Location. Use must be located inside a parking garage and not visible from the exterior of the parking garage.

(d) Drive-through Facility. In the districts where drive-through structures are permitted with development standards ("C"), the following applies. Refer to Figure 10-21-3C (1) for one illustration of the following requirements.

- i. Structure/Canopy. Drive-through structures or canopies shall be located on the rear facade of the building or in the rear of the lot behind the building, where permitted by use. The structure shall not be visible from any Primary Street.

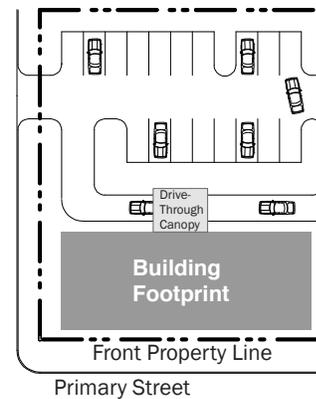


Figure 10-21-3C (1). Recommended drive-through Facility layout.

- ii. Stacking Lanes. Stacking lanes shall be located perpendicular to the Primary Street or behind the building.
 - iii. The canopy and structure shall be constructed of the same materials utilized on the building.
- (e) Home Occupation. An occupational use that is clearly subordinate to the principal use as a residence and does not require any alteration to the exterior of a building.
- (f) Outdoor Storage of Goods.
- i. Commercial Uses. This use includes outdoor storage of goods not typically housed or sold indoors, such as large scale materials and building and landscape supplies. In the districts where Outdoor Storage of Goods is permitted with development standards ("C"), the following apply:
 - (i) Loose materials shall not be stacked higher than six (6) feet.
 - (ii) Loose materials shall at a minimum be stored in a three sided shelter and shall be covered.
 - (iii) Materials shall be set back a minimum of ten (10) feet from any property line.
 - (iv) Materials shall not be located on required parking spaces or associated drive aisles.
 - (v) All outdoor storage areas shall be screened from view of adjacent parcels and vehicular rights-of-way using the Side or Rear Buffer.
 - ii. Residential Uses. This use includes temporary storage of personal or household goods in personal on demand storage (PODS) containers. In the districts where outdoor storage of goods is permitted with development standards ("C"), the following development standards apply:
 - (i) One (1) storage container can be located on a lot at a time.
 - (ii) On site storage of a container is permitted for up to two (2) months per year.
 - (iii) Containers shall not be located in the Front Yard, but permitted on a driveway.
- (g) Outdoor Restaurant Dining. Commercial outdoor dining, serviced by an adjacent service use, and permitted in any

- yard. When in a side yard along the front Lot Line, the Patio Frontage buffer is required.
- (h) **Parking Lot.** An uncovered paved surface used solely for the parking of vehicles, intended for use by the occupants in an adjacent building on the lot. Parking lot locations are regulated by Building Type. Refer to Section 10-21-4.
 - (i) **Parking Structure.** A structure used solely for the parking of vehicles, intended for use by the occupants in an adjacent building on the lot. Parking Structures within the buildings are regulated per Building Type. Refer to Section 10-21-4.
 - (j) **Roadside Produce Stand or Vending.** Refer to Section 10-21-1 for definition.
 - (k) **Sidewalk Sales.** A temporary outdoor sale of merchandise by retail businesses, typically occurring on the sidewalk within the public right-of-way for a limited period of time. In the districts where a Sidewalk Sale is permitted with development standards “●,” the following apply:
 - i. A minimum of four (4) feet of the sidewalk must remain available for passing pedestrians.
 - ii. Sidewalk Sales are permitted for up to three (3) days no more than six (6) times per calendar year.
 - iii. Merchandise must be stored inside the building during non-business hours.
 - iv. Merchandise sold shall either be regularly carried within the store, or have been regularly carried within the store in the past year.
 - (l) **Temporary Storage Containers.** Outdoor storage of personal or retail stock materials on a temporary basis within a portable storage container. When a Temporary Outdoor Storage use is permitted with development standards “●,” the following apply.
 - i. One (1) storage container may be located on a lot at a time.
 - ii. On site storage of a container is permitted for up to two (2) weeks; up to four (4) weeks is permitted with approval of the Director.
 - iii. The container shall be located in the rear yard, screened by the building from any primary or secondary street.
 - (m) **Mobile Food Vendors.** A motorized or towed wheeled vehicle that is designed and equipped to sell food. Shall include both “hot trucks” upon which food is cooked and prepared for vending, and “cold trucks” from which only ready to eat or packaged foods are handled. Refer to Section 10-3-14.

10-21-3 Uses

Neighborhood Retail

(less than 8,000 square feet)

Antique Shop
 Art & Education Supplies
 Art Gallery
 Bakery, Retail
 Bicycle Sales & Repair
 Book, Magazine, & Newspaper Store
 Camera & Photo Supply Store
 Candy Store
 China & Glassware Shop
 Clothing & Clothing Accessories
 Coffee Shop
 Computer Software Sales
 Delicatessen
 Electronic Sales & Service
 Fabric & Craft Store
 Florist
 Garden Center/Plant Nursery
 Gift, Novelty, & Souvenir Shop
 Hardware Store
 Hobby Shop
 Jewelry Sales & Repair
 Luggage & Leather Goods
 Microbreweries, Microwineries
 Music Store & Instruction
 Musical Instrument Repair & Sales
 Office Supply
 Optical Goods
 Paint & Wallpaper
 Party Supply Shop
 Pet & Pet Supply
 Specialty Food Market (Butcher, Fish Market, Produce, etc.), with no alcohol sales
 Sporting Goods Sales & Rental
 Stationary & Paper Store
 Toy Shop
 Video/Game Sales
 Wine Establishment (no liquor/beer)

General Retail

All Neighborhood Retail (over 8,000 square feet)
 Appliance Sales & Service
 Department Store
 Drug Store/Pharmacy
 Furniture & Home Furnishings
 General Merchandise Stores
 Grocery Store
 Medical Supply Store
 Used Merchandise Stores (not including books, clothing & antiques)

Recreation/Entertainment

Billiard Parlor
 Boat Sales & Rental
 Boat Storage
 Indoor Playground
 Indoor Shooting Gallery/ Gun Range
 Marina
 Outdoor Music Venue
 Railroad Passenger Station
 Skating Rink
 Stadium
 Swimming Pool

Table 10-21-3C (3). List of Typical Uses in Recreation/Entertainment Category

Office

Architecture/Engineering/Design
 Broadcasting & Telecommunications Studio
 Building Contractor (office only)
 Business Consulting
 Charitable Institutions
 Clinic
 Computer Programming & Support
 Detective Services
 Educational Services (tutor & testing)
 Employment Agency
 Financial & Insurance
 Government Offices
 Legal Services
 Management Services
 Medical & Dental with Laboratory
 Motion Picture & Sound Recording Studio
 PR & Advertising
 Professional Services
 Property Development
 Real Estate
 Research & Development
 Research Agency
 Surveying
 Tax Services

Table 10-21-3C (4). List of Typical Uses in Office Category.

Table 10-21-3C (1). List of Typical Uses in Retail Category.

Neighborhood Service

(less than 8,000 square feet)

Amusement Arcade
 Barber Shop & Beauty Salon
 Catering/Carry-Out & Delivery
 Communication Service
 Childcare Centers
 Dance/Fitness Studio
 Dry Cleaning & Laundry
 Emergency Care Clinic
 Framing
 Health & Fitness Center
 Home Furniture & Equipment Repair
 Locksmith
 Mailing Services
 Massage Establishments
 Nail Salon & Specialty Body
 Pet Grooming
 Photocopying & Printing
 Photography Studio & Supplies
 Physical Therapy/Physical Rehabilitation
 Repair of Small Goods & Electronics
 Restaurant/Bar
 Services (Spa)
 Shoe Repair
 Tailor & Seamstress
 Theater
 Travel Agency, Ticketing & Tour Operator
 Veterinarian (no outdoor training, boarding or kennels)

General Service

All Neighborhood Services (over 8,000 square feet)
 Bowling Alley

Table 10-21-3C (2). List of Typical Uses in Service Category.

Small-Scale Industry

Apparel & Finished Fabric Products
 Bakery & Confections
 Beverages, including Beer, Wine, Coffee
 Botanical Products
 Brooms & Brushes
 Canning & Preserving Food
 Dairy Products
 Electrical Fixtures
 Engraving
 Fabricated Metal Products
 Film Making
 Furniture & Fixtures
 Glass
 Jewelry, Watches, Clocks, & Silverware
 Leather Products
 Microbreweries, Microwineries
 Musical Instruments & Parts
 Pottery, Ceramics, & Related Products
 Research Laboratory
 Shoes & Boots
 Signs & Advertising
 Smithing
 Taxidermy
 Toys & Athletic Goods
 Upholstery
 Woodworking

Table 10-21-3C (5). List of Typical Uses in Small-Scale Industry Category.

This page intentionally left blank

DRAFT

10-21-4 Building Types

4.0 Building Types

A. Introduction to Building Type Standards

The buildings detailed in this section outline the Building Types permitted for new construction and renovated structures within the Districts defined in Section 10-21-2.

1. **General.** All Building Types shall meet the following requirements to achieve the intents defined for the districts.
 - (a) **Zoning Districts.** Each Building Type shall be constructed only within its designated districts. Refer to Table 10-21-4A (1) Permitted Building Types by Districts.
 - (b) **Uses.** Each Building Type can house a variety of uses depending on the district in which it is located. Refer to 15.3 for uses permitted per district. Some Building Types have additional limitations on permitted uses.
 - (c) **No Other Building Types.** All buildings constructed shall meet the standards of one of the Building Types within the zoning district of the lot.
 - (d) **Permanent Structures.** All buildings constructed shall be permanent construction without a chassis, hitch, or wheels, or other features that would make the structure mobile, unless otherwise noted.
 - (e) **Accessory Structures.**
 - i. Attached accessory structures are considered part of the

principal structure.

- ii. Detached accessory structures are allowed per each Building Type and shall comply with all setbacks except the following:
 - (i) Detached accessory structures are not permitted in the front yard.
 - (ii) Detached accessory structures shall be located behind the principal structure in the rear yard.
 - (iii) Detached accessory structures shall not exceed the height or size (area) of the principal structure.

2. **Page Layout.** Document pages are laid out to provide the maximum amount of information for each building type on one spread of two pages. Refer to Figure 10-21-4A (1) for a typical Building Type layout page.
 - (a) **Tables.** Refer to Section 10-21-4B for further information on each Table.
 - (b) **Graphics** typically represent one example of a building that could be developed utilizing the standards for that building type. Graphics are provided to illustrate general intent. In all cases, tables and text supercede graphic representations.

Building Types by Districts						
		Districts				
		S1: Bridge Street	S2: Hydraulic Street	S3: Van Emmon Street	S4: 'B' Street	S5: 'B' Street - Residential
Building Types	Downtown Commercial	●	●	●	●	
	Downtown Living		●	●	●	
	Cottage Commercial	●	●	●	●	
	Civic Building	●	●	●	●	●
	Attached Building			●	●	○
	Yard Building				○	●

Permitted Requires Permit / Review

Table 10-21-4A (1). Permitted Building Types by District

How To Use The Code

Page Layout

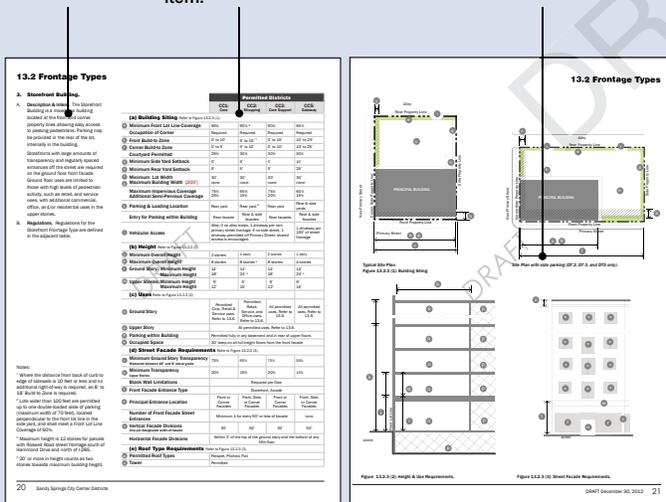
Building Type pages are laid out to provide the maximum amount of information for each Building Type on one spread of two pages.

Graphics typically represent one example of a building that could be developed utilizing the standards for that Building Type. Graphics are provided to illustrate general intent. In all cases, tables and text supercede graphic representations.

Tables define the regulations for each Building Type. Refer to 10-21-4B Explanation of Building Type Table Standards for narrative explanation of each line item.

Graphics related to the tables on the left. Graphics typically represent one example of Building Type standards

Intent of the Building Type.



One Building Type per Spread of Two Pages

Figure 10-21-4A (1). Representative Building Types Spread.

Building Type Tables

Refer to 10-21-4B for an explanation of each line item on the tables. Building Type tables contain all the standards for that Building Type. They are organized by district, so only the information in a parcel's district is applicable to that parcel. The other columns do not apply.

For example, a parcel in CC1 only applies the standards from that column, illustrated below.

	Permitted Districts		
	CC1: Core	CC2: Shopping	CC3: Core Support
(a) Building Siting Refer to Figure 13.2.2 (1).			
a Minimum Front Lot Line Coverage	95%	90% ²	90%
Occupation of Corner	Required	Required	Required
b Front Build-to Zone	0' to 10'	5' to 15' ¹	0' to 15'
c Corner Build-to Zone	0' to 5'	5' to 10'	0' to 10'
Courtyard Permitted	25%	30%	30%
d Minimum Side Yard Setback	0'	5'	5'
e Minimum Rear Yard Setback	5'	5'	5'
f Minimum Lot Width	30'	30'	30'
g Maximum Building Width	none	none	none
Maximum Impervious Coverage	75%	65%	75%
Additional Semi-Pervious Coverage	25%	15%	20%
h Parking & Loading Location	Rear yard	Rear yard ²	Rear yard
Entry for Parking within Building	Rear facade	Rear & side facades	Rear facades
i Vehicular Access	Alley; if no alley exists, 1 driveway per non-primary street frontage; if no side street, 1 driveway permitted off Primary Street; shared access is encouraged.	Alley; if no alley exists, 1 driveway per non-primary street frontage; if no side street, 1 driveway permitted off Primary Street; shared access is encouraged.	Alley; if no alley exists, 1 driveway per non-primary street frontage; if no side street, 1 driveway permitted off Primary Street; shared access is encouraged.
(b) Height Refer to Figure 13.2.2 (2)			
j Minimum Overall Height	2 stories	1 story	2 stories
k Maximum Overall Height	8 stories	8 stories ³	8 stories
l Ground Story: Minimum Height	14' 18'	14' 24' ⁴	14' 18'
m Upper Stories: Minimum Height	9' 12'	9' 15'	9' 12'
n Maximum Height			
(c) Uses Refer to Figure 13.2.2 (2)			
o Ground Story	Permitted Civic, Retail & Service uses. Refer to 13.6.	Permitted Retail, Service, and Office uses. Refer to 13.6.	All permitted uses. Refer to 13.6.
p Upper Story	All permitted uses. Refer to 13.6.	All permitted uses. Refer to 13.6.	All permitted uses. Refer to 13.6.
q Parking within Building	Permitted fully in any basement and in rear of upper floors	Permitted fully in any basement and in rear of upper floors	Permitted fully in any basement and in rear of upper floors
r Occupied Space	30' deep on all front facade	full height floors from the front facade	full height floors from the front facade
(d) Street Facade Requirements Refer to Figure 13.2.2 (3).			
s Minimum Ground Story Transparency Measured between 18" and 8' above grade	75%	65%	75%
t Minimum Transparency Upper Stories	20%	15%	20%
Blank Wall Limitations		Required per floor	Required per floor
u Front Facade Entrance Type		Storefront, Arcade	Storefront, Arcade
v Principal Entrance Location	Front or Corner Facades	Front, Side, or Corner Facades	Front or Corner Facades
w Number of Front Facade Entrances	Minimum 1 for every 60' or less of facade		
x Vertical Facade Divisions One per designated width of facade	30'	50'	30'
y Horizontal Facade Divisions	Within 3' of the top of the ground story and the bottom of any fifth floor		
(e) Roof Type Requirements Refer to Figure 13.2.2 (3).			
z Permitted Roof Types	Parapet, Pitched, Flat		
aa Tower	Permitted		

Figure 10-21-4A (2). Building Type Table Example.

10-21-4 Building Types

B. Explanation of Building Type Table Standards

The following explains and further defines the standards outlined on the tables for each Building Type, refer to Sections 10-21-4C through 10-21-4H.

1. **Building Siting.** The following explains the line item requirements for each Building Type Table within the first section entitled "Building Siting". Table 10-21-4B (1) illustrates an example of a Building Siting Table from a typical Building Type.
 - (a) Multiple Principal Structures. The allowance of more than one (1) principal structure on a lot.
 - (b) Minimum Front Lot Line Coverage. Refer to Figure 10-21-4B (1). Measurement defining the minimum percentage of street wall or building facade along the street. The width of the principal structure(s) (as measured within the front build-to zone) shall be divided by the maximum width of the front build-to zone (BTZ).
 - i. Certain buildings have this number set to also allow the development of a courtyard along the front lot line.
 - ii. Some frontage types allow side yard parking to be exempted from the front lot line coverage calculation. If such an exemption is permitted, the width of up to one (1) double loaded aisle of parking, located with the drive perpendicular to the street and including adjacent sidewalks and landscaping, may be exempted, to a set maximum in feet.
 - iii. When driveway is located at the front lot line (Figure 10-21-4B (1)) and a side yard parking is not utilized, a driveway width of twenty (20) feet may be deducted from the width of the BTZ and is not included in the calculation of the minimum front lot line.
 - (c) Occupation of Corner. Occupying the intersection of the front and corner build-to zones with a principal structure.
 - (d) Front Build-to Zone. The build-to zone or setback parallel to the front lot line. Building components, such as awnings or signage, are permitted to encroach beyond the build-to zone.
 - i. All build-to zone and setback areas not covered by building shall contain either landscape, patio space, or sidewalk space.
 - (e) Corner Build-to Zone. The build-to zone or setback parallel to the corner side property line.
 - i. All build-to zone and setback areas not covered by building shall contain either landscape, patio space, or sidewalk space.
 - (f) Minimum Side Yard Setback. The minimum required setback along a side property line.
 - i. All build-to zone and setback areas not covered by building shall contain either landscape, patio space, or sidewalk space.
 - (g) Minimum Rear Yard Setback. The minimum required setback along a rear property line.
 - i. All build-to zone and setback areas not covered by building shall contain either landscape, patio space, or sidewalk space.
 - (h) Minimum Lot Width. The minimum width of a lot, measured at the build-to zone.

Permitted Districts	
District A	District B

(a) Building Siting		
Multiple Principal Buildings	Not permitted	Not permitted
Minimum Front Lot Line Coverage	95%	65%
Occupation of Corner	Required	Required
Front Build-to Zone	0' to 20' ¹	5' to 20' ¹
Corner Build-to Zone	0' to 5'	0' to 10'
Minimum Side Yard Setback	0'	5'
Minimum Rear Yard Setback	5'; 25' if located adjacent to residential	
Minimum Lot Width	30'	30'
Maximum Building Width	none	none
Maximum Impervious Coverage	75% ³	65% ³
Additional Semi-Pervious Coverage	25%	15%
Parking & Loading Location	Rear yard	Rear yard
Entry for Parking within Building	Rear & Side Facades	
Vehicular Access	Alley; if no alley exists, 1 driveway per street frontage	

Table 10-21-4B (1). Example Building Siting Requirements Table from a Typical Building Type.

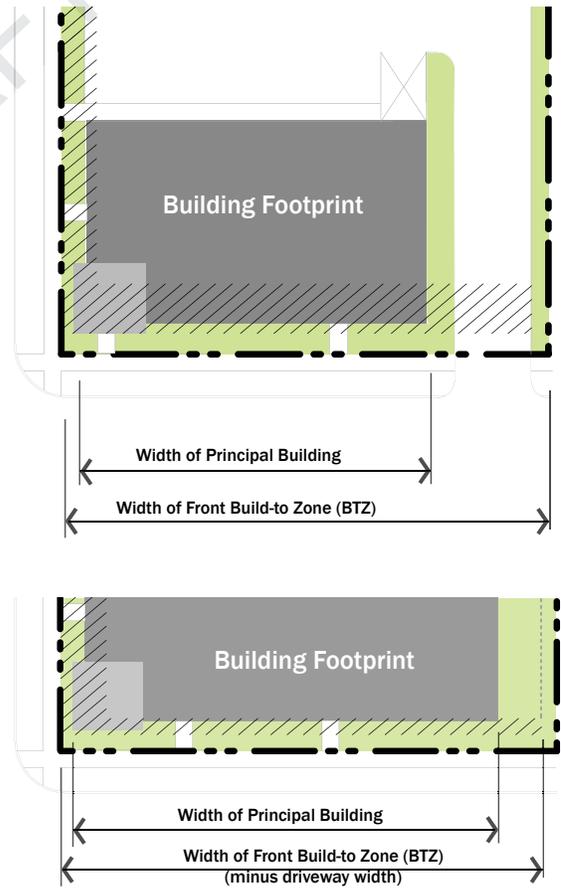


Figure 10-21-4B (1). Measuring Front Lot Line Coverage

10-21-4 Building Types



Figure 10-21-4B (2). Maximum Impervious & Additional Semi-Pervious Coverage.

- (i) Maximum Building Width. The maximum width of a building, measured across the front facade.
 - (j) Maximum Impervious Coverage. (Refer to Figure 10-21-4B (2)). The maximum percentage of a lot permitted to be covered by principal structures, accessory structures, pavement, and other impervious surfaces.
 - (k) Additional Semi-Pervious Coverage. The additional percentage of a lot beyond the Maximum Impervious Coverage, which may be surfaced in a semi-pervious material, including a green roof or pavers.
 - (l) Parking & Loading Location. The yard in which a surface parking lot, detached garage, attached garage door access, loading and unloading, and associated drive is permitted.
 - (m) Entry for Parking within Building. Permitted garage door location for parking entrance when parking is located within building.
 - (n) Vehicular Access. The permitted means of vehicular ingress and egress to the lot.
 - i. Alleys, when present, shall always be the primary means of access.
 - ii. When alleys are not present, a driveway may be permitted per Building Type and, if an alternative is available, shall not be located off a Primary Street.
2. **Height.** The following explains the line item requirements for each Building Type Table within the second section entitled "Height". Table 10-21-4B (2), illustrates an example of a Height Requirements Table from a typical Building Type.
- (a) Minimum Height (in Stories). The minimum overall height for

	Permitted Districts	
	District A	District B
(b) Height		
Minimum Overall Height	2 stories	2 story
Maximum Overall Height	5 stories	5 stories
Ground Story: Minimum Height	14'	14'
Maximum Height	18'	18'
Upper Stories: Minimum Height	9'	9'
Maximum Height	12'	12'

Table 10-21-4B (2). Example Height Requirements Table from a Typical Building Type.

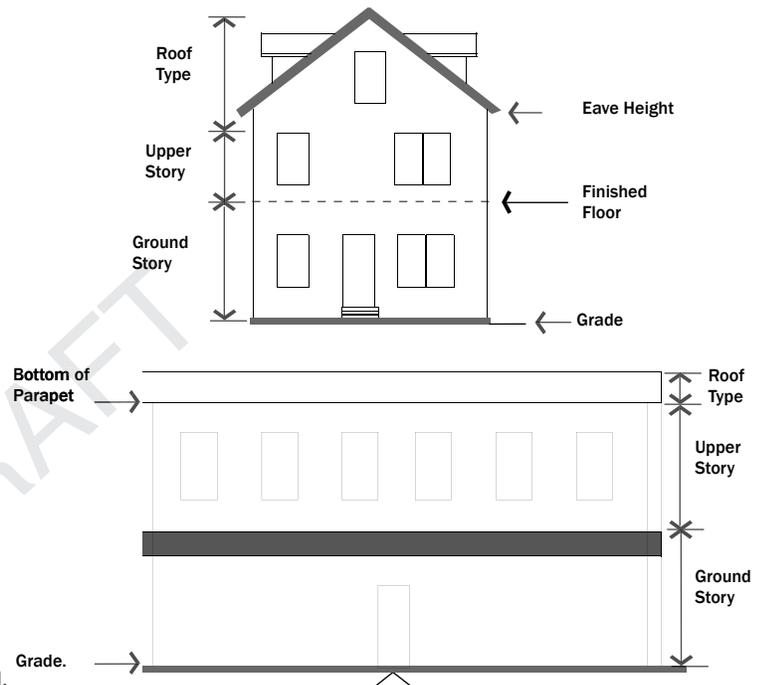


Figure 10-21-4B (3). Measuring Height

the building shall be located within the build-to zone; stories above the minimum height may be stepped back from the facade.

- (b) Maximum Height (in Stories). The sum of a building's total number of stories.
 - i. Half stories are located either completely within the roof structure with street-facing windows or in a visible basement exposed a maximum of one half story above grade.
 - ii. A building incorporating both a half story within the roof and a visible basement shall count the height of the two (2) half stories as one (1) full story.
 - iii. Some Building Types require a building facade to step back as its height increases. The upper stories of any building facade with street frontage shall be setback a designated amount beyond the building facade of the lower stories.

10-21-4 Building Types

- iv. Floors within the building shall be visibly designated on the street facades by the use of expression lines or the layout of the windows.
- (c) Ground Story and Upper Story, Minimum and Maximum Height. (Refer to Figure 10-21-4B (3)). Each frontage type includes a permitted range of height in feet for each story. Additional information is as follows:
 - i. Floor height is measured in feet between the floor of a story to the floor of the story above it.
 - ii. For single story buildings and the uppermost story of a multiple story building, floor to floor height shall be measured from the floor of the story to the tallest point of the ceiling.
 - iii. Double height spaces may be located along any non-street facade or in the entrance way to the building. The entrance way shall not exceed fifty percent (50%) of the street facade.

3. **Uses.** The following explains the line item requirements for each Building Type Table within the third section entitled “Uses.” Refer to Section 10-21-3 for uses permitted within each District. The requirements in this section of the Building Type Tables may limit those uses within a specific Building Type. Table 10-21-4B (3) illustrates an example of the Uses table from a typical Building Type.

- (a) Ground and Upper Story. The uses or category of uses which may occupy the ground and/or upper story of a building.
- (b) Parking Within Building. The area(s) of a building in which parking is permitted within the structure.
- (c) Occupied Space. The area(s) of a building that shall be designed as occupied space, defined as interior building space regularly occupied by the building users. It does not include storage areas, utility space, or parking.

4. **Street Facade Requirements.** The following explains the line item requirements for each Building Type Table within the fourth section entitled “Street Facade Requirements.” These requirements apply only to facades facing a public or private street right-of-way. The rear or interior side yard facades are not required to meet these standards unless otherwise stated. Table 10-21-4B (4) illustrates an example of a Street Facade Requirements Table from a typical Building Type.

- (a) Minimum Ground Story and Upper Floor Transparency. (Refer to Figure 10-21-4B (5)). The minimum amount of transparency on street facades with street frontage.
 - i. Transparency is any glass in windows and/or doors, including any mullions, that is highly transparent with low reflectance.
 - (i) Ground Story Transparency, when defined separately from the overall minimum transparency, shall be measured between two (2) feet and eight (8) feet from the base of the front facade.
 - (ii) A general Minimum Transparency requirement shall be measured from floor to floor of each story.
- (b) Blank Wall Limitations. A restriction of the amount of windowless area permitted on a facade with street frontage. If required, the following shall both be met for each story:

- i. No rectangular area greater than thirty percent (30%) of a story’s facade, as measured from floor to floor, may be windowless; and
- ii. No horizontal segment of a story’s facade greater than fifteen (15) feet in width may be windowless.

5. **Entrance Type.**

- (a) Front Facade Entrance Type. The Entrance Type(s) permitted for the entrance(s) of a given Building Type. A mix of permitted Entrance Types may be utilized. Refer to Section 10-21-4I Entrance Types for definition of and additional requirements for each.
- (b) Principal Entrance Location. The facade on which the primary building entrance is to be located.

	Permitted Districts	
	District A	District B
(c) Uses		
Ground Story	Per Section 15.3 Uses	
Upper Story	Per Section 15.3 Uses	
Parking within Building	Permitted fully in any basement and in rear of upper floors	
Occupied Space	20' deep on all full height floors from the front facade	

Table 10-21-4B (3). Example Uses Table from a Typical Building Type.

	Permitted Districts	
	District A	District B
(d) Street Facade Requirements		
Minimum Ground Story Transparency <i>Measured between 2' and 8' above grade</i>	65%	65%
Minimum Transparency <i>Upper Stories</i>	20%	20%
Blank Wall Limitations	Required per floor	
Front Facade Entrance Type	Storefront, Arcade	
Principal Entrance Location	Front or Corner Facades	
Number of Street Entrances	Minimum 1 for every 50' or less of facade	
Ground Story Vertical Divisions	One per every 30' of facade width	
Horizontal Facade Divisions	Within 3' of the top of the ground story and the bottom of any fifth floor	
Facade Variety Required <i>Refer to 15.4B.4(h) for requirements.</i>	Every 80' of facade width	

Table 10-21-4B (4). Example Street Facade Requirements Table from a Typical Building Type.

10-21-4 Building Types

- (c) Number of Street Entrances. The minimum number of and maximum spacing between entrances on the ground floor building facade with street frontage.
- (d) Ground Story Vertical Divisions. The use of a vertically oriented expression line or form to divide the ground floor facade into increments no greater than the dimension shown in Figure 10-21-4I (1), as measured along the base of the facade, and extending a minimum of eighty percent (80%) from the average grade of the facade elevation to the interior ceiling. Elements may include a column, pilaster, or other continuous vertical ornamentation.
- (e) Horizontal Facade Divisions. The use of a horizontally oriented expression line or form to divide portions of the facade into horizontal divisions, extending a minimum of ninety percent (90%) of the full width of the facade. Elements may include a cornice, belt course, molding, string courses, or other continuous horizontal ornamentation a minimum of one and a half inch depth.
- (f) Facade Variety Requirements. Building design shall vary between designated vertical facade divisions, where required per the Building Type, and from adjacent buildings by the type of dominant material or color, scale, or orientation of that material and at least two of the following. Refer to Figure 10-21-4B (5) for one illustration of this requirement.
 - i. The proportion of recesses and projections.
 - ii. The location of the entrance and window placement, unless storefronts are utilized.
 - iii. Roof type, plane, or material, unless otherwise stated in the Building Type requirements.

6. **Roof Type.** The following explains the line item requirements for each Building Type Table in Sections 10-21-4C through 10-21-4H, within the fifth section entitled "Roof Types." Table 10-21-4B (5) illustrates an example of a Roof Type Requirements Table from a typical Building Type.

- (a) Permitted Roof Type. The roof type(s) permitted for a given Building Type. Refer to Section 10-21-4J for more specific requirements.
- (b) Tower. A vertical building extension that may be permitted in conjunction with another roof type on certain Building Types. Refer to Section 10-21-4J.



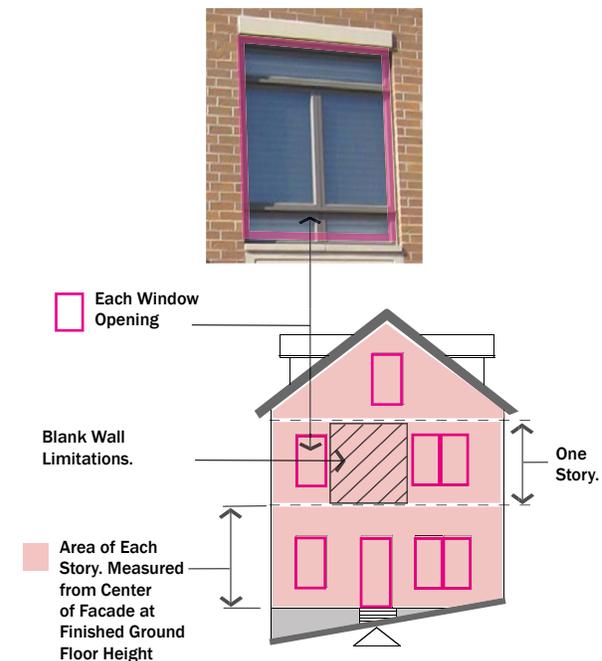
Figure 10-21-4B (4). Building Variety.

		Permitted Districts	
		District A	District B
(e) Roof Type Requirements			
Permitted Roof Types		Parapet, Pitched, Flat	
Tower		Permitted	

Table 10-21-4B (5). Example Roof Type Requirements Table from a Typical Building Type.



Measuring Ground Floor Transparency on a Storefront Base.



Measuring Transparency on Each Story with Slope.

Figure 10-21-4B (5). Measuring Transparency.

10-21-4 Building Types

C. Downtown Commercial Building

1. **Description & Intent.** The Downtown Commercial Building is a building located at the front and corner property lines allowing easy access to passing pedestrians. Parking may be provided in the rear of the lot, internally in the building, or, in some cases, no off-street parking is required.

Storefronts with large amounts of transparency and regularly spaced entrances off the street are utilized on the ground floor front facade. Ground floor uses are limited to those with high levels of pedestrian activity, such as retail, service, and office uses, with additional commercial, office, and/or residential uses in the upper stories.

2. **Regulations.** Regulations for the Downtown Commercial Building Type are defined in the adjacent table.

Notes:

¹ Each building shall meet all requirements of the Building Type.

² A max. six (6) foot gap is allowed if it serves as a walkable passage.

³ If located adjacent to flood hazard area, refer to requirements of the Yorkville Stormwater Ordinance No. 2012-56. Refer to FEMA National Flood Hazard Layer FIRMette maps for all flood hazard areas.

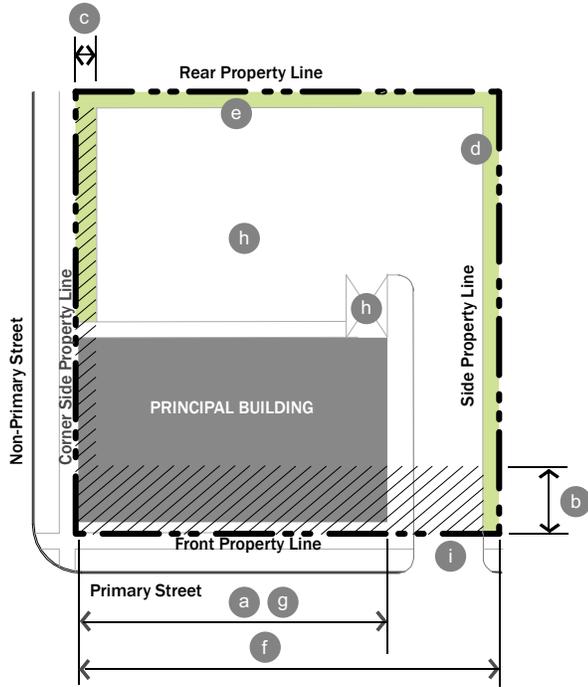
⁴ Maximum Impervious Coverage shall be applied only to all non-flood hazard areas. Refer to FEMA National Flood Hazard Layer FIRMette maps for all flood hazard areas.

⁵ Lots wider than one hundred (100) feet are permitted one double-loaded aisle of parking (maximum width of seventy (70) feet), located perpendicular to the front lot line and shall meet a Front Lot Line Coverage of sixty percent (60%).

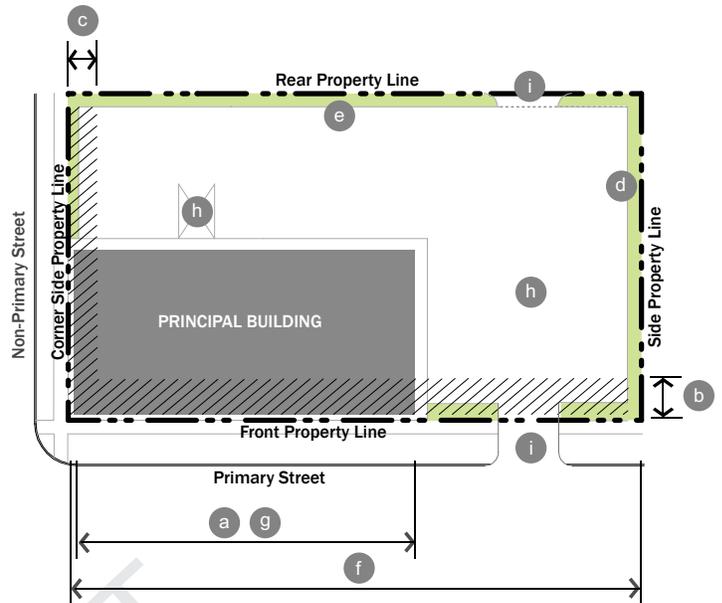
⁶ Eighteen (18) feet or more in height counts as two (2) stories towards maximum building height.

		Permitted Districts			
		S 1: Bridge Street	S 2: Hydraulic Street	S 3: Van Emmon Street	S 4: 'B' Street
(a) Building Siting Refer to Figures 10-21-4C (1) and 10-21-4C(2)					
Multiple Principal Buildings		Permitted ¹			
a	Minimum Front Lot Line Coverage	100% ²	75%	90% ²	65%
Occupation of Corner		Required			
b	Front Build-to Zone	0' to 5'			0' to 10'
c	Corner Build-to Zone	0' to 5'	0' to 10'		
d	Minimum Side Yard Setback	0'; 5' if adjacent to other Building Type		5'	
e	Minimum Rear Yard Setback	5'; 25' if located adjacent to residential ³			
f	Minimum Lot Width	none	16'		
g	Maximum Building Width	none	none		
Maximum Impervious Coverage		95% ⁴		80% ⁴	
Additional Semi-Pervious Coverage		5%		20%	
h	Parking & Loading Location	Rear yard; existing developed sites require no add'l parking and/or loading facilities		Rear Yard & Side Yard ⁵	
Entry for Parking within Building		Rear & Side Facades			All Sides
i	Vehicular Access	none	Alley; if no alley exists, 1 driveway per every 80' of frontage is permitted off non-Primary street; if no side street, 1 driveway permitted off Primary Street; shared access is encouraged.		
(b) Height Refer to Figure 10-21-4C (3)					
j	Minimum Overall Height	2 stories		1 story	
k	Maximum Overall Height	6 stories (with min. 3' setback at 3 stories)		4 stories	
l	Ground Story: Minimum Height	14'	12'	14'	12'
	Maximum Height	24' ⁶	24' ⁶	24' ⁶	24' ⁶
m	Upper Stories: Minimum Height	9'			
	Maximum Height	14'			
(c) Uses Refer to Figure 10-21-4C (3)					
n	Ground Story	Per Section 10-21-3 Uses; residential permitted provided that it is located at least 20' from the front primary facade			
o	Upper Story	Per Section 10-21-3 Uses			
p	Parking within Building	Permitted fully in any basement and in rear of all floors			
q	Occupied Space	20' deep on all full height floors from the front facade			
(d) Street Facade Requirements Refer to Figure 15.4C (4)					
r	Minimum Ground Story Transparency Measured between 2' and 8' above grade	50%			30%
s	Minimum Upper Story Transparency Street-Facing Stories	20%		15%	
t	Front Facade Entrance Type	Storefront, Elevated Storefront (permitted only on parcels in flood hazard areas per Section 18 of the City's Code of Ordinances)			
u	Principal Entrance Location	Front or Corner Facades			
Number of Street Entrances		Minimum 1 for every 30' or less of facade	Minimum 1 for every 50' or less of facade	Minimum 1 for every 80' or less of facade	
Ground Story Vertical Divisions		One per every 20-40' of facade width			
Horizontal Facade Divisions		Within 3' of top of ground story and the bottom of any fifth floor		Within 3' of top of ground story	
Facade Variety Required Refer to 10-21-4B(4)(h) for requirements.		Every 40' of facade width		Every 50' of facade width	
(e) Roof Type Requirements Refer to Figure 10-21-4C (4)					
v	Permitted Roof Types	Parapet, Pitched, Flat			
	Tower	Permitted			

10-21-4 Building Types



Typical Site Plan.
Figure 10-21-4C (1): Building Siting.



Site Plan with side parking (S 2 and S4 only).
Figure 10-21-4C (2): Building Siting.

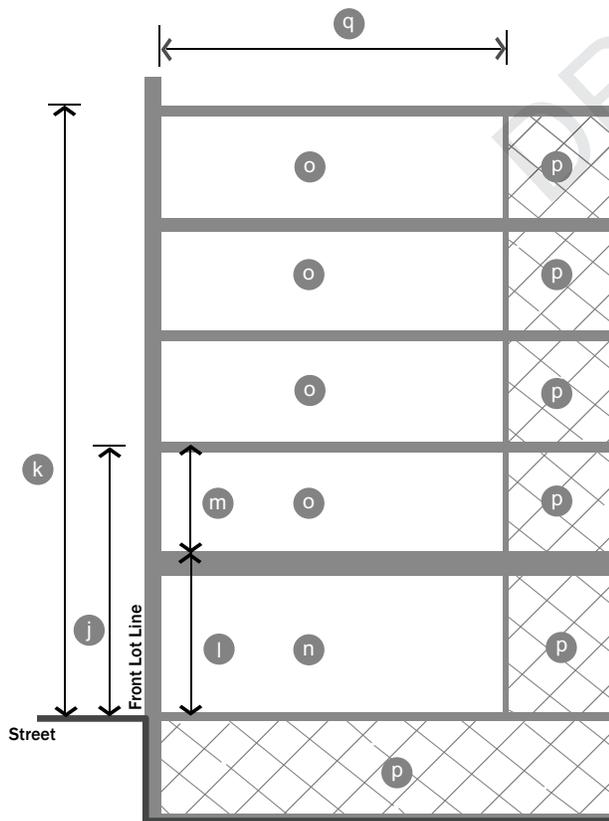


Figure 10-21-4C (3): Height & Use Requirements.

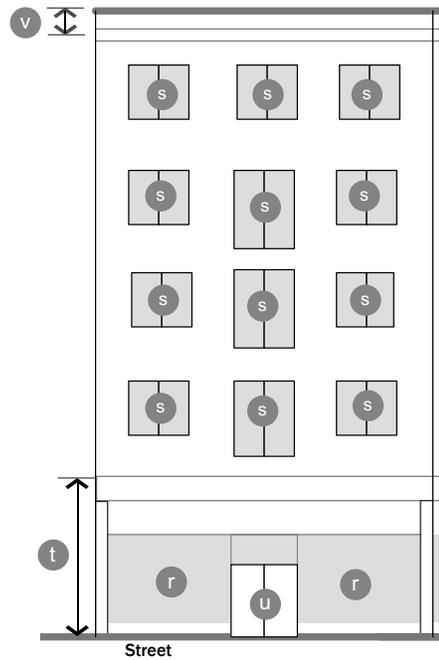


Figure 10-21-4C (4): Street Facade Requirements.

10-21-4 Building Types

D. Downtown Living Building

- Description & Intent.** The Downtown Living Building Type is limited in terms of uses by the district within which it is located, generally housing office and/or residential uses. Similar to the Downtown Commercial Building, the Downtown Living building is intended to be built close to the front and corner property lines, but generally allows for more landscape space between the building and the street. Parking may be provided in the rear of the lot, internally in the building, or, in some cases, one double loaded aisle of parking is permitted in the interior or the side yard at the front lot line.
- Regulations.** Regulations for the Downtown Living Building Type are defined in the adjacent table.

Permitted Districts		
S 2: Hydraulic Street	S 3: Van Emmon Street	S 4: 'B' Street

(a) Building Siting Refer to Figures 10-21-4D (1) and 10-21-4D (2)			
Multiple Principal Buildings		Permitted ¹	
a	Minimum Front Lot Line Coverage	75%	50%
Occupation of Corner		Required	
b	Front Build-to Zone	5' to 15'	5' to 20'
c	Corner Build-to Zone	0' to 10'	5' to 20'
d	Minimum Side Yard Setback	5'	10'
e	Minimum Rear Yard Setback	5'; 25' if located adjacent to residential ²	
f	Minimum Lot Width	16'	
g	Maximum Building Width	none	
Maximum Impervious Coverage		65% ³	75% ³
Additional Semi-Pervious Coverage		20%	20%
h	Parking & Loading Location	Rear yard ⁴ , Side Yard	
Entry for Parking within Building		Rear & Side Facades	
i	Vehicular Access	Alley; if no alley exists, 1 driveway per every 80' of frontage is permitted off non-Primary street; if no side street, 1 driveway permitted off Primary Street; shared access is encouraged.	
(b) Height Refer to Figure 10-21-4D (3)			
j	Minimum Overall Height	1 story	
k	Maximum Overall Height	4 stories	
l	All Stories: Minimum Height	9'	
	Maximum Height	14'	
(c) Uses Refer to Figure 10-21-4D (3).			
m	All Stories	Per Section 10-21-3 Uses; residential dwelling allowed provided that it is located at least 20' from the front primary facade	
n	Parking within Building	Permitted fully in any basement and in rear of all floors	
o	Occupied Space	20' deep on all full height floors from the front facade	
(d) Street Facade Requirements Refer to Figure 10-21-4D (4).			
p	Minimum Transparency Per each Story	20%	
q	Front Facade Entrance Type	Stoop, Porch	
r	Principal Entrance Location	Front facade or corner facade	
Number of Street Entrances		Minimum 1 for every 100' or less of facade	
Ground Story Vertical Divisions		Every 60' of facade width	
Horizontal Facade Divisions		Within 3' of the top of the ground story and any visible basement	
Facade Variety Required Refer to 10-21-4B(4)(h) for requirements.		Every 50' of facade width	
(e) Roof Type Requirements Refer to Figure 10-21-4D (4)			
s	Permitted Roof Types	Parapet, pitched, flat	
t	Tower	Permitted	

Notes:

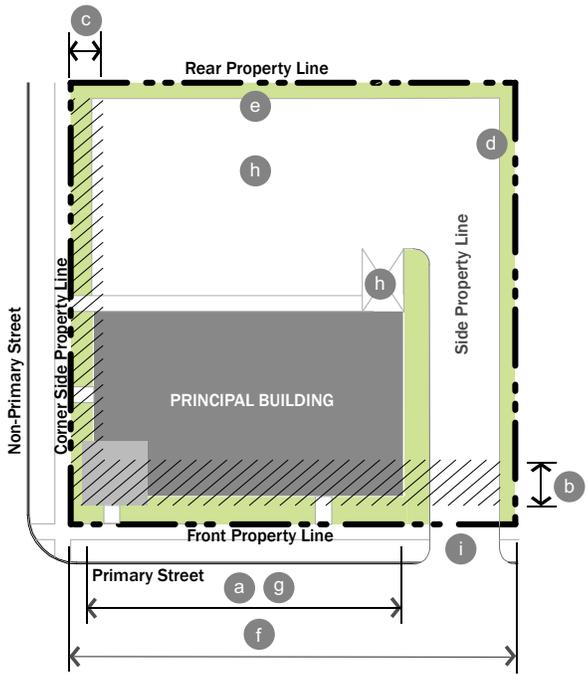
¹ Each building shall meet all requirements of the Building Type.

² If located adjacent to flood hazard area, refer to requirements of the Yorkville Stormwater Ordinance No. 2012-56. Refer to FEMA National Flood Hazard Layer FIRMette maps for all flood hazard areas.

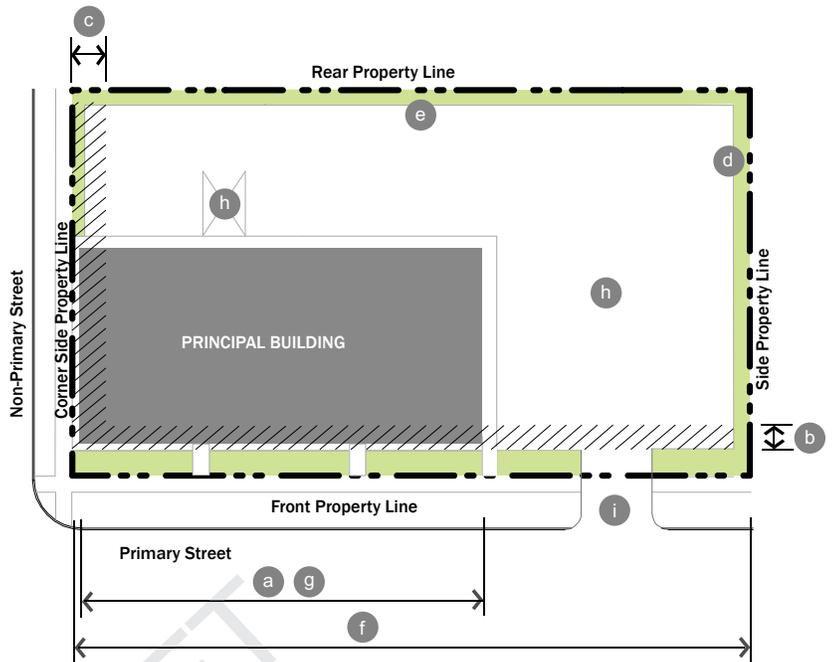
³ Maximum Impervious Coverage shall be applied only to all non-flood hazard areas. Refer to FEMA National Flood Hazard Layer FIRMette maps for all flood hazard areas.

⁴ Lots wider than one hundred (100) feet are permitted one double-loaded aisle of parking (maximum width of seventy (70) feet), located perpendicular to the front lot line and shall meet a Front Lot Line Coverage of sixty percent (60%).

10-21-4 Building Types



Typical Site Plan.
Figure 10-21-4D (1): Building Siting.



Site Plan with side parking (S 3 and S 4 only).
Figure 10-21-4D (2): Building Siting.

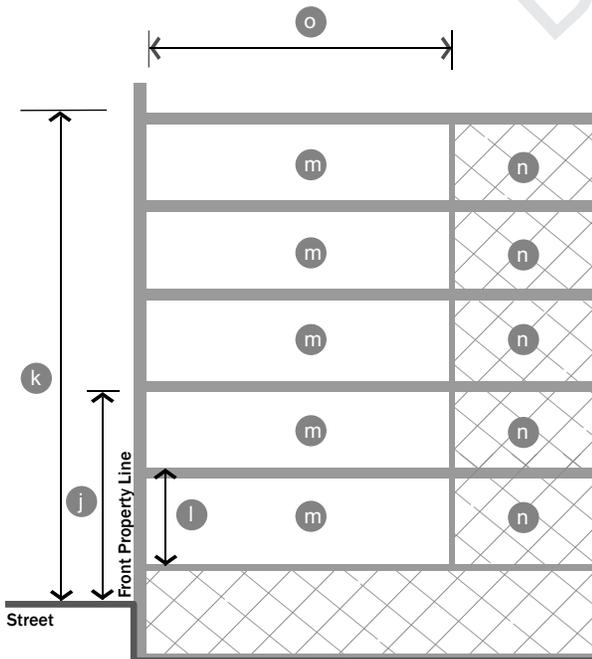


Figure 10-21-4D (3): Height & Use Requirements.

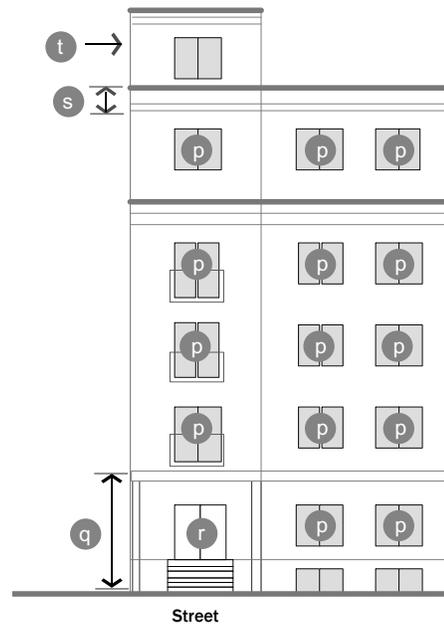


Figure 10-21-4D (4): Street Facade Requirements.

10-21-4 Building Types

E. Cottage Commercial Building

1. **Description & Intent.** The Cottage Commercial Building combines characteristics of the Downtown Commercial Building Type with physical characteristics of a residential cottage, such as a pitched roof and a front stoop or porch.

This lower-scale building has a pedestrian-friendly storefront, stoop, or porch entrance type with moderate transparency and a primary entrance that faces the street. Constructed with Setbacks similar to a residential cottage, this building typically has more landscape area than the Downtown Commercial Building Types. Parking is permitted in the rear of the lot or a side aisle (with conditions).

The Cottage Commercial Building may contain a mix of uses, including retail, service, and office uses on the ground floor, with residential uses on upper floors.

2. **Regulations.** Regulations for the Cottage Commercial building type are defined in the adjacent table.

Notes:

¹ Each building shall meet all requirements of the Building Type.

² If located adjacent to flood hazard area, refer to requirements of the Yorkville Stormwater Ordinance No. 2012-56. Refer to FEMA National Flood Hazard Layer FIRMette maps for all flood hazard areas.

³ Maximum Impervious Coverage shall be applied only to all non-flood hazard areas. Refer to FEMA National Flood Hazard Layer FIRMette maps for all flood hazard areas.

⁴ Lots wider than eighty (80) feet are permitted one single-loaded aisle of parking (maximum width of forty (40) feet), located perpendicular to the front lot line, and shall meet a Front Lot Line Coverage of fifty percent (50%).

⁵ Attached garages are considered part of the principal building and shall meet all setbacks. Detached garages shall meet all setbacks unless an alley is present. When an alley is present, detached garages shall have a minimum rear setback of five (5) feet.

		Permitted Districts			
		S 1: Bridge Street	S 2: Hydraulic Street	S 3: Van Emmon Street	S 4: 'B' Street
(a) Building Siting Refer to Figure 10-21-4E (1)					
Multiple Principal Buildings		Refer to District Street Details			
a	Minimum Front Lot Line Coverage	60%	50%		40%
Occupation of Corner		Required			
b	Front Build-to Zone	5' to 10'	5' to 20'		
c	Corner Build-to Zone	5' to 10'	5' to 25'		5' to 15'
d	Minimum Side Yard Setback	10'	15'		
e	Minimum Rear Yard Setback	25' ²			
f	Minimum Lot Width	25'			
g	Maximum Building Width	50'			
Maximum Impervious Coverage		70% ³		60% ³	
Additional Semi-Pervious Coverage		15%		15%	
h	Parking & Loading Location	Rear yard; attached garages access off rear facade only. ^{4,5}			
Entry for Parking within Building		Not Permitted on Primary Street			
i	Vehicular Access	Alley; if no alley exists, 1 driveway permitted off non-Primary street; if no side street, 1 driveway permitted off Primary Street; shared access is encouraged.			
(b) Height Refer to Figure 10-21-4E (2)					
j	Minimum Overall Height	none			
k	Maximum Overall Height	3.5 stories			
l	Minimum Ground Story Height	10'			
(c) Uses Refer to Figure 10-21-4E (2)					
m	Ground Story	Per Section 10-21-3 Uses			
n	Upper Stories	Per Section 10-21-3 Uses			
o	Parking within Building	Not Permitted on Primary Street			
p	Occupied Space	20' deep on all full height floors from the front facade			
(d) Street Facade Requirements Refer to Figure 10-21-4E (3)					
q	Minimum Ground Story Transparency Measured between 2' and 8' above grade	40%	30%		
r	Minimum Transparency Upper Stories	15%			
Blank Wall Limitations		Required per floor			
s	Front Facade Entrance Type	Porch (Storefront, Elevated Storefront permitted only on parcels in flood hazard areas per Section 18 of the City's Code of Ordinances; Stoop permitted with design review			
t	Principal Entrance Location	Front or corner side facade			
Number of Street Entrances		Minimum 1 per Building			
Ground Story Vertical Divisions		Not required			
Horizontal Facade Divisions		Within 3' of the top of any visible basement			
Facade Variety Required Refer to 10-21-4B(4)(h) for requirements.		Between adjacent buildings			
(e) Roof Type Requirements Refer to Figure 10-21-4E (3)					
u	Permitted Roof Types	Pitched			
Tower		Permitted			

10-21-4 Building Types

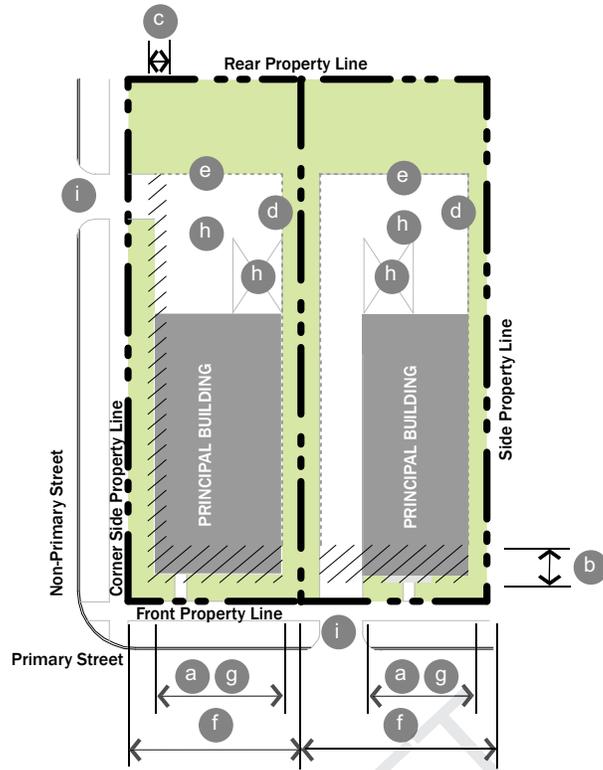


Figure 10-21-4E (1). Building Siting.

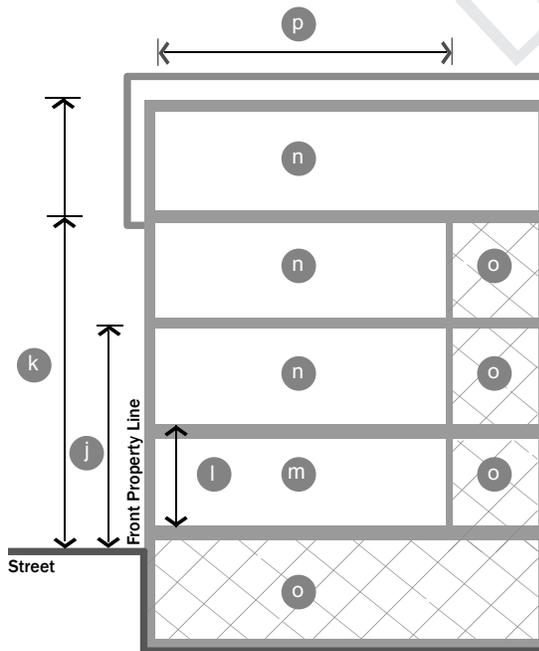


Figure 10-21-4E (2) Height & Use Requirements.

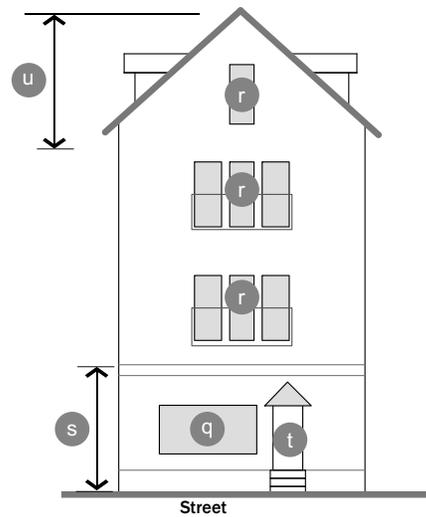


Figure 10-21-4E (3). Street Facade Requirements.

10-21-4 Building Types

F. Civic Building

1. Description & Intent. The Civic Building Type is a more flexible Building Type intended only for civic and institutional types of uses. These buildings are distinctive within the community fabric created by the other Building Types. Parking is limited to the rear in most cases. The maximum heights of this Building Type depend on the district within which it is located.
2. Regulations. Regulations for the Civic Building Type are defined in the adjacent table.

Permitted Districts				
S 1: Bridge Street	S 2: Hydraulic Street	S 3: Van Emmon Street	S 4: 'B' Street	S 5: 'B' Street Residential

(a) Building Siting Refer to Figure 10-21-4F (1)				
Multiple Principal Buildings		Refer to District Street Details		
Minimum Front Lot Line Coverage		Refer to District Street Details		
Occupation of Corner		Required		
a Front Build-to Zone	0' to 10'	0' to 15'		
b Corner Build-to Zone				
c Minimum Side Yard Setback	5'	10'		
d Minimum Rear Yard Setback	5'; 25' if located adjacent to residential ¹			
e Minimum Lot Width	16'	30'		
Maximum Building Width	none	none		
Maximum Impervious Coverage	70% ²	70% ²	50% ²	
Additional Semi-Pervious Coverage	20%	10%	20%	
f Parking & Loading Location	Rear yard ³			
Entry for Parking within Building	Rear & Side Facades			
g Vehicular Access	Alley	Alley; if no alley exists, 1 driveway is permitted per street frontage		
(b) Height Refer to Figure 10-21-4F (2)				
h Minimum Overall Height	1 story			
i Maximum Overall Height	6 stories (with min. 3' setback at 3 stories)	4 stories		3 stories
j Ground Story: Minimum Height	14'	12'	14'	12'
Maximum Height	24' ⁴	24' ⁴	24' ⁴	18' ⁴
k Upper Stories: Minimum Height	9'			9'
Maximum Height	14'			12'
(c) Uses Refer to Figure 10-21-4F(2)				
l All Stories	Limited to uses in the Civic category and Entertainment Uses by Special Use. Refer to 15.3 Uses.			
m Parking within Building	Permitted fully in any basement and in rear of all floors			
n Occupied Space	30' deep on all full height floors from the front facade			
(d) Street Facade Requirements Refer to Figure 10-21-4F (3)				
o Minimum Transparency Per each Story	12%			
Blank Wall Limitations	None			
p Front Facade Entrance Type	None required			
q Principal Entrance Location	Front or corner Facade			
Number of Street Entrances	1 per each 150' of front facade			
Ground Story Vertical Divisions	none required			
Horizontal Facade Divisions	none required			
Facade Variety Required Refer to 10-21-4B(4)(h) for requirements.	none required			
(e) Roof Type Requirements Refer to Figure 10-21-4F (3)				
r Permitted Roof Types	Parapet, Pitched, Flat, Other Roofs with approval per 10-21-4J			Pitched
s Tower	Permitted			

Notes:

¹ If located adjacent to flood hazard area, refer to requirements of the Yorkville Stormwater Ordinance No. 2012-56. Refer to FEMA National Flood Hazard Layer FIRMette maps for all flood hazard areas.

² Maximum Impervious Coverage shall be applied only to all non-flood hazard areas. Refer to FEMA National Flood Hazard Layer FIRMette maps for all flood hazard areas.

³ Lots wider than one hundred (100) feet are permitted one double-loaded aisle of parking (maximum width of seventy (70) feet), located perpendicular to the front lot line.

⁴ Eighteen (18) feet or more in height counts as two (2) stories towards maximum building height.

10-21-4 Building Types

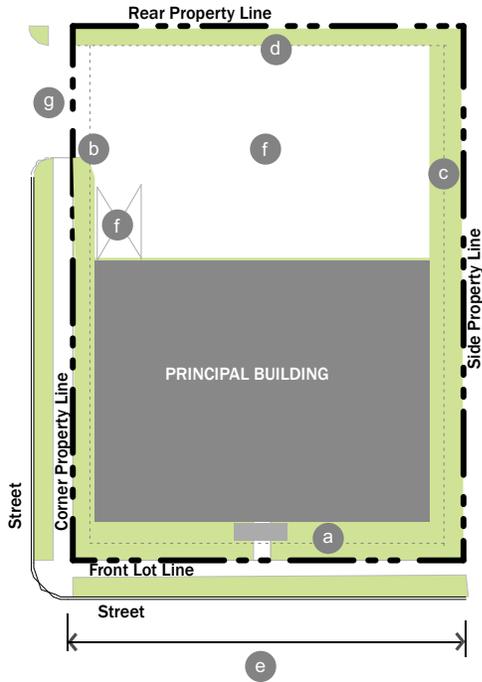


Figure 10-21-4F (1). Building Siting.

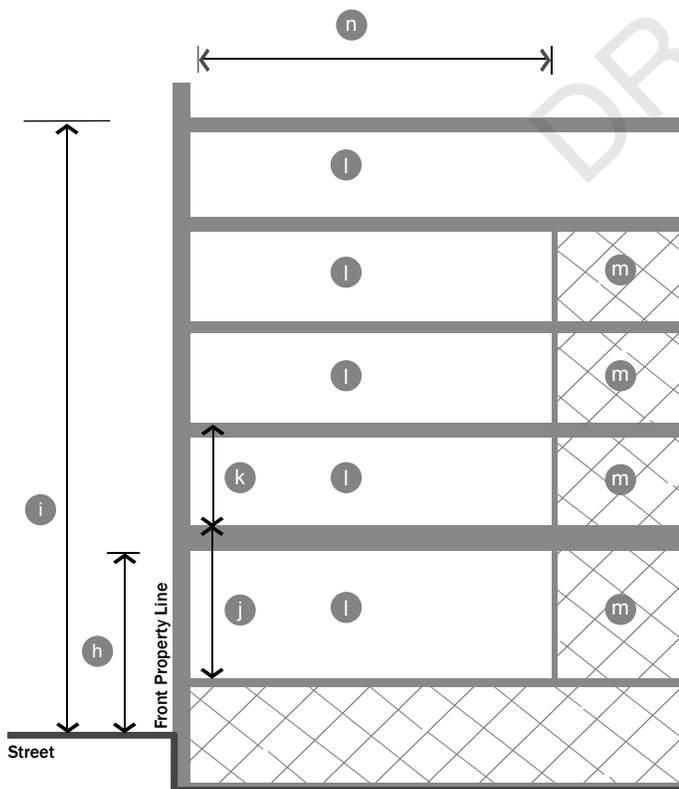


Figure 10-21-4F (2). Height & Use Requirements.

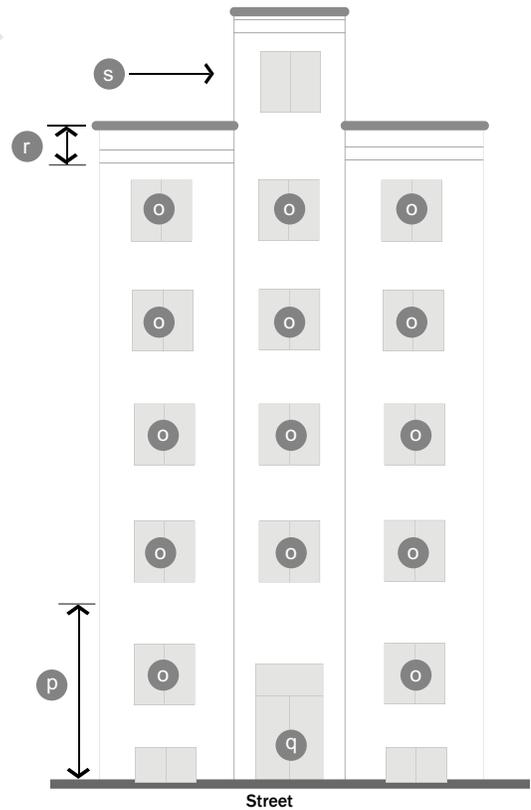


Figure 10-21-4F (3). Street Facade Requirements.

10-21-4 Building Types

G. Attached Building

1. **Description & Intent.** The Attached Building is a building comprised of multiple vertical units, each with its own entrance to the street. This Building Type may be organized as townhouses or rowhouses.

Parking is required to be located in the rear yard and may be incorporated either into a detached garage or in an attached garaged accessed from the rear of the building. However, when the garage is located within the building, a minimum level of occupied space is required on the front facade to ensure that the street facade is active.

2. **Regulations.** Regulations for the Attached Building Type are defined in the adjacent table.

Notes:

¹ For the purposes of the Attached Building, a building consists of a series of units. When permitted, multiple buildings may be located on a lot within the minimum space between them. However, each building shall meet all requirements of the Building Type.

² Each building shall meet the front lot line coverage requirement, except one of every five (5) units may front a courtyard with a minimum width of thirty (30) feet. The courtyard shall be defined on three (3) sides by units.

³ Maximum Impervious Coverage shall be applied only to all non-flood hazard areas. Refer to FEMA National Flood Hazard Layer FIRMette maps for all flood hazard areas.

⁴ Attached garages are considered part of the principal building and shall meet all setbacks. Detached garages shall meet all setbacks unless an alley is present. When an alley is present, detached garages shall have a minimum rear setback of five (5) feet.

Permitted Districts		
S 3: Van Emmon Street	S 4: 'B' Street	S 5: 'B' Street Residential

(a) Building Siting Refer to Figures 10-21-4G (1) and 10-21-4G (2)

Multiple Principal Buildings	Permitted ¹		
a Minimum Front Lot Line Coverage	75% ²	70% ²	65% ²
Occupation of Corner	Required		
b Front Build-to Zone	5' to 20'	10' to 20'	10' to 25'
c Corner Build-to Zone	5' to 10'	10' to 15'	
d Minimum Side Yard Setback	0' per unit; 10' between buildings'		0' per unit; 15' between buildings'
e Minimum Rear Yard Setback	15', if alley present 5'		20', if alley present 5'
f Minimum Unit Width	16' per unit	16' per unit	18' per unit
g Maximum Building Width	Maximum 8 units per building	Maximum 6 units per building; maximum 120' width	Maximum 6 units per building; maximum 120' width
Maximum Impervious Coverage	65% ³	60% ³	50% ³
Additional Semi-Pervious Coverage	20%	20%	20%
h Parking & Garage Location	Rear yard; attached garages access off rear facade only. ⁴		
i Vehicular Access	Alley; if no alley exists, one driveway is permitted per building		

(b) Height Refer to Figure 10-21-4G (3)

j Minimum Overall Height	1 story		
k Maximum Overall Height	4 stories	3.5 stories	
l Minimum Ground Floor Height:	12'		

(c) Uses Refer to Figure 10-21-4G (3)

m All Stories	Residential only		
n Parking within Building	Permitted fully in any basement and in rear of all floors		
o Occupied Space	30' deep on all full height floors from the front facade		

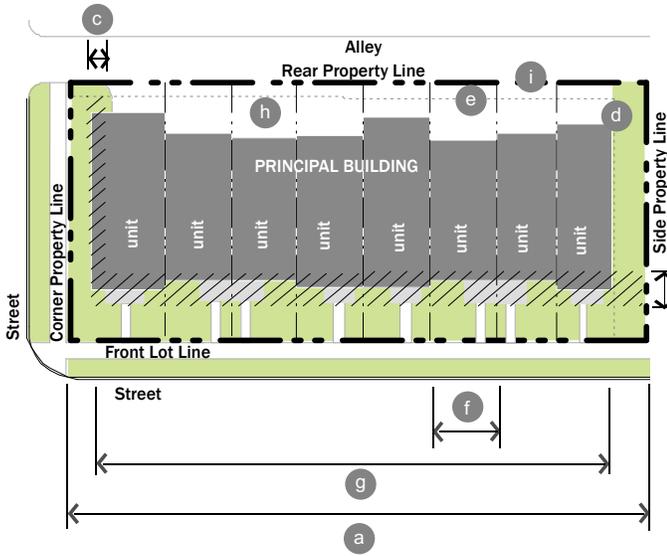
(d) Street Facade Requirements Refer to Figure 10-21-4G (4)

p Minimum Transparency per each Story	15%	12%	
q Front Facade Entrance Type	Stoop, Porch		
Principal Entrance Location	Front or Corner Side Facade		
r Number of Street Entrances	1 per unit		
Ground Story Vertical Divisions	none required		
Horizontal Facade Divisions	none required		
Facade Variety Required Refer to 10-21-4B(4)(h) for requirements.	Between adjacent buildings		

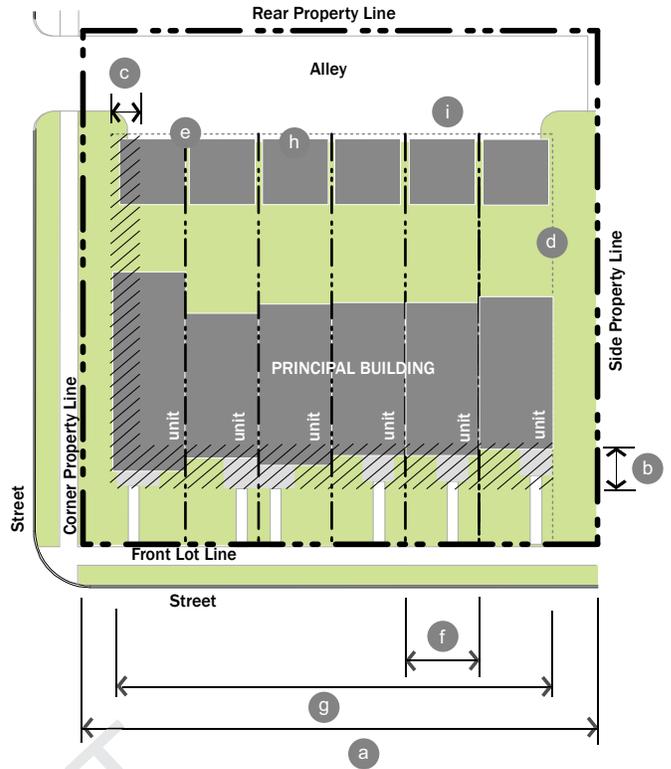
(e) Roof Type Requirements Refer to Figure 10-21-4G (4)

s Permitted Roof Types	Parapet, Pitched, Flat		
Tower	Permitted		

10-21-4 Building Types



Site Plan without Rear Yard
Figure 10-21-4G (1): Building Siting.



Site Plan with Rear Yard
Figure 10-21-4G (2): Building Siting.

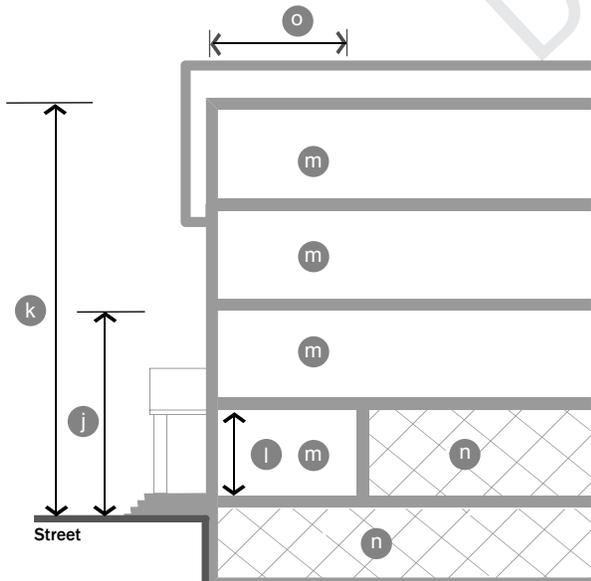


Figure 10-21-4G (3): Height & Use Requirements.

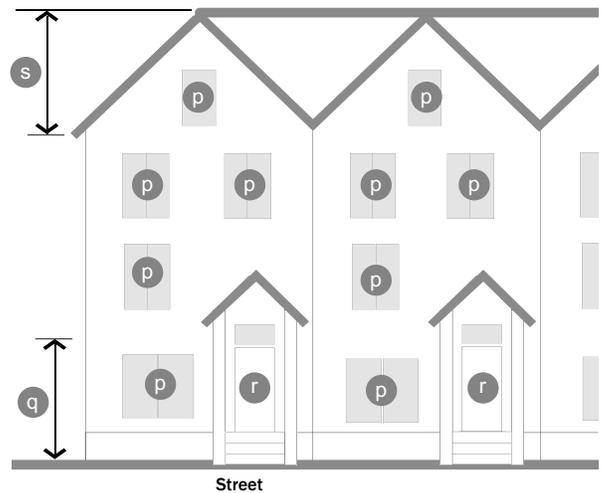


Figure 10-21-4G (4): Street Facade Requirements.

10-21-4 Building Types

H. Yard Building

- Description & Intent.** The Yard Building is a residential building, incorporating a landscaped yard surrounding all sides of the building. Parking and garages are limited to the rear only with preferred access from an alley.
- Regulations.** Regulations for the Yard Building Type are defined in the adjacent table.

Permitted Districts	
S 4: 'B' Street	S 5: 'B' Street Residential

(a) Building Siting Refer to Figure 10-21-4H (1)		
Multiple Principal Buildings	Permitted ¹	
a Minimum Front Lot Line Coverage	50% ²	40%
b Occupation of Corner	Required	
c Front Build-to Zone	10' to 20'	10' to 25'
Corner Build-to Zone	10' to 20'	10' to 25'
d Minimum Side Yard Setback	5'	7.5'
e Minimum Rear Yard Setback	35', if alley present 5'	
f Minimum Lot Width	30'	30'
Maximum Lot Width	50'	70'
g Maximum Impervious Coverage	60% ³	50% ³
Additional Semi-Pervious Coverage	20%	20%
h Parking & Garage Location	Rear yard; attached garages access off rear or side facade only. ⁴	
Vehicle Access	Alley; if no alley exists, one driveway is permitted per building	
(b) Height Refer to Figure 10-21-4H (2)		
i Minimum Overall Height	1 story	
j Maximum Overall Height	3.5 stories	
k All Stories: Minimum Height	9'	
Maximum Height	14'	
(c) Uses Refer to Figure 10-21-4H (2)		
l All Stories	Residential only	
m Parking within Building	Permitted	
n Occupied Space	30'	
(d) Street Facade Requirements Refer to Figure 10-21-4H (3)		
o Minimum Transparency per each Story	12%	
p Front Facade Entrance Type	Stoop, porch	
q Principal Entrance Location	Front or side facade	
Number of Street Entrances	Any	
Ground Story Vertical Divisions	None required	
Horizontal Facade Divisions	None required	
Facade Variety Required Refer to 10-21-4B(4)(h) for requirements.	Between adjacent buildings	
(e) Roof Type Requirements Refer to Figure 10-21-4H (3)		
r Permitted Roof Types	Parapet, Pitched, Flat	Pitched
Tower	Not permitted	

Notes

¹ Each building shall meet all requirements of the Building Type.

² When multiple buildings are located on a single lot, the buildings shall collectively meet the front lot line coverage requirement. Buildings located internal to the lot may be arranged with a courtyard or bungalow court that is a minimum of twenty (20) feet in width. The width of the courtyard shall be exempt from minimum front lot line coverage requirements. The courtyard or bungalow court shall be defined on three (3) sides by units. This layout shall not be allowed on corner lots, only lots internal to a block segment.

³ Maximum Impervious Coverage shall be applied only to all non-flood hazard areas. Refer to FEMA National Flood Hazard Layer FIRMette maps for all flood hazard areas.

⁴ Attached garages are considered part of the principal building and shall meet all setbacks. Detached garages shall meet all setbacks unless an alley is present. When an alley is present, detached garages shall have a minimum rear setback of five (5) feet.

10-21-4 Building Types

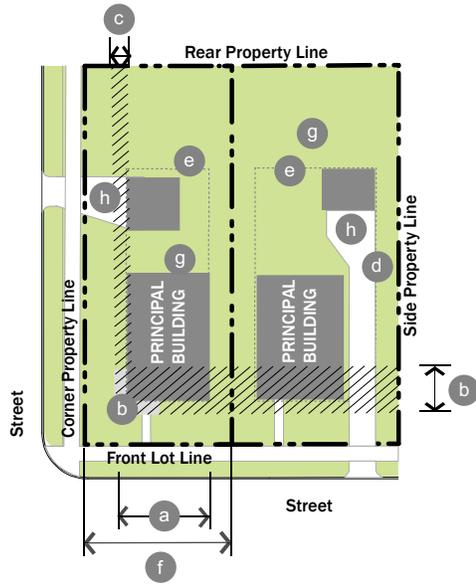


Figure 10-21-4H (1). Building Siting.

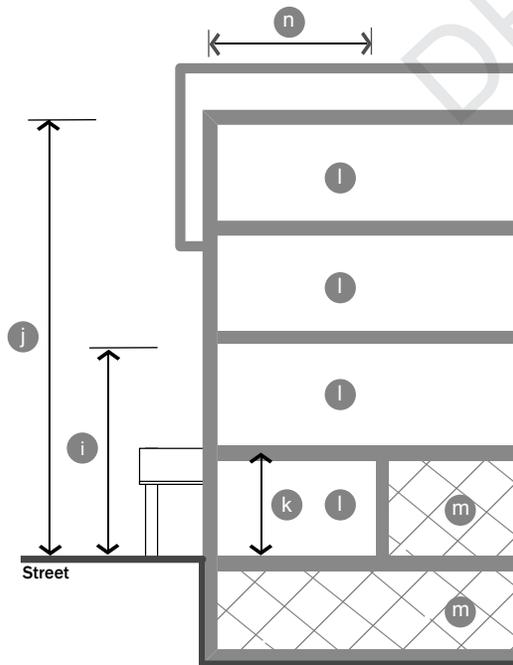


Figure 10-21-4H (2). Height and Use Requirements.

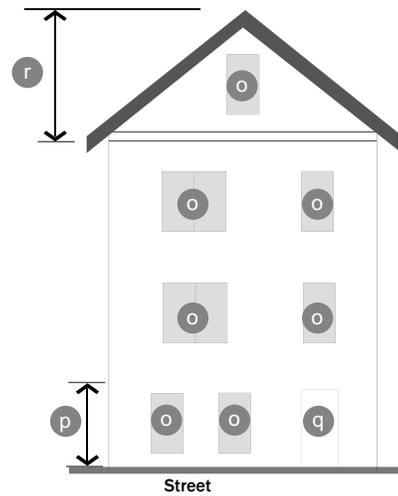


Figure 10-21-4H (3). Street Facade Requirements.

10-21-4 Building Types

I. Entrance Types

Entrance type standards apply to the ground story and visible basement of front facades of all Building Types as defined in this Section. Refer to the Building Type Table Requirements, Sections 10-21-4C through 10-21-4H.

1. **General.** The following provisions apply to all entrance types.
 - (a) **Intent.** To guide the design of the ground story of all buildings to relate appropriately to pedestrians on the street. Treatment of other portions of the building facades is detailed in each Building Type standard (refer to Building Types 10-21-4C through 10-21-4H).
 - (b) **Applicability.** The entire ground story street-facing facade(s) of all buildings shall meet the requirements of at least one (1) of the permitted entrance types, unless otherwise stated.
 - (c) **Measuring Transparency.** Refer to Section 10-21-4B Explanation of Building Type Table Standards, for information on measuring building transparency.
 - (d) **Visible Basements.** Visible basements, permitted by entrance type, are optional. The visible basement shall be a maximum of one-half the height of the tallest story.
2. **Storefront Entrance Type.** Refer to Figure 10-21-4I (1). The Storefront entrance type is a highly transparent ground story treatment designed to serve primarily as the display area and primary entrance for retail or service uses.
 - (a) **Transparency.** Minimum transparency is required per Building Type.
 - (b) **Elevation.** Storefront elevation shall be between zero (0) and one (1) foot above street sidewalk.

- (c) **Visible Basement.** A visible basement is not permitted.
 - (d) **Horizontal Facade Division.** Horizontally define the ground story facade from the upper stories.
 - (e) **Entrance.** All entries shall be recessed from the front facade closest to the street.
 - i. Recess shall be a minimum of three (3) feet and a maximum of eight (8) feet deep, measured from the portion of the front facade closest to the street.
 - ii. When the recess falls behind the front build-to zone, the recess shall be no wider than eight (8) feet.
3. **Elevated Storefront Entrance Type.** Refer to Figure 10-21-4I (2). The Elevated Storefront entrance type is a highly transparent ground story treatment similar to the Storefront, but permitted to be elevated above the sidewalk for buildings located on parcels with flood hazard areas.
 - (a) **Transparency.** Minimum transparency is required per Building Type.
 - (b) **Elevation.** Storefront elevation may be a half story above the street sidewalk elevation.
 - (c) **Visible Basement.** A visible basement is permitted and does not require Occupied Space.
 - (d) **Horizontal Facade Division.** Horizontally define the ground story facade from the upper stories and any Visible Basement from the ground story.
 - (e) **Entrance.** All entries shall be located off a secondary walk along the building face within the build-to zone.
 - i. The secondary sidewalk shall be elevated above and essentially parallel to the street sidewalk to provide

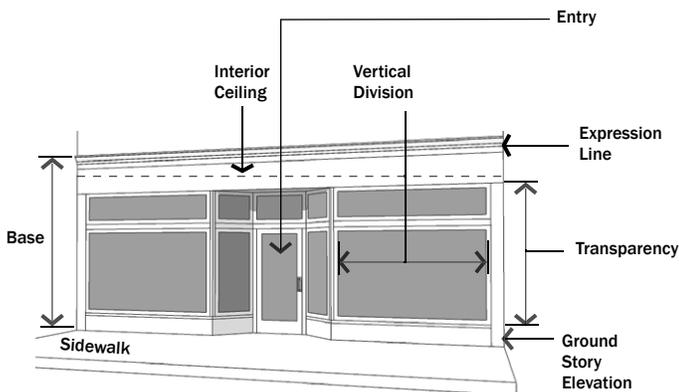


Figure 10-21-4I (1). Storefront Entrance Type.

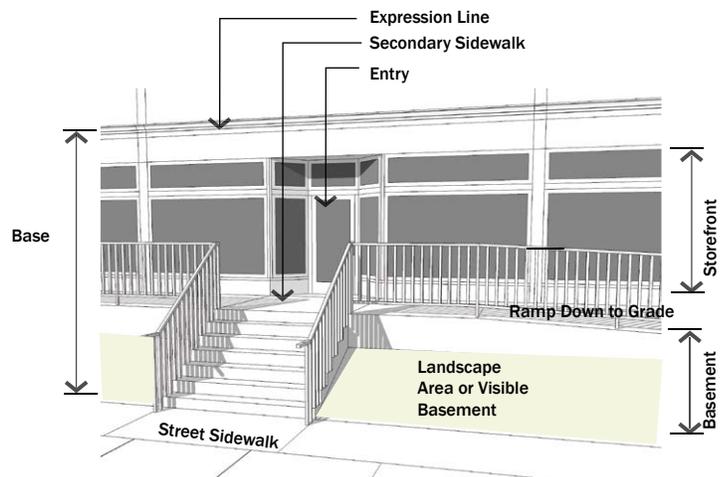


Figure 10-21-4I (2). Elevated Storefront Entrance Type.

10-21-4 Building Types

- continuous walking along the facade of the building.
 - ii. The secondary sidewalk shall be continuous along the facade of the building and shall connect to the street sidewalk by steps and ramps every fifty (50) feet.
 - iii. The secondary sidewalk shall connect to any other adjacent developments secondary sidewalks, when feasible. Drive crossings shall be of the same material as the secondary walk.
 - iv. The transition between the secondary sidewalk and street sidewalk shall include landscape, patios, and connecting walks.
 - v. The visible basement shall be located a minimum of five (5) feet from the street sidewalk to allow softening of the transition.
 - vi. The street and the secondary sidewalks shall be a minimum of eight (8) feet in width.
4. **Stoop Entrance Type.** Refer to Figure 10-21-4I (3). A stoop is an unroofed, open platform.
- (a) Transparency. Minimum transparency is required per Building Type.
 - (b) Stoop Size. Stoops shall be a minimum of five (5) feet deep by six (6) feet wide.
 - (c) Elevation. Stoop elevation shall be located a maximum of two (2) feet six (6) inches (2' 6") above the sidewalk without visible basement and a maximum of four (4) feet six (6) inches (4' 6") above the sidewalk with a visible basement.
 - (d) Visible Basement. A visible basement is permitted and shall be separated from the ground story by an expression line.
- (e) Entrance. All entries shall be located off a stoop. The stoop may be continuous along the facade of the building.
- (f) Landscape Area. A minimum five (5) foot wide landscape area is required within the build-to zone along the length of this entrance type with the exception of walks accessing the building.
5. **Porch Entrance Type.** Refer to Figure 10-21-4I (4). A porch is a raised, roofed platform that may or may not be enclosed on all sides. If enclosed, the space shall not be climate controlled.
- (a) Transparency.
 - i. Minimum transparency per Building Type is required.
 - ii. If enclosed, a minimum of forty percent (40%) of the enclosed porch shall be comprised of highly transparent, low reflectance windows.
 - (b) Porch Size. The porch shall be a minimum of five (5) feet deep and eight (8) feet wide.
 - (c) Elevation. Porch elevation shall be located a maximum of two (2) feet six (6) inches (2' 6") above the sidewalk without a visible basement and a maximum of four (4) feet six (6) inches (4' 6") above the sidewalk with a visible basement.
 - (d) Visible Basement. A visible basement is permitted.
 - (e) Height. Porch may be two (2) stories to provide a balcony on the second floor.
 - (f) Entrance. All entries shall be located off a porch.

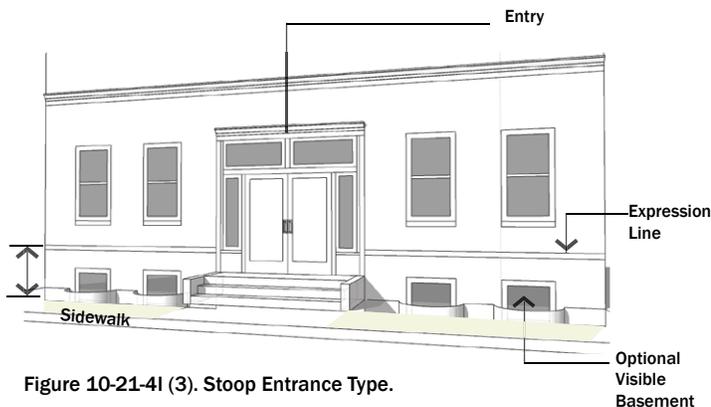


Figure 10-21-4I (3). Stoop Entrance Type.

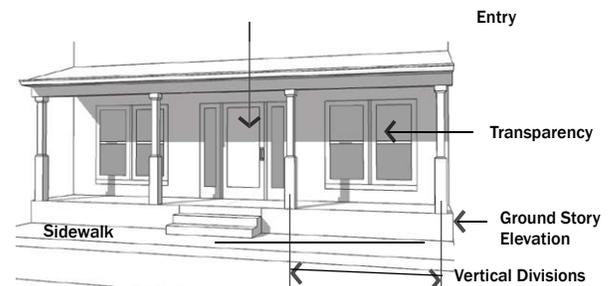


Figure 10-21-4I (4). Porch Entrance Type.

10-21-4 Building Types

J. Roof Types

Roof type standards apply to the roof and cap of all Building Types as defined in this Section. Refer to the Building Type Table Requirements, Sections 10-21-4C through 10-21-4H.

1. **General Provisions.** The following provisions apply to all roof types.

- (a) Intent. To guide the design of the cap of all buildings.
- (b) Applicability. All buildings shall meet the requirements of one of the roof types permitted for the Building Type.
- (c) Measuring Height. Refer to Section 10-21-4B for information on measuring building height.
- (d) Other roof types. Other building caps not listed as a specific type may be requested with the following requirements:
 - i. The roof type shall not create additional occupiable space beyond that permitted by the Building Type.
 - ii. The shape of the Roof Type shall be significantly different from those defined in this section 10-21-4J, i.e. a dome, spire, vault.
 - iii. The building shall warrant a separate status within the community from the fabric of surrounding buildings, with a correspondence between the form of the roof type and the meaning of the building use.

2. **Parapet Roof Type.** Refer to Figure 10-21-4J (1). A parapet is a low wall projecting above a building’s roof along the perimeter of the building. It can be utilized with a flat or low pitched roof and also serves to limit the view of roof-top mechanical systems from the street.

- (a) Parapet Height. Height is measured from the top of the upper story to the top of the parapet.
 - i. Minimum height is two (2) feet with a maximum height of six (6) feet.

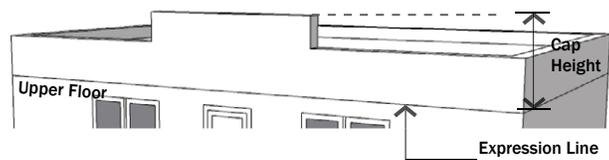


Figure 10-21-4J (1). Parapet Roof Type

- ii. The parapet shall be high enough to screen the roof and any roof appurtenances from view of the street(s).
 - (b) Horizontal Expression Lines. An expression line shall define the parapet from the upper stories of the building and shall also define the top of the cap.
 - (c) Occupied Space. Occupied space shall not be incorporated behind this roof type.
3. **Pitched Roof Type.** Refer to Figure 10-21-4J (2). This roof type has a sloped or pitched roof. Slope is measured with the vertical rise divided by the horizontal span or run.
- (a) Pitch Measure. The roof may not be sloped less than a 4:12 (rise:run) or more than 16:12.
 - i. Slopes less than 4:12 are permitted to occur on second story or higher roofs. Refer to Figure 10-21-4J (2).
 - (b) Configurations.
 - i. Hipped, gabled, and combination of hips and gables with or without dormers are permitted.
 - ii. Butterfly roofs (inverted gable roof) are permitted with a maximum height of eight (8) feet, inclusive of overhang.
 - iii. Gambrel and mansard roofs are not permitted.
 - (c) Parallel Ridge Line. A gabled end or perpendicular ridge line shall occur at least every one hundred (100) feet of roof when the ridge line runs parallel to the front lot line. Refer to Figure 10-21-4J (3).
 - (d) Roof Height. Roofs without occupied space and/or dormers shall have a maximum height on street-facing facades equal to the maximum floor height permitted for the Building Type.
 - (e) Occupied Space. Occupied space may be incorporated behind this roof type.

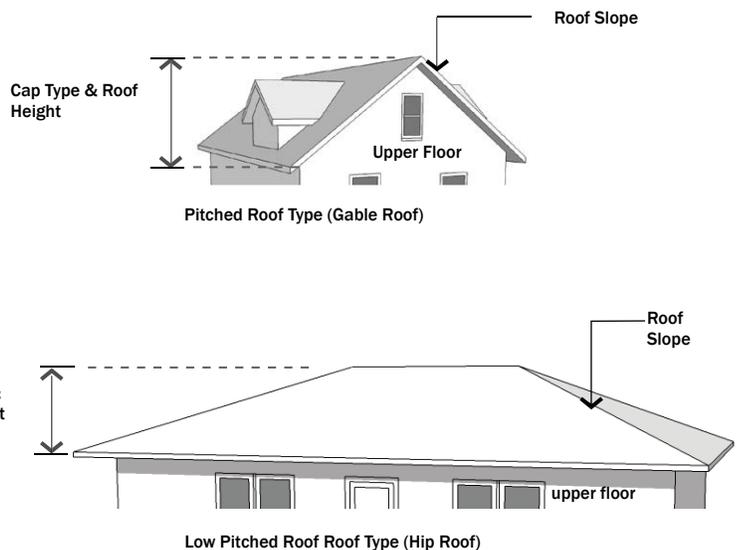


Figure 10-21-4J (2). Pitched Roof Type

10-21-4 Building Types

4. **Flat Roof Type.** Refer to Figure 10-21-4J (5), Flat Roof Type. This roof type has a flat roof with overhanging eaves.
 - (a) Configuration. Roofs with no visible slope are acceptable. Eaves are recommended on all street facing facades.
 - (b) Eave Depth. Eave depth is measured from the building facade to the outside edge of the eave. Eaves shall have a depth of at least fourteen (14) inches.
 - (c) Eave Thickness. Eave thickness is measured at the outside edge of the eave, from the bottom of the eave to the top of the eave. Eaves shall be a minimum of eight (8) inches thick.
 - (d) Interrupting Vertical Walls. Vertical walls may interrupt the eave and extend above the top of the eave with no discernible cap.
 - i. No more than one-half of the front facade can consist of an interrupting vertical wall.
 - ii. Vertical walls shall extend no more than four (4) feet above the top of the eave.
 - (e) Occupied Space. Occupied space shall not be incorporated behind this roof type.
 - (f) No mechanical equipment on roof shall be visible from the adjacent sidewalk.

5. **Towers.** Refer to Figure 10-21-4J (4). A tower is a rectilinear or cylindrical, vertical element, that shall be used with other roof types.
 - (a) Quantity. All Building Types, with the exception of the Civic Building, are limited to one (1) tower per building.
 - (b) Tower Height. Maximum height, measured from the top of the parapet or eave to the top of the tower, is the equivalent of the height of one (1) upper floor of the building to which the tower is applied.

- (c) Tower Width. Maximum width along all facades is one-third the width of the front facade or thirty (30) feet, whichever is less.
- (d) Occupied Space. Towers may be occupied by the same uses allowed in upper stories of the Building Type to which it is applied.
- (e) Application. May be combined with all other roof types.
- (f) Tower Cap. The tower may be capped by the parapet, pitched, low pitched, or flat roof roof types, or the spire may cap the tower.

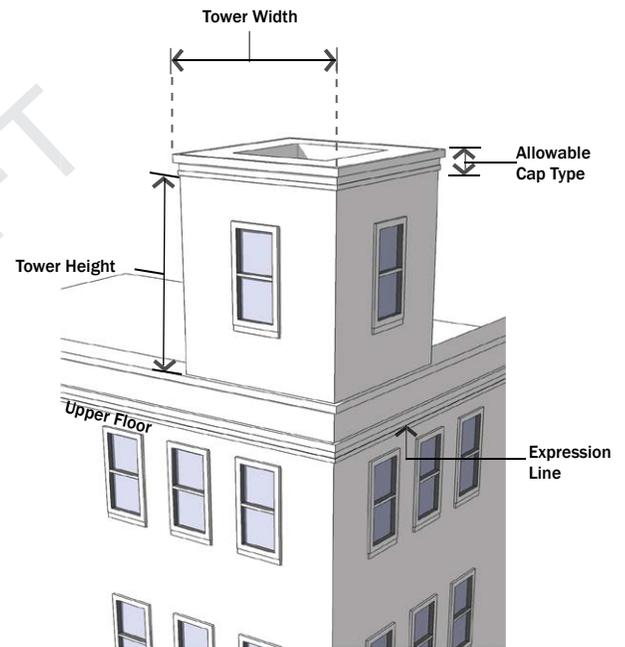


Figure 10-21-4J (4). Tower

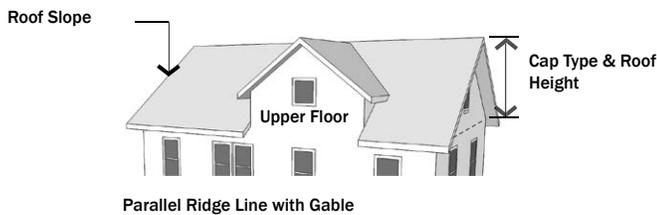


Figure 10-21-4J (3). Parallel Ridge Line

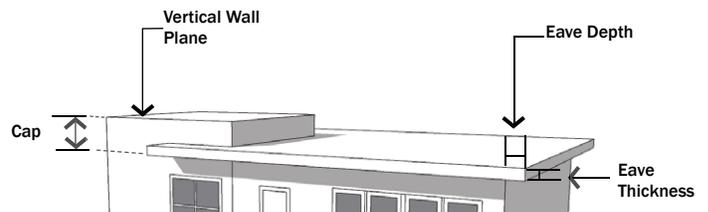


Figure 10-21-4J (5). Flat Roof Type

10-21-4 Building Types

K. Additional Design Requirements

The following outlines the district design requirements that affect a building's appearance and district cohesiveness. They improve the physical quality of buildings, enhance the pedestrian experience, and protect the character of the neighborhood.

1. Materials and Color.

- (a) Primary Facade Materials. Eighty percent (80%) of each street facade shall be constructed of primary materials. Street facade materials shall continue around the corner a minimum depth of twenty (20) feet onto the side facade.
 - i. Permitted primary building materials include high quality, durable, natural materials, such as stone, brick; wood lap siding; fiber cement board lapped, shingled, or panel siding; glass. Other high quality synthetic materials may be approved during the site plan process with an approved sample and examples of successful, high quality local installations. Refer to Figure 10-21-4K (1).
- (b) Secondary Facade Materials. Secondary materials are limited to details and accents and include gypsum reinforced fiber concrete for trim and cornice elements; metal for beams, lintels, trim, and ornamentation, and exterior architectural metal panels and cladding.
 - i. Exterior Insulation and Finishing Systems (EIFS) is permitted on upper floor facades only.

- (c) Roof Materials. Acceptable roof materials include three hundred (300) pound or better, dimensional asphalt composite shingles, wood shingles and shakes, metal tiles or standing seam, slate, and ceramic tile. "Engineered" wood or slate may be approved during the site plan process with an approved sample and examples of successful, high quality local installations. Refer to Figure 10-21-4K (2).
- (d) Color. Main building colors shall utilize any historic palettes from any major paint manufacturer. Other colors may be utilized for details and accents, not to exceed a total area larger than ten percent (10%) of the facade surface area.
- (e) Appropriate Grade of Materials. Commercial quality doors, windows, and hardware shall be used on all ground floor Building Types with the exception of the Attached Building and the Yard Building. Refer to Figure 10-21-4K (3).

2. Windows, Awnings, and Shutters.

- (a) Windows. All upper story windows on all historic, residential, and mixed use buildings shall be recessed, and either casement or double hung. Percent of transparency is required per Building Type. Horizontal or vertical strip windows, tinted or reflective glass, and glass block (Figure 10-21-4K (4)) are prohibited on street facades.
- (b) Security Grills. Grills shall be fully retractable and completely within the interior of the building and inconspicuous to the



Primary Materials: Brick



Primary Materials: Stone



Roof Materials: Asphalt Composite Shingles



Roof Materials: Metal



Primary Materials: Painted Wood



Roof Materials: Ceramic Tile

Figure 10-21-4K (1). Primary Materials.

Figure 10-21-4K (2). Roof Materials.

10-21-4 Building Types

extent possible. Exterior bars are prohibited on any window. Refer to Figure 10-21-4K (5).

- (c) Awnings. All awnings shall be canvas or metal. Plastic awnings and canopy awnings that extend from the front facade into the right-of-way are prohibited. Awning types and colors for each building face shall be coordinated. Refer to Figure 10-21-4K (6).
- (d) Shutters. If installed, shutters, whether functional or not, shall be sized for the windows. If closed, the shutters shall not be too small for complete coverage of the window. Shutters shall be wood. "Engineered" wood may be approved during the site plan process with an approved sample and examples of successful, high quality local installations.

3. **Rear Parking Facade Design.** The following applies in all locations where a public building entrance occurs on the rear facade adjacent to a parking lot. Refer to Figure 15.4K (7).

- (a) Entrance Type. An Entrance Type shall be utilized for a minimum of twenty (20) feet of rear facade. Refer to 10-21-4(2)-(5).



Prohibited: Glass block windows on front facade.

Figure 10-21-4K (4).Windows.



Prohibited: Residential Grade Doors on Commercial Buildings.



Permitted: Fully retractable, interior security grills.

Figure 10-21-4K (5).Security Grills.



Prohibited: Exterior grills and bars.



Permitted Awnings: Metal (left) and Canvas (right)



Permitted: Commercial Grade Doors & Windows on Commercial Buildings.

Figure 10-21-4K (3).Commercial Grade Doors & Windows.



Prohibited Awnings: Canopy awnings that extend from the front facade into the right-of-way

Figure 10-21-4K (6). Awnings.

15.4 Building Types



Front Facade Example.

Rear Facade Example.

Figure 10-21-4K (7). Rear Parking Facade Design.

- (b) Transparency Requirement. Public building entrance facade area, minimum twenty (20) feet wide, shall utilize one of the following:
 - i. When the Storefront Entrance Type is utilized, a minimum forty five percent (45%) transparency is required for the ground floor facade entrance, and the door shall be a minimum of forty five percent (45%) transparent.
 - ii. When any other Entrance Type is utilized, the minimum transparency required for upper floors of the street facade shall apply to the rear ground floor entrance area, and the door shall be a minimum of forty five percent (45%) transparent.

(c) Awnings and signage are encouraged.

4. **Balconies.** The following applies in all locations where balconies are incorporated into the facade design facing any street or parking lot. Refer to Figure 10-21-4K (8).

(a) Size. Balconies shall be a minimum of six (6) feet deep and five (5) feet wide.

(b) Connection to Building. Balconies shall be integral to the facade at the street line. Balconies on stepbacked stories shall be independently secured and unconnected to other balconies.

(c) Facade Coverage. A maximum of forty percent (40%) of the front and corner side facades, as calculated separately, may be covered with balconies, including street-facing railing and balcony structure.

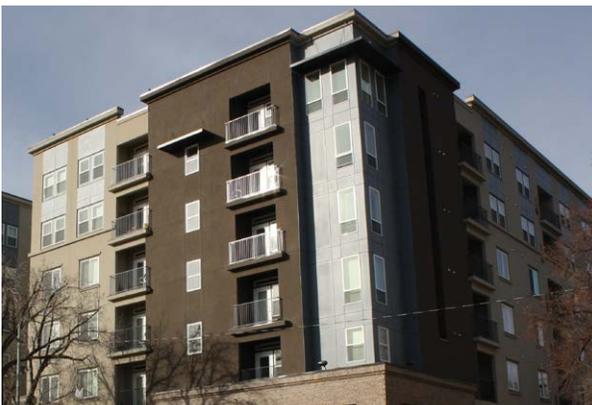


Figure 10-21-4K (8). Balconies Integral to Facade.

This page intentionally left blank

DRAFT

10-21-5 Site Development Standards

A. Signage

1. **General Requirements.** Refer to Section 10-20 of the Yorkville City Code for all signage regulations applicable to the Downtown Overlay Districts.
2. **Revisions to the Signage Regulations.** The following revises Section 10-20 of the Yorkville City Code specific to the S Districts.
 - (a) Freestanding Low Monument Signs. (Refer to Section 10-20-4) Low Monument Signs are permitted only in the S 4 District.

B. Parking Requirements

1. **Applicability.** This section shall apply to all new development and changes in use or intensity of use for existing development in any S Districts.
2. **General Requirements.** Off-street parking spaces shall be provided in conformance with Section 10-16 Off-Street Parking and Loading Regulations, unless revised in this Section 10-21-5B.
 - (a) **Required Vehicle Parking.** The Required Vehicle Parking Table 10-21-5B (1) indicates the maximum vehicle parking ratio for a given use.
3. **Parking Credits.** Vehicular parking standards within Section 10-16 may be reduced by achieving one or all of the following credits.
 - (a) **On-Street Parking Credit.** For all non-residential uses, on-street parking spaces that meet the following shall be credited against the parking requirement.
 - i. Spaces shall be designated on-street parking available twenty four (24) hours of every day.
 - ii. On-street space located a minimum of fifty percent (50%) adjacent to the property line of the lot.
 - (b) **Public Parking Credit.** For all non-residential uses, public parking spaces located within six hundred and sixty (660) feet of any property line may be credited against the parking requirement at a rate of one credit for every three public parking spaces.
 - (c) **Car-Share Parking Credit.** The vehicular parking requirements can be reduced with the inclusion of car-share parking spaces as follows.
 - i. Per each car-share parking space provided, required parking spaces shall be reduced by four (4) spaces.
 - ii. Required parking spaces may be reduced up to forty percent (40%).
 - iii. Approval. Applicant must provide documentation of an agreement with a car-share company. If this agreement should terminate at any point, applicant shall be required to provide parking as otherwise required herein.
 - (d) **Shared Parking.** Required Parking may be reduced to the lower amount if at least eighty percent (80%) of non-residential parking is available as publicly shared parking. Otherwise, the higher standard parking requirement shall apply.
 - (e) **Other Parking Reductions.** Additional reductions may be approved by the Planning and Zoning Commission with the submittal of a parking study illustrating the reduction.

4. Bicycle Parking.

- (a) **Required Bicycle Parking.** The Required Bicycle Parking Table 10-21-5B (2) indicates the minimum bicycle parking ratio for a given use.
 - i. Bicycle parking is not required for uses not listed.
 - ii. Bicycle parking is not required for uses less than 2,500 square feet in size.
 - iii. No Use, other than Civic is required to accommodate more than twenty (20) bicycles.
- (b) **Bicycle Parking Dimensions.**
 - i. Required bicycle parking spaces shall have minimum dimensions of two (2) feet in width and six (6) feet in length.
 - ii. An aisle a minimum of five (5) feet wide shall be provided behind bicycle parking facilities to allow for maneuvering.
 - iii. A minimum of two (2) feet shall be provided beside each parked bicycle to allow access. This access may be shared by adjacent bicycles.
 - iv. Racks shall be installed a minimum of two (2) feet from any wall or other obstruction.

Land Use	Vehicle Spaces
Residential (Studio and 1 bedroom)	1 per unit
Residential (2 or more bedrooms)	1.5 per unit
Civic/Institutional	max. 2 per 1,000 square feet
Retail/Services (less than 8,000 square feet, excluding Restaurants)	no min. or max. parking requirements
Retail/Services (8,000 square feet or more, excluding Restaurants)	max. 2 per 1,000 square feet
Restaurants	max. 4 per 1,000 square feet
Office	max. 2 per 1,000 square feet

Table 10-21-5B (1). Required Vehicle Parking.

Land Use	Bicycle Spaces
Multifamily	1 per 2 Vehicular Spaces for buildings with 8+ units
Civic/Institutional	1 per 10 Vehicular Spaces, min. of 4
Retail	1 per 10 Vehicular Spaces
Services	1 per 10 Vehicular Spaces
Office	1 per 10 Vehicular Spaces

Table 10-21-5B (2). Required Bicycle Parking.

10-21-5 Site Development Standards

- (c) Location. Bicycle parking should be located within fifty (50) feet of the entrance of the Use.
 - i. Indoor or outdoor spaces are permitted, provided they are located on the lot with which they are associated.
 - ii. Bicycle parking facilities shall be separated from vehicular parking areas to protect parked bicycles from damage. The separation may be accomplished through grade separation, distance or physical barrier, such as curbs, wheel stops, poles or other similar features.
- (d) Racks and Structures. Racks and structures shall be provided for each unprotected parking space, and shall be designed to accommodate both chain and U-shaped locking devices supporting the bicycle frame at two (2) points.

C. Landscape

1. **General Requirements.** Refer to Section 10-17 Fencing and Screening for all landscaping and screening requirements.
2. **Build-to Zones and Setbacks.** All build-to zone and setback areas not covered by building shall contain either landscape, patio space, or sidewalk space.
 - (a) Driveways are permitted to cross the front and corner build-to zone and rear setbacks perpendicularly at a maximum of twenty five (25) feet in width.
 - (b) Driveways may encroach upon the side setbacks longitudinally on parcels fifty (50) feet or less in width.
 - (c) Parking lots shall not encroach upon any setbacks. Side and rear yard parking lots shall not be located closer to the front or corner lot line than the building.

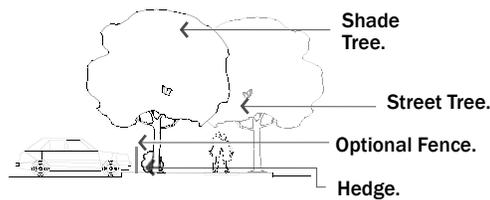
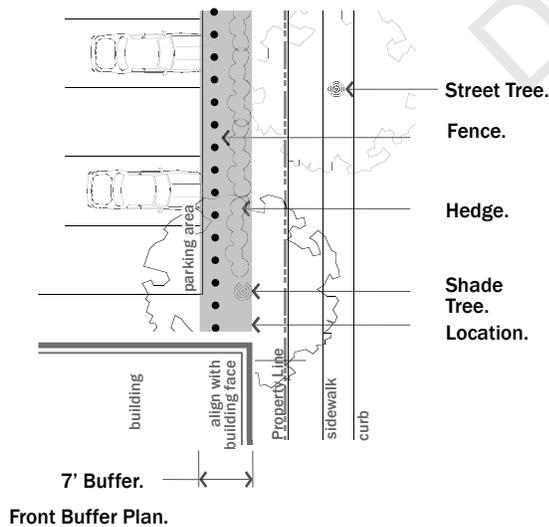


Figure 10-21-5C (1). Frontage Buffer Plan and Section.

Frontage Buffer Requirements	
Buffer Depth & Location ¹	
Depth	7'
Location on Site	Between street facing property line and vehicular areas ²
Buffer Landscape Requirements	
Uses & Materials	Uses and materials other than those indicated are prohibited in the buffer
Shade Trees	Medium or large shade tree with full, spreading canopies required at least every 40'; Locate on the street side of the fence; Spacing should alternate with street trees
Hedge	Required continuous hedge on street side of fence, between shade trees & in front of parking areas
Hedge Composition	Individual shrubs with a minimum width of 24", spaced no more than 36" on center
Existing Vegetation	May be credited toward buffer area
Fence	
Location	2' from back of curb of vehicular area
Materials	Non-galvanized steel or painted PVC; Masonry Columns (maximum width 2'6") and Low Wall (maximum 18" height) permitted
Minimum Height	3' for Steel or Painted PVC
Maximum Height	4' for Steel or Painted PVC, 18" for Low Wall
Colors	Black, gray, or dark green for Steel or Painted PVC
Opacity	Minimum 30%; Maximum 60% for Steel or Painted PVC
Gate/Opening	One gate permitted per street frontage; Opening width maximum 6'

Notes:

- ¹ This screening requirement does not prohibit the installation of or provision for openings necessary for allowable access drives and walkways connecting to the public sidewalk.
- ² In Front and Corner Yards, when the parking area is located adjacent to any building on the lot, the buffer must be located so that it aligns with or is behind the face of the adjacent building back to the vehicular area. The area between the buffer and the property line must be landscaped.

Table 10-21-5C (1). Frontage Buffer Requirements.

3. **Frontage Buffer Requirements.** Refer to Figure 10-21-5C (1). The following additional requirements are specific to the S Districts and is in addition to information within Section 10-17 Fencing and Screening guidelines.
 - (a) Intent. To lessen the visual impact of parking areas visible from the street.
 - (b) General Applicability. Applies to properties in all S Districts where a parking area is located adjacent to a right-of-way.
 - (c) Exceptions. Parking areas along alleys, except when a residential district is located across the alley. Single and two family residences are also excepted.

10-21-5 Site Development Standards

D. Street Guidelines

1. **General Street Guidelines.** The following guidelines should apply to all new streets within S Districts with the intent of creating pedestrian oriented, multimodal streets.
 - (a) **Typical Street Elements.** All street rights-of-way should include the following vehicular and pedestrian realm considerations. Refer to Figure 10-21-5D (1).
 - (1) **Vehicular Realm.** The vehicular realm is comprised of the travel lanes, bicycle lanes, and parking lanes.
 - (2) **Pedestrian Realm.** The pedestrian realm is comprised of pedestrian facilities, such as sidewalk. A buffer area that serves to buffer pedestrians or bicyclists from the movements of higher speed vehicles in the vehicular realm shall consist of one (1) of the following:
 - (i) **Landscape Zone.** A landscape area between the back of curb to the sidewalk in which street trees, stormwater swales, lighting, and signage may be located. Typically used adjacent to residential ground floor uses.
 - (ii) **Furnishings Zone.** A hardscape area that extends from the sidewalk to the back of curb, in which street trees, street furniture, lighting, and signage may be located. Typically used adjacent to commercial or office ground floor uses.
 - (b) **Bicycle Facilities.** Bicycle facilities, such as dedicated lanes and dedicated shared lanes should be included on any streets based on the City's bicycle plan. New streets within S districts shall utilize shared lanes. A shared lane refers to a street that does not have bicycle lanes or a designated shared lane, but the speed and configuration of the street is such that bicycles could comfortably share lanes with traffic.
 - (c) **Vehicular On-Street Parking.** On-street parking, whether parallel or diagonal, shall be included according to the District Street Details for that street.

2. **Street Trees.** Street trees are required along all existing and new street frontages.
 - (a) All planting material requirements within Landscaping and Screening Guidelines shall be utilized.
 - (b) Street trees shall be located in either a Landscape Zone (within a planting bed or lawn) or a Furnishings Zone (in trees wells with grate as required).
 - (c) **Permeable Surface.** For each tree preserved or planted, a minimum amount of permeable surface area is recommended.
 - i. Preserved trees should have a permeable surface area equal to the critical root zone. The critical root zone is equal to half of the radius of the tree's mature canopy, measured from the trunk out to the dripline.
 - ii. Planted trees have a suggested minimum permeable area and soil volume based upon tree size; refer to Table 10-21-5D (1) for details.
 - iii. Permeable area for one (1) tree cannot count toward that of another tree.
 - (d) **Structural Soil.** When the critical root zone of an existing tree or the suggested permeable surface area requirement of a newly planted tree extends below any pavement, structural soil is required underneath the pavement.
3. **Pedestrian Lighting.** Pedestrian light fixtures shall be installed per the street requirements of the City's Department of Public Works and any streetscape master plan adopted by the City.

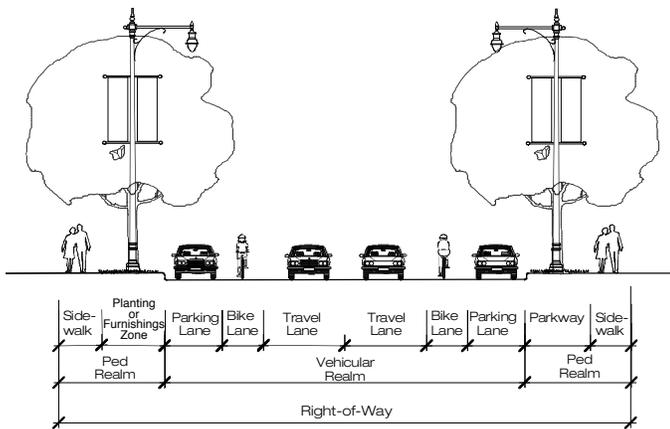


Figure 10-21-5D (1). Typical Right-of-Way Elements.

Tree Size Type	Soil Volume (cubic ft)	Soil Surface Area (sq ft) with 2.5' Soil Depth	Permeable Surface Area Requirement (sq ft)
Medium	2,852	1141 (approx. 34' x 34')	225 (15' x 15')
Large	6,532	2681 (approx. 50' x 50')	400 (20' x 20')

Table 10-21-5D (1). Minimum Recommended Soil Volumes and Permeable Area per Planted Tree.

10-21-5 Site Development Standards

4. **New Streets and Subdivision.** For all developments with total parcel acreage larger than five (5) acres, subdivision and construction of a new street will yield the most buildings. (Building Types require buildings to front streets). Refer to Figure 10-21-5D (2) for an example of a typical new block and street configuration. The following recommendations apply:

- (a) **Interconnected Street Pattern.** Streets shall connect and continue existing streets from adjoining areas and cul-de-sac and dead end streets should be avoided.
- (b) **Blocks.**
 - i. The shape of a block shall be generally rectangular, but may vary due to natural features or site constraints.
 - ii. Blocks shall typically be two (2) lots deep with the exception of blocks containing open space. Blocks may also include an alley. Blocks may include existing lots within an existing zoning district.
 - iii. Blocks shall typically be fronted with lots on at least two (2) faces, preferably on the longest street faces.
 - iv. Consider lot and block orientation for maximum energy efficiency. For example, block orientation along an east-west longitudinal axis will encourage development of buildings oriented along an east-west axis, with smaller east and west facing facades, able to take advantage of passive solar technology.
 - v. Block size should be less than four hundred (400) feet.
- (c) **Access Points.** A minimum of two (2) access points should be provided for each development, with a minimum of one (1) per every 1,500 feet of boundary recommended.
- (d) **Primary Streets.** Designate primary streets so that all buildings front at least one primary street. Vehicular access should not be located off a Primary Street, unless the parcel is fronted by more than two primary streets.
- (e) Blocks may include interior alleys or lanes.
- (f) **Typical Lot Configuration.** All lots shall have frontage along a public street unless otherwise specified in Building Type requirements. Flag lots are prohibited.

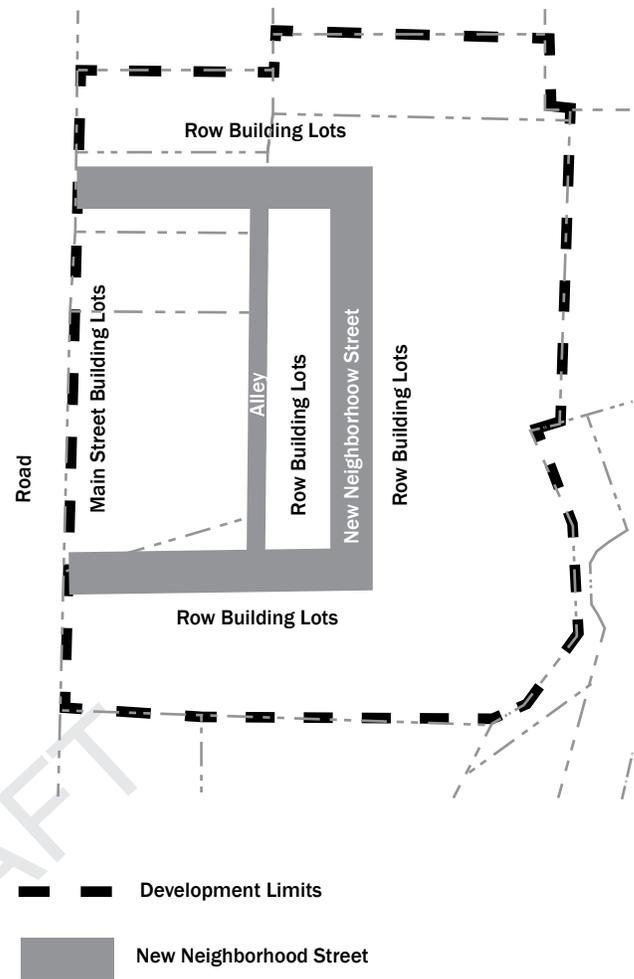


Figure 10-21-5D (2). Example of New Street and Block Configuration for Large Parcels or When Multiple Parcels are Combined.

10-21-5 Site Development Standards

E. Temporary Structures

- General Requirements.** Refer to 10-17 Fencing and Screening for all landscaping and screening requirements.
- Description and intent.** Temporary Structures are allowed in all Districts according to the following guidelines.

The small scale activity or display area can be a patio for outdoor eating or display of goods and should constitute the majority of the site. The frontage required continues the streetwall of the adjacent buildings, allowing a continuous pedestrian experience from the street with views into the outdoor space.

Two accessory structures are permitted. A temporary building may be erected in the rear of the Lot and allows patrons to enter the building. A permanent kiosk may be located anywhere on the Lot, but allows employees only in the interior.
- Regulations.** Regulations for Temporary Structures are defined in the adjacent table.
- Mobile Food Vendors.** Refer to Section 10-3-14 Mobile Food Vendor Vehicles & Retail Vendor Vehicles for all mobile food vendor requirements.

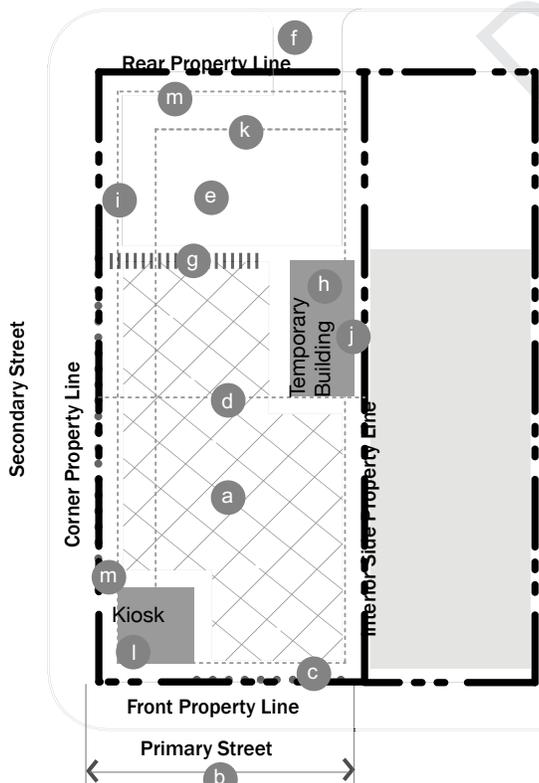


Figure 10-21-5E (1). Temporary Structure Siting

Temporary Structures Requirements		
1. Siting		
Minimum area of Unenclosed, Fully or Partially Paved Outdoor Activity/Display Area	45% of parcel	(a)
Min Lot Width	none	(b)
Max Lot Width	50'	
Max Impervious Coverage	80%	
Additional Semi-pervious Coverage	15%	
Required Front & Corner Side Frontage Type	0' to 5'	(c)
Parking Setbacks		(d)
Front	75'	
Corner, Side, Rear	5'	
Parking & Loading Facility Location		(e)
Access	From Alley or Side Street	(f)
Required Buffer between Parking & Activity/Display Area	none	(g)
2. Accessory Buildings		
Number of Permitted Accessory Buildings	2	
Max Building Coverage	20%	(h)
Temporary Building		
Front Yard Setback	75'	(i)
Corner Side Setback	15'	(j)
Side Yard Setback	0'	
Rear Yard Setback	5'	(k)
Kiosk Building		
Front, Corner, Side, Rear Setback	5'	(l, m)
Max Size	500 sf	
Max Height	one story or 15'	
Min Front Facade Transparency	20%	
Roof Type	Pitched, Flat, Parapet	
Existing Vegetation	May be credited toward buffer area	
3. Uses		
Permitted Uses	Assembly General Retail General Service	

Notes:

- ¹ This screening requirement does not prohibit the installation of or provision for openings necessary for allowable access drives and walkways connecting to the public sidewalk.
- ² In Front and Corner Yards, when the parking area is located adjacent to any building on the lot, the buffer must be located so that it aligns with or is behind the face of the adjacent building back to the vehicular area. The area between the buffer and the property line must be landscaped.

Table 10-21-5E (1). Temporary Structures Requirements.

10-21-5 Site Development Standards

This page intentionally left blank

DRAFT

Administrator Checklist: Pre-Application Meeting

